DOC # 4214066
03/12/2013 03:09:32 PM
Requested By
SPARKS CITY
Washoe County Recorder
Kathryn L. Burke - Recorder
Fee: $276.00 RPTT: $0.00
Page 1 of 235

APN: (Not required
Per NRS 111.312.1)

When recorded, mail to
City of Sparks
P.O. Box 857
Sparks, Nevada 89432-0857
ATTN: Community Services Dep't

(Space above for recorder's use only)

NOTICE OF ADOPTION OF DEVELOPMENT STANDARDS HANDBOOK AMENDMENT

(NRS Chapter 278A)

Name of Development: Legends at Sparks Marina

Name of Plan: Planned Development Handbook, Legends at Sparks Marina, Destination Retail Development.

Approval Date: May 9, 2011

City No. PCN10020

NOTICE IS HEREBY GIVEN that on the above indicated date, the City Council of the City of Sparks, Nevada, gave final approval to the above described plan as the development plan for the above named development, whose legal description is attached as **Exhibit A**.

Pursuant to NRS 278A.570 (2) after this amendment is recorded, all zoning and subdivision regulations applicable to the property described in **Exhibit A** cease to apply and are replaced with the plan attached as **Exhibit B** hereto.

Dated this 11th day of March, 2013

City of Sparks, Nevada

By Teresa Gardner
Teresa Gardner, City Clerk

State of Nevada)
) Acknowledgement in representative capacity
County of Washoe) (NRS 240.1665)

This instrument was acknowledged before me on 3-11-13
By TERESA GARDNER, as City Clerk for the City of Sparks, Nevada

C BROOKS
Notary Public - State of Nevada
Appointment Recorded in Washoe County
No: 09-9466-2 - Expires January 14, 2017

C Brooks
Notary Public

City File #	Adopted/ Approved	Approval Date	Recorded in Official Records of Washoe County:
PCN06028	Legends at Sparks Marina PD Handbook	12-11-06	Recorded on March 13, 2007 Doc. # 3508258
PCN08032 PCN06028	Legends at Sparks Marina PD Handbook Amendment of page	05-19-08	Recorded on September 09, 2008 Doc. # 3688961
PCN10020	Legends at Sparks Marina PD Handbook Amendment	05-09-11	Recorded Herewith

EXHIBIT A

LEGAL DESCRIPTION

Planned Unit Development

All that certain parcel of land situate in the South ½ of Section 3 and the North ½ of Section 10, T19N, R20E, M.D.M., City of Sparks, County of Washoe, State of Nevada, being a portion of Parcel 1 of Parcel Map No. 229 for D&B TRUST as recorded in Washoe County Official Records on March 8, 1976 under File No. 399191, and being all of Parcel C of Parcel Map No. 930 for D&B TRUST as recorded in Washoe County Official records on September 4, 1979 under File No. 627277, and being Dedication Tract Map No. 2692 for ROBERT L. HELMS of a portion of East Lincoln Way as recorded in Washoe County Official Records on May 22, 1990 under File No. 1401628 , and being all of Parcels 1 and 3 of Parcel Map No. 2477 for ROBERT L. HELMS as recorded in Washoe County Official Records on November 28, 1990 under File No. 1443479, and being all of Parcels 2-C, 3-A, 3-B and portions of East Lincoln Way and Marina Gateway Drive of Dedication Tract Map No. 4084 of MARINA GATEWAY DRIVE, A PORTION OF EAST LINCOLN WAY and OTHER LANDS TO THE CITY OF SPARKS as recorded in Washoe County Official Records on May 29, 2002 under File No. 2693070, and being all of Parcel C of Parcel Map No. 4081 for MARINA BUSINESS PARK, LLC as recorded in Washoe County Official Records on September 9, 2003 under File No. 2918146 and being more particularly described as follows:

BEGINNING at the most southeasterly corner of said Parcel 1 of Parcel Map No. 2477:

Thence S 80°38'07" W, 65.49 feet along the southern boundary of said Parcel 1;

Thence along a tangent curve to the left having a radius of 1060.00 feet, a central angle of 17°11'19" and an arc length of 318.00 feet along said southern boundary;

Thence S 53°18'19" W, 187.45 feet along the southern boundary of said Parcels 1 and 3 of Parcel Map No. 2477;

Thence S 63°26'48" W, 186.61 feet along the southern boundary of said Parcel 3;

Thence along a tangent curve to the right having a radius of 973.00 feet, a central angle of 20°29'02" and an arc length of 347.86 feet along said southern boundary;

Thence N 81°41'01" W, 1413.29 feet along said southern boundary;

Thence S 00°50'27" W, 43.37 feet along said southern boundary;

Thence S 38°06'40" W, 16.71 feet along said southern boundary;

Thence N 81°41'01" W, 21.28 feet to the southwest corner of said Parcel 3;

Thence N 00°56'20" E, 1928.67 feet along said West boundary of said Parcel 3 and the West boundary of said Parcel 3-B of Dedication Tract Map No. 4084 to the North ¼ corner of said Section 10;

Thence N 00°51'36" E, 925.49 feet along the West boundary of said Parcels 3-B and 3-A to the northwest corner of Parcel 3-A;

Thence N 89°27'50" E, 2.32 feet along the northerly boundary of said Parcel 3-A and southerly right-of-way of East Lincoln Way;

Thence along a tangent curve to the right having a radius of 356.50 feet, a central angle of 29°08'07" and an arc length of 181.28 feet along the northerly boundary of said Parcel 3-A and the southerly right-of-way of East Lincoln Way;

Thence S 71°30'33" E, 283.93 feet;

Thence S 35°52'48" E, 28.21 feet to a point on the easterly right of way said Marina Gateway Drive and a point on the northwesterly boundary of said Parcel C of Parcel Map No. 4081;

Thence N 60°35'39" E, 15.28 feet along the said northwesterly boundary of Parcel C;

Thence along a tangent curve to the left having a radius of 650.00 feet, a central angle of 14°02'05" and an arc length of 159.22 feet along the boundary of said Parcel C;

Thence S 43°32'31"E, 41.79 feet along the boundary of said Parcel C;

Thence S 61°49'09" E, 503.58 feet along the boundary of said Parcel C;

Thence S 89°16'53"E, 221.96 feet to the northeast corner of said Parcel C of Parcel Map No. 4081 to a point on the West boundary of said Parcel C of Parcel Map No. 930;

Thence N 00°41'10" E, 49.93 feet to the northwest corner of said Parcel C of Parcel Map No. 930;

Thence N 89°54'27" E, 1196.47 feet to the northeast corner of said Parcel C;

Thence S 01°07'16 W, 658.53 feet along the East boundary of said Parcel C;

Thence S 00°38'31" W, 427.42 feet along the East boundary of said Parcel C and the East boundary said Parcel 1 of Parcel Map No. 229 to the southeast corner of said Dedication Tract Map No. 2692;

Thence N 78°05'29" W, 168.24 feet along the South boundary of said Dedication Tract Map No. 2692;

Thence S 89°54'05" W, 60.34 feet along said southern boundary to an angle in the boundary of said Parcel 1 of Parcel Map No. 2477;

Thence S 00°38'31" W, 230.84 feet along the boundary of said Parcel 1;

Thence N 89°54'05" E, 225.35 feet along the boundary of said Parcel 1;

Thence S 00° 38'31" W, 722.80 feet along the boundary of said Parcel 1;

Thence S 11°33'53" W, 332.62 feet along said boundary to the point of beginning.

Excepting therefrom Parcel 2 of said Parcel Map No. 2477

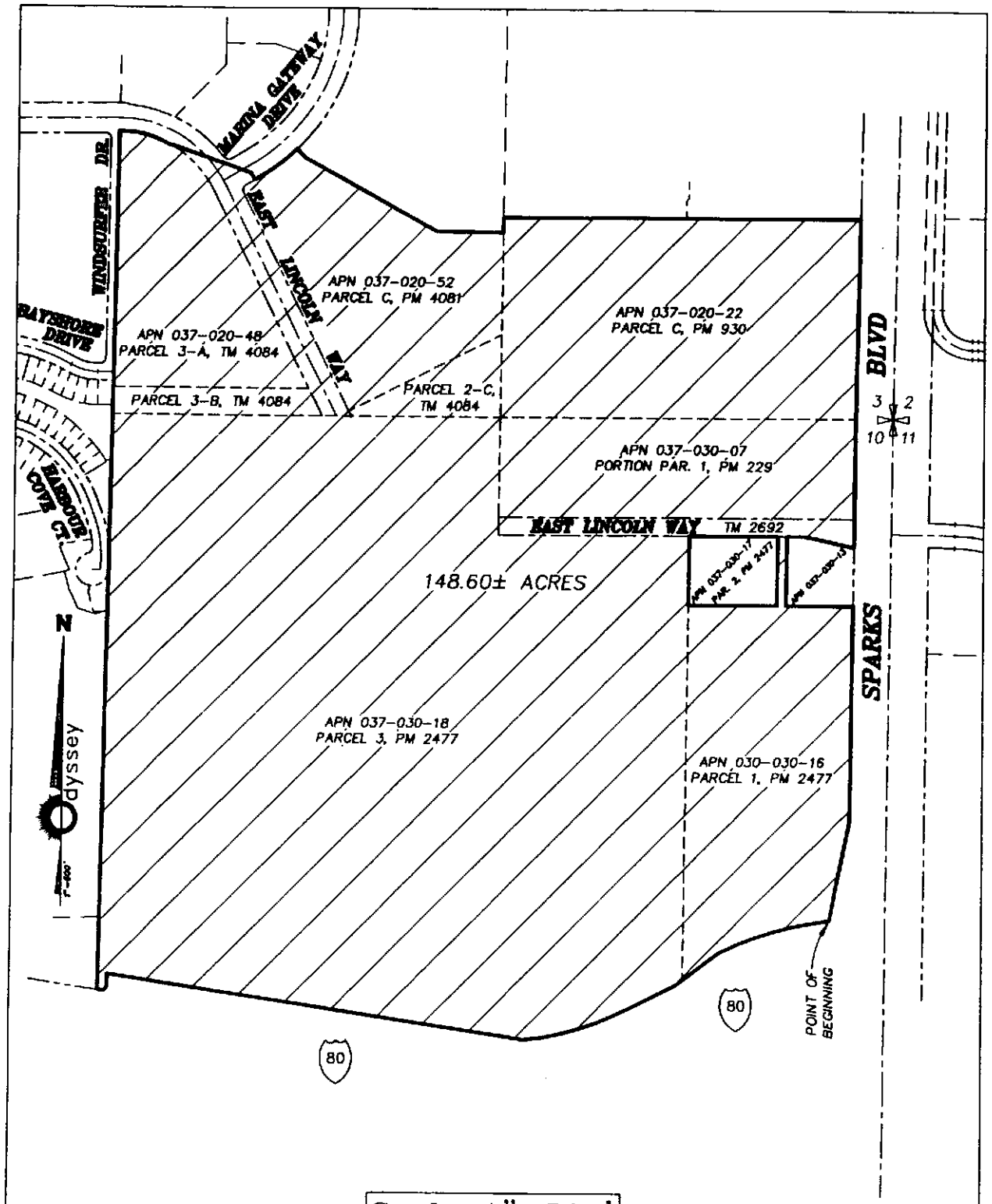
Containing 148.60 Acres more or less

Basis of bearing for this description is NAD 83/94, Nevada State Plane, West Zone, utilizing Washoe County control points and a combined grid to ground factor of 1.000197939.

Prepared by:
Ronald C. Hoops, P.L.S.
ODYSSEY ENGINEERING, INC.
895 Roberta Lane, Suite 104
Sparks, NV 89431

PROFESSIONAL LAND SURVEYOR - STATE OF NEVADA
RONALD C. HOOPS
Exp: 6-30-07
No. 10839

11-3-06



Scale 1"=500'

DISPLAY

Odyssey ENGINEERING INCORPORATED

PLANNED UNIT DEVELOPMENT

Planned Development Handbook

The Legends at Sparks Marina

Destination Retail Development

RED SM

The City of Sparks

Berger Devine Yaeger

THE PRESTON PARTNERSHIP, LLC

cfa PLANNERS • ENGINEERS • LANDSCAPE ARCHITECTS
SURVEYORS • CONSTRUCTION INSPECTION

1150 CORPORATE BLVD. RENO, NV 89502
(775) 856-1150 FAX: (775) 856-1160

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BRUNGARDT HONOMICHL & COMPANY, P.A.
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October 31, 2006

Revised November 10, 2010

Revised April 13, 2020

Revised August 5, 2020

Planned Development Submittal

The Legends at Sparks Marina

October 31, 2006

Revised May 19, 2008

Revised March 30, 2011

Revised April 13, 2020

Revised August 6, 2020

Table of Contents

TAB 4-Exhibits (continued)

Appendices

DEAR APPLICANT:

THE CITY OF SPARKS APPLICATION PROCESS REQUIRES THAT THE PROPERTY OWNER AUTHORIZE THE APPLICANT TO REQUEST DEVELOPMENT RELATED APPLICATIONS. DEVELOPMENT APPROVALS REMAIN WITH THE LAND; THEREFORE, THE PROPERTY OWNER IS ALWAYS RESPONSIBLE FOR ANY ACTIVITY ON THE PROPERTY.

OWNER AFFIDAVIT

STATE OF ~~NEVADA~~ ARIZONA)
) SS.
COUNTY OF ~~WASHOE~~ MARICOPA)

I, RED Sparks SPE, LLC being duly sworn, depose and say that I am an owner of property/authorized agent involved in this petition and that I authorize RED Sparks SPE, LLC to request development related applications on my property. I also give permission for site visitation by the Planning Commission, City Council and City Staff.

RED Sparks SPE, LLC, a Delaware limited liability company

Name: _____

Title: Michael L Ebert, President

Signed _____

Subscribed and sworn to before me this 24th Day of June, 2020.

Renee C Stryjecki, Maricopa AZ
Notary Public in and for said County and State

My commission expires: March 1, 2021

RENEE C STRYJECKI
NOTARY PUBLIC, ARIZONA
MARICOPA COUNTY
My Commission Expires
March 01, 2021

APPLICANT AFFIDAVIT

STATE OF ~~NEVADA~~ ARIZONA)
) SS.
COUNTY OF ~~WASHOE~~ MARICOPA

I, RED Sparks SPE, LLC being duly sworn, depose and say that I am the applicant involved in this petition and that the foregoing statements and answers herein contained and the information herewith submitted are in all respects complete, true and correct to the best of my knowledge and belief. I also give permission for site visitation by the Planning Commission, City Council and City Staff.

Name: RED Sparks SPE, LLC, a Delaware limited liability company

Title: Michael L Ebert, President

Signed: _____

Subscribed and sworn to before me this 24th Day of June, 2020.

Renee C Stryjecki, Maricopa AZ
Notary Public in and for said County and State

My commission expires: March 1, 2021

RENEE C STRYJECKI
NOTARY PUBLIC, ARIZONA
MARICOPA COUNTY
My Commission Expires
March 01, 2021

OWNER AFFIDAVIT

STATE OF NEVADA)
) SS.
COUNTY OF WASHOE)

I,

_____being duly sworn, depose and say that I am an owner of property/authorized agent involved in
this petition and that I authorize
_____to
request development related applications on my property. I also give permission for site visitation by the Planning Commission, City Council and City Staff.

Name:_____

Title:_____

Signed_____

Subscribed and sworn to before me this _____ Day of _____, 20_____.

_____
Notary Public in and for said County and State

My commission expires: _____

APPLICANT AFFIDAVIT

STATE OF NEVADA)
) SS.
COUNTY OF WASHOE)

I,_____
_____being duly sworn, depose and say that I am the applicant involved in this petition and that the foregoing statements and answers herein contained and the information herewith submitted are in all respects complete, true and correct to the best of my knowledge and belief. I also give permission for site visitation by the Planning Commission, City Council and City Staff.

Name:_____

Title:_____

Signed:_____

Subscribed and sworn to before me this _____ Day of _____, 20_____.

_____
Notary Public in and for said County and State

My commission expires: _____

WATER SUPPLY CALCULATIONS

USE TYPE	SQ. FT.	AC. FT. YR. / 10,000 SQ. FT.	DOMESTIC USE AFY (1/4 - COMMERCIAL & BASEBALL, 1/2 - RESTAURANT)	IRRIGATION USE AFY (3/4 - COMMERCIAL & BASEBALL, 1/2 - RESTAURANT)
RETAIL	1,363,499	2	68.17	204.52
HOTEL	76,000	2	3.80	11.40
RESTAURANT	83,700	1.5	6.28	6.28
CONDO	42,000	1.5	3.15	3.15
HEALTH CLUB	32,000	1.5	2.40	2.40
BASEBALL	224,240	1.5	8.41	25.23
		TOTALS	**92.21**	**252.98**

2

Planned Development Submittal

The Legends at Sparks Marina

Chapter 1 Project Description

I. Introduction

Developer/Property Owner Information
Sparks Legends Development, LLC
4717 Central
Kansas City, MO 64112

Officers:

Dan Lowe

Michael Ebert

Scott Rehorn

Jeff McMahon

Steven Maun

Jeff Teets

Assessor's Parcel Numbers and Existing Zoning
This development will occupy parcels south of East Lincoln Way identified as 037-030-16 and 037-030-18 (known as the Capital Development property). The existing Zoning for all parcels is TC (Tourist Commercial). However, a zone change to NUD (New Urban District) is associated with this Planned Development Handbook that will apply to all parcels in the development.

This development will also occupy parcels on the south side of the existing East Lincoln Way right-of-way identified as 037-020-48 and 037-020-49 (known as the Marina Properties property). The existing Zoning for these parcels is NUD (New Urban District).

This development will occupy parcels on the north side of East Lincoln Way identified as 037-020-22 and 037-030-07 (known as the Don Mello property). The existing Zoning for these parcels is TC (Tourist Commercial).

This development will also occupy parcels on the north side of the existing East Lincoln Way right-of-way identified as 037-020-47 and 037-020-52 (known as the Lakemont property). The existing Zoning for these parcels is TC (Tourist Commercial).

Finally, acquisition of portions of three existing parcels is being pursued to obtain additional right-of-way width for Sparks Boulevard at the intersection with Lincoln Way, and for realignment of the Lincoln Way right-of-way at the northwest corner of the development.

The development will occupy a total of approximately 149.85 acres, including all rights-of-way through the development. Of that area, the Lincoln Way right-of-way occupies 10.12 acres, and the new access road rights-of-way occupy 7.47 acres. There will be approximately 17.59 acres devoted to public right-of-way, which includes both new and existing right-of-way. The existing right-of-way includes both existing and planned sections of East Lincoln Way. The proposed use for the development is destination retail (similar to TC-Tourist Commercial) and entertainment. The development is intended to provide a regional destination retail/entertainment development that will be unique in the Reno/Sparks area and become a tourism destination with a market area extending beyond state lines.

II. Project Location

The project location is in the Marina District Redevelopment Area 2 as defined by the City of Sparks Community Development Department. A vicinity map is provided in Exhibit A-1.

III. Project Goals and Policies

This development supports many of the goals and policies of the City of Sparks Master Plan, including the Land Use element and the Transit Oriented Development Corridor Area Plan, as well as goals and policies of the Truckee Meadows Regional Plan. When the Planned Development Handbook was originally adopted in December 2006 and the subsequent amendment adopted in May 2008, it's important to note that the Marina Area Plan was a part of the City's Master Plan document. The Marina Area Plan played a large role in the creation of this planned development handbook and standards contained herein. With the adoption of the Transit Oriented Development Corridor Area Plan in July 2009, the Marina Area Plan was removed from the City's Master Plan document. Although the Marina Area Plan is no longer a part of the City's Master Plan, the goals, policies, and standards from that plan which have been incorporated into this handbook, are consistent with the goals and policies of the City's current master plan document, including the Transit Oriented Development Corridor Area Plan.

City of Sparks Master Plan—Land Use Plan Goals and Policies
The following goals and policies are supported by this development:

GOAL LU1: To create a growth pattern which assures flexible, feasible and efficient developments and which includes natural and cultural amenities."

POLICIES:

LU1a. The City will support a preferred growth pattern which applies consistent and uniform standards to areas planned for similar uses.

LU1b. The City will ensure that development is in accord with the Master Plan and other land use controls to accomplish growth management goals.

LU1c. The City will approve development plans which address conditions unique to the developing area to minimize impacts to adjacent properties and assure protection of natural and cultural resources."

ACTION STRATEGIES

1. Apply appropriate sections of the City's Municipal Code to all development proposals.
2. Allow only developments which meet the proper land use designation of the City's Master Plan and the Regional Master Plan.
3. Review all projects in relation to their geographic location, impacts to adjacent communities, fiscal impact and mitigation measures to protect natural and cultural resources. Apply specific conditions of approval tailored for each development proposal."

GOAL LU4: To ensure areas for open space, recreation and greenbelts. To create identifiable entrances and gateways to the City.

POLICIES:

LU4a. The City supports the protection of wetlands and wildlife habitats.

LU4b. The City supports development of a comprehensive park system which includes neighborhood, community and regional parks.

LU4c. The City supports all scenic and recreational resources to be preserved as open space development within planned developments and/or cluster type projects.

ACTION STRATEGIES

1. Create land uses to designate and protect wetlands and wildlife habitats.

2. Establish designated areas for parks and joint development of recreational facilities.

GOAL LU5: To support land uses and development that assures an appropriate balance of population and employment distribution within the City.

POLICIES:

LU5a. The City will encourage land uses and development which maintains a balance of population, housing and employment within urban areas.

ACTION STRATEGIES

1. Maintain a data base on employment, housing and population. Maintain a potential ratio of 1.5 jobs per dwelling unit by allocating land use acreages in the Master Plan which will accommodate this ratio.

GOAL H1: To provide a spectrum of housing to meet the City's needs, ranging from affordable entry level to more expensive homes, emphasizing value and quality.

POLICIES:

H1a. Plan for adequate residentially zoned property for the development of housing for all income groups.

H1b. Support the development of affordable housing through incentive programs to encourage the construction of housing units affordable to low and moderate income households.

H1c. Review new housing proposals (private, non-profit and public agencies) and support those proposals and programs that meet the City's housing needs and are consistent with other City and Regional goals and policies.

H1e. Pursue federal, state and local funding to support the joint public/private development of low and moderately priced housing."

GOAL H3: To encourage infill, conservation, redevelopment and rehabilitation through public and private development.

POLICIES:

H3f. Provide for high density residential development near the redevelopment core and elsewhere in the community where appropriate.

ACTION STRATEGIES

9. In the City of Sparks review of proposed residential projects is conducted in a timely manner and consistent standards for quality are applied to residential development.

The proposed development will also further the goals and policies of the Marina District within the City of Sparks Transit Oriented Development Corridor Area Plan as detailed below:

Goal 3.1: Highlight Sparks Marina as a Regional Recreation Destination

The City will work with the development community to develop the Sparks Marina District as a vibrant mixed-use area that is connections and mix of uses.

Key strategies for the City to take to achieve this goal:

3.1 a Support Waterfront Redevelopment and Infill

Redevelop underutilized parcels north of the Sparks Marina waterfront to support its mixed-use recreation activities.

3.1 b. Integrate Legends at Sparks Marina with Adjacent Development

New development adjacent the Legends at Sparks Marina project should be integrated to the development through appropriate use, height, and design transitions and through connected streets and pathways.

3.1 c. Build on Recreation and Entertainment Theme

The City will work with the Sparks and Reno-Sparks Chambers of Commerce to market the Sparks Marina area to new businesses to further build on the recreation and entertainment theme.

Goal 3.2: Create Cohesive Neighborhoods throughout the District Emphasizing Connectivity to Sparks Marina

Existing and new neighborhoods within the district should be well connected to the marina activity center to facilitate use of the recreation amenities and goods and services by nearby residents.

Key strategies for the City to take to achieve this goal:

3.2 a. Develop the Northern Gateway Marina Neighborhoods
Develop the vacant lands to the north of the Red Development area as a gateway into the district by emphasizing coordinated landscaping and streetscape improvements. Residential neighborhoods should each relate to the other through transitions and architecturally compatible styles and color schemes in-keeping with the established marina-themed development.

3.2 b. Improve Connections between Existing and Future Neighborhoods and the Marina and Activity Center Development
As redevelopment of underutilized lands around the Marina moves forward, efforts should be made to improve pedestrian connectivity between existing older neighborhoods along Prater Way and the Marina-focused activity.

Truckee Meadows Regional Plan—Plan Goals and Policies
The following goals and policies are supported by this development:

GOAL 1.2: Between 2002 and 2022, Local Government and Affected Entity Master Plans, Facility Plans and other similar plans will provide for the necessary resources, services and infrastructure to support the following population and job growth: at least 35% of the region's population growth and 35% of the region's jobs growth within McCarran Blvd.; and not more than 64% of the region's population growth and 64% of the region's jobs growth within the TMSA, outside McCarran Blvd.

POLICIES:

1.2.2 The Regional Plan defines the following priorities for regional development, in the order listed, for the provision of public facilities and services in order to support the desired development pattern, which may extend beyond this 20 year plan.
1) Downtown Centers;
2) TOD Corridors between Downtown Reno Center and the Downtown Sparks Center and between the Downtown Reno Center and the Meadowood Center;
3) Regional Centers within or transected by the McCarran Blvd. ring road;
4) TOD Corridors from the Downtown Reno Center to North McCarran Blvd., the Downtown Reno Center to West McCarran Blvd., and the Downtown Reno Center to the Reno-Tahoe Airport Center;
5) Infill development within McCarran Blvd.;
6) TOD Corridors from North McCarran Blvd. to the Stead Center, from the Meadowood Center to the Redfield Center, and west from McCarran Blvd. to I-80, and from the **Downtown Sparks Center to Vista Blvd**;

7) Regional Centers outside the McCarran Blvd. ring road; and,
8) All other areas within the Truckee Meadows Service Areas.

1.2.11 To conform with the Regional Plan, Local Government Master Plans must include incentives, strategies, programs or policies to:
1) promote infill development;
2) preserve neighborhoods;
3) revitalize urban areas; and
4) meet the population and employment growth capacity goals consistent with Table 1.2.1 in Policy 1.2.1.;
In addition, Master Plans are encouraged to address human-scale design, inter-connections to other parts of the region and internally, diversity of land uses, and multi-modal transportation. Infill incentives, strategies, programs or policies may provide encouragements for public and/or private improvements in Downtowns and other development target areas. To conform with the Regional Plan, Local Government Master Plans must include appropriate incentives, strategies, programs or policies to promote infill within the McCarran Blvd. Ring road. These may include, but are not limited to:
1) Allowing higher density development – especially transit oriented development – along TOD Corridors and in other designated **infill locations** by right when implemented with development and design standards that provide for compatibility with existing neighborhoods along or adjacent to the TOD Corridor or infill area;
2) Adjustments to permit fees, impact fees and exactions, to encourage infill development within the McCarran ring road, by lowering development costs and accelerating the development approval process;
3) Use of public investment (capital improvement programs, including those of Affected Entities such as the school district and RTC) and subsidies (e.g., redevelopment funds, HOME and CDBG funds, federal transportation money, etc.) to offset costs of infill development;
4) Appropriate development standard adaptations (e.g., parking standard reductions); and
5) Other policies, strategies or programs to achieve the requirements of NRS 278.0274 (3) and (4).

The proposed project is also located within the Transit Oriented Development (TOD) Corridor Area Plan and the City's Redevelopment Area 2.

IV. Surrounding Land Uses/Vicinity

Surrounding Zoning and Land Uses
The current zoning directly adjacent to the proposed development property includes TC (Tourist Commercial), R4/PUD, and NUD (New Urban District) to the north of the development and PD (Planned Development) and R1-40 to the west of the development. Property located across Sparks Boulevard to the east of the proposed development includes properties zoned as I (Industrial), IC (Industrial Commercial) and C2 (Commercial). Property located across Interstate 80 to the south of the proposed development is zoned as CI (Commercial Industrial) and I (Industrial). See Exhibit A-2 for existing zoning map.

The current and master planned land uses directly adjacent to the proposed development property includes TC (Tourist Commercial) and MDR (Medium Density Residential) to the north of the development and P (Park) and HDR (High Density Residential) to the west of the development. Property located across Sparks Boulevard to the east of the proposed development includes existing and planned uses of GC/CI (General Commercial/Commercial Industrial) and I (Industrial). Property located across Interstate 80 to the south of the proposed development includes existing and planned uses of CI (Commercial Industrial) and I (Industrial). See Exhibit A-3 for existing Master Plan land use plan.

Proposed Land Use Relative to the Adopted City Master Plan
The proposed land use is consistent with the adopted City of Sparks Master Plan. The parcel is planned for TC (Tourist Commercial) on the City of Sparks Master Plan Land Use Plan. The proposed land use of the Planned Development is consistent with this zone.

V. Site Analysis

Flood plain/Floodway Issues
The site does not generally include flood plain area, based on the newly revised FEMA flood plain maps, except the portion of the site which is existing lake and a small portion at the southeast corner of the Don Mello property. The portion on the Don Mello property will be removed from the 100-year flood plain via a Letter of Map Revision (LOMR) which is now in process.

Slope Map
The site topography does not require slope analysis.

Site Grading
The existing site is bounded on the south by Interstate Highway I-80, on the east by Sparks Boulevard, on the west by Sparks Marina Lake and existing residential development (multi-family), and on the north by the Ghiggeri property and existing residential property (multifamily). The existing site topography, shown on Exhibits C0.1 and C0.2 is fairly flat with slight fall to the west towards the Marina Lake but the largest amount of relief being construction debris deposits from the previous owner of the land.

Due to the high profile status of this project for the City and RED Development there are many elements and factors that are incorporated and addressed with the site grading. The site grading addresses each of these as follows:

- Provide easy access into the site with gradual and gentle slopes off of East Lincoln Way and Sparks Boulevard. The entry slopes range from 1% to 2%.

- Maintain positive drainage to collection points that convey the storm water to the Sparks Marina Lake or south to the Truckee River. Most of the storm water is conveyed to the Sparks Marina Lake with a small remainder conveyed to the Truckee River. A storm water drainage report has been prepared for this project and is located in the appendix.

- Provide for easy pedestrian access to all areas of the site, including the Marina Pathway. Pedestrian access routes range from 1% to 2% making the entire site easy to navigate by pedestrians, including those with disabilities.

- Provide for a prominent high elevation element that is centrally located to the entertainment district and baseball field. This is located just on the south side of the entrance into the baseball field and the focal point to the entertainment district.

- Provide the elevation element necessary to allow the baseball field to be located below the surrounding topography in a "bowl-like" setting. The baseball field is set approximately 8-15 feet below the surrounding grades.

- Provide for matching the existing boundary topography based on the grading criteria set by the City of Sparks.

- Provide for grading the site to adequately allow for gravity flow of sanitary sewers to the proposed 60" sanitary sewer that will be installed by the City of Sparks; and to an existing sanitary lift station at the northwest corner of the site possessing sufficient capacity to handle a portion of the project's load.

The developer shall submit a grading plan for any phase of the project to the approval of the engineering manager for community development. Prior to the issuance of a grading permit for the development, the developer shall post a surety bond for re-grading and reclamation as well as proof of a storm water discharge permit from the Nevada Department of Environmental Protection.

The site grading is shown on Exhibits C2.1 and C2.2.

VI. Project Description

Planned Components of the Development

The portion of the development south of Lincoln Way (Main Retail Center – Labeled as Retail A through I on Exhibits A-4, A-4.1 and A-4.2), occupying a total of approximately 115.66 acres including rights-of-way, will include retail stores ranging in size from as small as 1000 square

feet up to as much as approximately 250,000 square feet. The majority of the retail space will be developed around a landscaped pedestrian plaza which incorporates numerous landscaped areas and water features that extend throughout the development and connect to the lake west of the property. Part of the retail space may include large retailers which will be located as stand-alone stores away from the main center to make better use of the parking area and to balance the parking needs on the site.

In addition to the retail space, restaurants will be included in the development, possibly including a sports bar/restaurant and a restaurant/live music venue. These spaces will be integrated throughout the center along the pedestrian way and at the lakefront. Additionally, pad sites for additional restaurants are located near the development's main entry drives and the highly visible southeast corner of the site. (Pad sites are labeled as Pads 1 through 13 on Exhibits A-4, A-4.1 and A-4.2)

The development plan also currently includes a minor league baseball stadium, planned to have approximately 5500 fixed seats and 24 private suites. The baseball stadium will add to the entertainment component of the development and will be designed to be physically and aesthetically integrated into the retail and entertainment center.

The inclusion of the baseball stadium in the plan is contingent upon several factors which are not defined at this time (ballpark ownership, financing, franchise agreement, etc.). Therefore, if the baseball stadium is not deemed feasible by the developer, an additional amount (up to 150,000 square feet) of retail, restaurant, or entertainment development may be added to the plan in place of the baseball stadium. Refer to Appendix 3 for a supplementary plan and Appendix 4 for traffic impacts related to this optional scheme.

A single building presently planned for a location directly adjacent to the baseball stadium may include retail space/health club, a hotel (approx. 100 rooms), and residential condominium units (approx. 60 units). The building will include structured parking levels for the hotel and condominium units (approx. 170 spaces). The gross finished floor area will occupy approximately 150,000 square feet.

The portion of the development north of Lincoln Way (Northeast Anchor site), occupying a total of approximately 24.08 acres, will include a large retailer (approx. 204,000 square feet – Retail N on Exhibit A-4) and up to five stand-alone pad sites (Pads 8 through 12 on Exhibits A-4, A-4.1 and A-4.2). The design of the buildings in this portion of the development will incorporate the same materials as the Main Retail Center, with additional materials subject to approval of the administrator.

The portion of the development at the northwest corner of the site (Luxury Motor Coach Dealer – Retail X on Exhibits A-4, A-4.1 and A-4.2), occupying a total of approximately 8.31 acres, will include a single use retailer (approx. 35,000 square feet) for the purpose of sales and minor service of Luxury Motor Coaches and related recreational vehicles and products. The design of the building in this portion of the development will also incorporate the same materials as the Main Retail Center, with additional materials subject to approval of the administrator.

Project Summary Table

Project Component	Land Use	Acres	Area (S.F.)
Main Retail Center A-M, Pads 1-7, 13	Retail/Restaurant/Entertainment/ Hotel/Condo/Retail-Health Club	107.03	1,373,680
Baseball Stadium	Baseball Stadium – Includes Field	Incl. Above	224,240
Northeast Anchor Tenant (Retail N and Pads 8-12)	Retail/Restaurant	34.19	203,319
Luxury Motor Coach Dealer (Retail X)	Retail	8.31	35,200
Public ROW (Lincoln Way)	Public Street	10.12	n/a
Public ROW (Site access)	Public Street	7.47	n/a

A conceptual development plan is attached as Exhibit A-4.

VII. Architectural Theme

The Legends at Sparks Marina development, located next to the Sparks Marina and bounded by I-80 to the south; East Lincoln Way and the northern property line to the north; and Sparks Boulevard on the east, is an exciting Destination Development.

The architectural and landscape elements and character of this site will create a high quality retail lifestyle/entertainment center. Through the use of sophisticated building materials like cultured stone, cast stone and brick combined with diverse and well-designed landscaped areas, the retail center will be an attractive asset to the community. On approach, the extensive landscape canopies will make the site visually pleasing. Upon entering the site the retail buildings will be attractive by the use of quality materials and architectural detailing at the pedestrian level. Paving, landscape and building design will all be coordinated in the level of detail and quality.

Elements such as trellises, colonnades and overhangs will be used on this site. The combination of different materials in the landscape and the building façade is key to keeping the development interesting and enjoyable on the pedestrian level. The main concept of this site allows shoppers to walk all around the site, thus the use of decorative paving surfaces, landscaped areas and covered architectural elements are key to the success of the development.

Illustrative example of development quality.

The buildings on the site will be "four-sided" buildings with the same quality materials being used on every part of the building. Exterior equipment and service and loading areas will be screened with landscape and/or architectural walls. The buildings will include sloped roofs at an angle which is suitable to the architectural style of the buildings. Site and building lighting will help maintain the quality appearance of the project at night.

The shops and restaurants will be designed in a way that interacts with the water and landscaping along the pedestrian path, allowing for key elements along the way, such as fountains, outdoor fireplaces and themed planting areas. A central signage tower attracts the visitor to the center and creates a focal point. Lighting at night brings the place alive for sunset dining by the water's edge or a stroll through the center. Enhanced paving, plantings, fun storefronts, and pedestrian amenities make for an enjoyable daytime experience.

Illustrative example of development quality.

Parking areas will be landscaped to provide a comfortable walk from the car or transit stop to the shopping district. Extensive attention to pedestrian walkways will interconnect different areas of the site and provide connections to the surrounding community. The perimeter streets are buffered with landscaping and bermed to screen the parking areas. Services, roof top equipment, ground equipment and loading will be screened with fragment walls and bermed landscape.

Conceptual elevations are shown in Exhibits A-5.1 through A-5.12 for the Main Retail Center, A5.21 through A5.24 for the Luxury Motor Coach Dealer, and A5.31 through A5.33 for the Northeast Anchor Tenant, and preliminary material selections are shown in Exhibits A-6.1 through A6.7.

VIII. Infrastructure

Water Service

The bulk of the existing site does not have water service but there are adjacent water service mains available for extending service to and through the site. At the abandoned outlet mall there is a main loop that comes off of the water main that runs along Sparks Boulevard. Most of this existing main loop at the abandoned outlet mall will need to be removed to accommodate new construction but the connection to the main along Sparks Boulevard may be utilized. There is also a water main that has been extended east along East Lincoln Way at Marina Gateway Drive

that will be extended along East Lincoln Way to make a connection to the water main on Sparks Boulevard. Exhibits C5.1 and C5.2 show the water system schematic for providing domestic water service and fire protection to the development sites.

The schematic design of the water main for the site development and an application for Level 2 Discovery has been submitted to the Truckee Meadows Water Authority for review. Truckee Meadows Water Authority is in the process of determining the exact amount of water rights. At a later date an application for commercial service will be applied for and the project will be put into the queue for commercial water service.

The City of Sparks currently uses effluent from their sanitary sewer system to irrigate the existing sports fields on the Don Mello parcel. The effluent discharge point is located at the southeast corner of the site. The City has acknowledged that the effluent can be used for irrigation purposes of this development, which can be used as a credit towards obtaining water rights for this property. This effluent will be incorporated into the irrigation for this development during final design.

The developer will dedicate sufficient water rights per S.M.C. 17.12.075(a)(1) to adequately serve the project prior to the issuance of a building permit for the project.

A summary of preliminary water demand calculations is found prior to Chapter 1 of this document.

Sanitary Sewers

The existing site does not have any sanitary sewer service available except the existing sewers that are in place to serve the abandoned outlet mall. Currently, the City of Sparks has completed design to extend a 60" sanitary sewer main interceptor south along Sparks Boulevard that turns westward along the north right-of-way of Interstate Highway I-80 for approximately 1250 feet then continues on south under Interstate Highway I-80 to the treatment plant. This alignment is shown on Exhibits C5.3 and C5.4. The 60" sanitary sewer main is being designed by Shaw Engineers and they are looking at additional alternative routes for the City and putting cost estimates together to confirm the best alignment.

Based on the current design of the proposed 60" sanitary sewer main interceptor and raising the site grade where appropriate it appears that most if not all of the Main Retail Center site can be conveyed to the new interceptor via gravity sewers. However, final design of the interceptor along with final site grades may require a portion of the site to be conveyed to the existing system that is in place for the abandoned outlet mall. The proposed sanitary sewer alignment is shown on Exhibits C5.3 and C5.4.

For the Luxury Motor Coach Dealer, there is an existing 12" sanitary sewer main in the existing right-of-way for East Lincoln Way. Portions of this existing main will need to be relocated, but this main will be sufficient to serve this site.

The City of Sparks currently uses an estimated quantity of 10,000 gallons per day per acre for sanitary sewer usage. Since the total site area is approximately 149.85 acres this would equate to

1,498,500 gallons per day. At this time it would appear that the proposed 60" sanitary sewer facilities have the capacity to serve the site sanitary sewer that would be conveyed to it. Final design will incorporate the size and depths of the proposed sanitary sewer that will convey sewage from the development to the proposed 60' sanitary sewer interceptor.

There is also an existing sanitary sewer lift station off-site near the northwest corner of the property, which, along with its feeder lines, has adequate capacity to serve the northwest portion of the site, including the RV dealership and adjacent pad sites. Utilization of this lift station will reduce the amount of import fill material that would otherwise be required to drain this corner of the site by gravity alone to the 60" sewer noted above.

Storm Water Reports and Plans
The developer shall provide a final hydrological report for the project in conformance with the city's draft Hydrological Criteria and Drainage Design Manual (HCDDM) for review and approval by the Engineering Manager for Community Development prior to approval of a building permit for any portion of the project. Each successive phase of the project shall submit an updated hydrological report for review and approval by the Engineering Manager for Community Development showing the cumulative effect of the developed portion of the project along with the proposed phase's effect on the total discharge into the drainage system. The storm water and drainage plans for the development shall be reviewed and approved by the Engineering Manager for Community Development prior to the issuance of a grading permit for the project.

The developer shall utilize structural controls for the treatment of storm water runoff in accordance with the methods and practices prescribed in the Truckee Meadows Structural Controls Design Manual.

Preliminary Hydrology Report
The hydrology report is attached as Appendix 1.

Traffic Report
The Traffic Report is attached as Appendix 4.

Easements or Restrictions
Utility easements are shown on Exhibit C0.1 and C0.2.

Avigation Easement
The developer shall demonstrate to the approval of the administrator that an avigation easement has been granted to and accepted by the Airport Authority of Washoe County prior to issuance of any building permit for the project.

Waste Management
The developer shall obtain a "will serve" letter from waste management/Sparks sanitation prior to the issuance of a building permit for the project.

District Health Department Requirements
The developer shall comply with the requirements of the Washoe County District Health Department, including but not limited to the items discussed in the letters transmitted to the developer regarding this project, to the approval of the administrator and public works director.

Fire Department Requirements
The developer shall comply with all requirements of the fire department, including but not limited to developing a plan to provide adequate emergency access throughout the site, developing a master fire hydrant and fire flow plan, developing a master fire department connection (FDC) plan, and installing fire suppression and/or alarm systems to the approval of the Fire Chief prior to the issuance of a building permit. Refer to Exhibits C-6.1 and C-6.2.

Reciprocal easements
The developer shall provide irrevocable reciprocal parking, access, storm drainage maintenance, and landscape easements over the entire approximately 149.85 acres commercial site to the approval of the Public Works Director and the Administrator prior to the issuance of a building permit. These easements shall be incorporated into the recorded documents for the commercial site.

IX. Project Phasing

Phasing
The project is proposed to be constructed in multiple phases. Exact timing and sequencing of those phases are to be determined.

The project may be constructed in phases under requirements dictated by market conditions. The Planned Development will permit projects not shown on the approved plan (which may or may not include a casino/hotel/resort) to be constructed in multiple phases. The specifics of such phasing shall be a condition of approval of the Special Use Permit for such project. Any non-restricted gaming operation shall include the construction of more than 200 rooms available for sleeping accommodations and any other requirements set forth in Nevada Revised Statutes for a resort hotel. A non-restricted gaming operation which does not meet these requirements shall not be approved. The sleeping accommodations shall be constructed and operated in conjunction with the non-restricted gaming operation.

Fiscal Analysis
A fiscal analysis of the project has been previously provided to the City of Sparks.

X. Administration

Submittal Requirements for Projects within The Legends at Sparks Marina

1. Site Plan Review

The development shall be approved and adopted by the City of Sparks as a Planned Development project with The Legends at Sparks Marina Design Standards as the controlling document for project design. A site plan review shall be required in conjunction with the development of each zone within The Legends at Sparks Marina development. Exhibit A-12 in Appendix 5 describes the general limits of each proposed review zone. A zone may include multiple parcels. If certain allowable uses not shown on the approved plan are proposed for the development, a Special Use Permit may be required (refer to 'Allowable Uses' in Chapter 2). Refer to the following Project Approval Process for specific submittal requirements.

The adopted standards form the framework for development. The Legends at Sparks Marina Owners shall certify in writing conformance with the standards prior to submittal to the City of Sparks. All development projects will be required to conform to the Final Planned Development and The Legends at Sparks Marina Design Standards. A Site Plan Review application shall be submitted for each zone per the City of Sparks. With each site plan review, the applicant shall demonstrate how the request conforms to the Design Standards, Circulation Plan, Landscaping, Parking, Architecture and Common Open Space. Projects shall be required to submit all plans for review and approval through The Legends at Sparks Marina Owner's certification process and the City of Sparks Site Plan Review process. The City of Sparks and The Legends at Sparks Marina Owner shall determine the project's conformance to the Standards set forth for development.

All architectural elevations shall be reviewed and approved by the planning commission as a general business item as a condition of the site plan review prior to the issuance of any building permit.

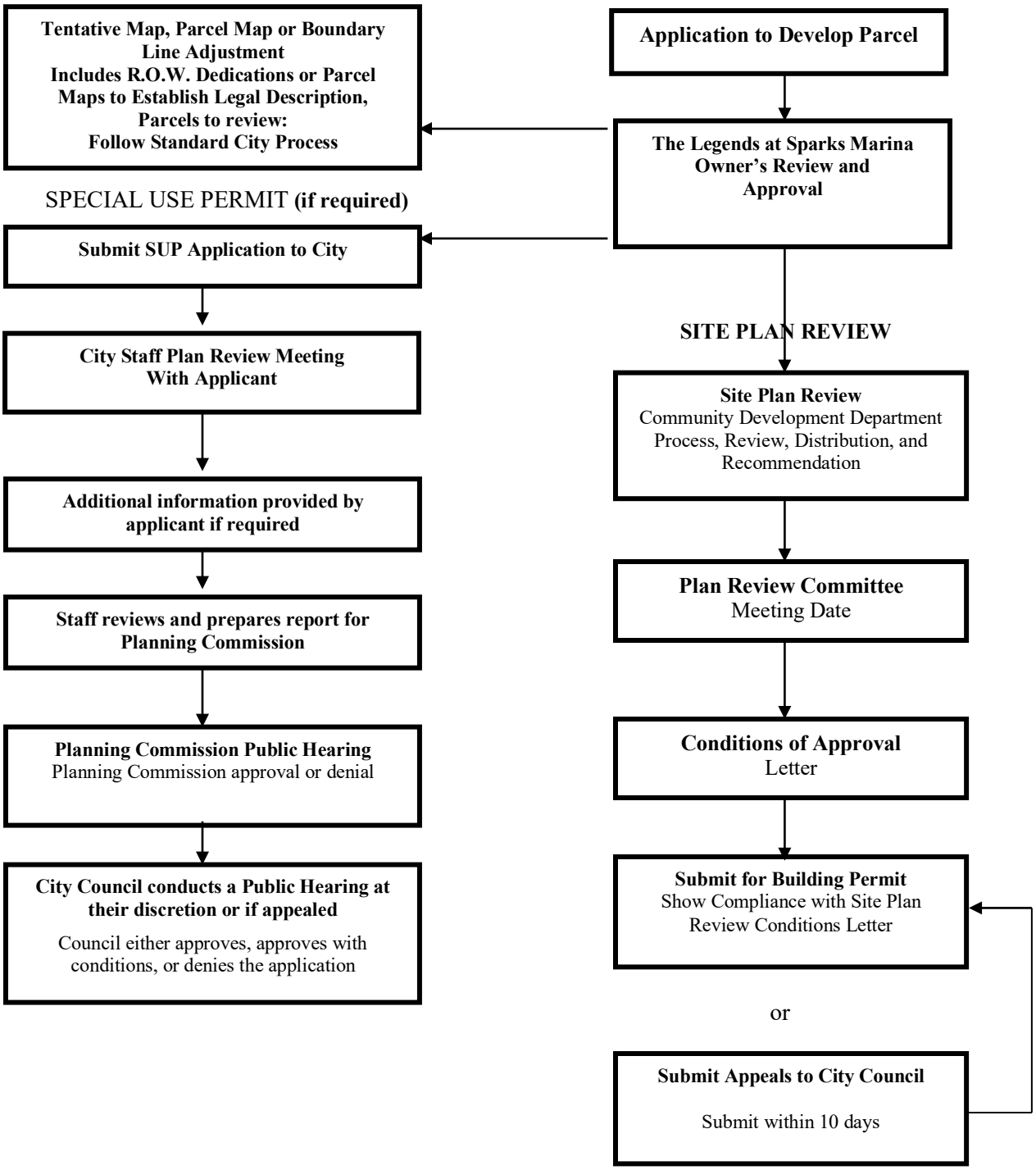
Each developed parcel within The Legends at Sparks Marina shall require a Commercial Subdivision or Parcel map to be submitted when the initial map is recorded. Subsequent legal descriptions and Record of Survey will be required to further subdivide the properties.

2. PD Standards and Planned Development Shall Apply

The standards set forth within these PD Design Standards and the Final Planned Development regulate the land uses and establish the development standards for The Legends at Sparks Marina Development. In case of a conflict with any State or Federal regulation, the more restrictive shall apply. When an item is not addressed, Sparks Municipal Code, State or Federal regulations shall apply.

3. Process Flow Chart

The following flow chart depicts the Site Plan Review process to be followed for submittal and approval of any commercial project within The Legends at Sparks Marina development.



For direction pursuant to the final approvals required for each parcel's development, it is recommended The Legends at Sparks Marina Design Standards be reviewed in their entirety. Special attention should be given to the Design Standards and the "Building Permit Application Checklist".

Implementation – The Legends at Sparks Marina Owners, Master and Parcel Developer Responsibility

1. Definitions

- "The Legends at Sparks Marina Owner" is defined as Sparks Legends Development, LLC, their successors and assigns. They may also be the Master Developer.

- "Master Developer": is defined as the entity or corporation that owns or controls all of The Legends at Sparks Marina. The Master Developer may also be a Parcel Developer. The Master Developer may also be referred to in these regulations as the "Developer."

- "Parcel Developer": is defined as the entity or corporation that purchases one or more of the development parcels or phases from the Master Developer or The Legends at Sparks Marina Owners. His/Her responsibility is to construct the commercial construction, and infrastructure necessary to support those facilities, per the Development Standards and Regulations and all other applicable codes and regulations. The Parcel Developer may also be referred to in these regulations as the "Tenant."

The design standards shall be used by the Master Developer (or his designee) and City of Sparks agencies to review each Parcel Developer's proposal for conformance with the overall design objectives. Design review by the Master Developer shall be a required element of all purchase agreements entered into by the Master Developer and each Parcel Developer, if different from the Master Developer. The Master Developer will provide each Parcel Developer with a copy of the approved Design Standards and supplemental CC&R's. These documents should be reviewed carefully by each Parcel Developer.

It shall be the responsibility of each Parcel Developer within The Legends at Sparks Marina development to prepare development plans including landscape design plans in conformance with the Design Standards. The Master Developer (or his designee) shall review all proposed plans for development prepared pursuant to these Design Standards prior to submission to the City of Sparks. The purpose of this initial plan review is to ensure compliance with the plan as finally approved by the City of Sparks. A letter and checklist from the Master Developer and or The Legends at Sparks Marina Owners, stating that the project is in conformance with the plan must accompany any project submitted to the City of Sparks.

The City of Sparks requires a licensed landscape architect to prepare landscape plans according to these standards. Prior to installation of landscape improvements all plans are to be reviewed by the City and Master Developer (or designee), as provided by these Design Standards recorded against each property. Approval by Master Developer does not imply that the project is in compliance with all applicable city codes, ordinances, or other regulations. Each Parcel Developer is responsible for submittal and processing of all final plans and permits reviewed and approved by the City prior to commencement of construction.

2. Site Plan Review Requirements

The site plan review shall use the Sparks application forms, complying with the submittal requirements for minor Site Plan review.

3. Administrative Approval for Minor Revisions

The Administrator shall have the authority at his/her discretion to administratively approve minor deviations in the plans, standards, and guidelines as requested by the Developer prior to the submission of a final development plan for each phase. Minor deviations include but are not limited to such items as parcel configurations, parcel sizes, irregular lots, building heights and setback conditions (up to 10%). Any deviation or modification that exceeds 10% will require approval by the planning commission as a General Business Item.

The substitution of retail space (up to 150,000 square feet) for the proposed baseball stadium (as described in Section VI--Project Description), if proposed by the Master Developer, will be considered a minor revision and will not require an amendment of the final planned development. Additional information, including an alternate development plan and information regarding the impact of the change on traffic and utilities, is included in Appendix 4.

Where provisions of the plan do not address a specific subject, provisions of the Sparks Municipal code and NRS or other ordinances governing development of land shall prevail. The provisions outlined in NRS 278A.390 must run in favor of and are enforceable by the City of Sparks.

4. Construction Phase Issues

Construction Phase Issues
The developer shall comply with all requirements of the building department including, but not limited to, providing plans that show where the property lines are to be located, to the approval of the building official prior to the issuance of a building permit.

The developer shall designate to the Administrator a project contact person responsible/authorized to correct problems regarding the project on a 24-hour/7-days a week basis. The developer shall designate the project contact person to the Administrator prior to issuance of a grading permit for the project.

The developer shall install any on- and off-site improvements including but not limited to street lights to the approval of the Engineering Manager for Community Development and the Administrator.

The developer shall limit all outdoor and loud noise-generating construction and construction-related activities to between the hours of 7:00 a.m. to 7:00 p.m., Monday through Saturday and 9:00 a.m. to 5:00 p.m. on Sunday. Exceptions may be granted (with the approval of the Administrator) and may include items such as concrete or asphalt placements 10,000 sf in size or greater, and landscaping in the pedestrian plaza. The developer shall install signs, in English and Spanish, at all access points to the project that clearly indicate these limited hours of activity on-

site prior to the start of any construction-related activities to the approval of the administrator. The developer shall maintain these signs in good repair for the duration of the construction of the project. Once construction is finished, the developer shall remove these signs.

The developer shall locate and utilize a sufficient number of trash containers on-site to be utilized during the construction of the project to maintain the project site in a clean and orderly state to the approval of the Administrator.

No combustibles may be brought on-site prior to the installation and acceptance by the Fire Chief and Engineering Manager for Community Development of an all weather surface and installation of fire hydrants to the approval of the Administrator, Engineering Manager, and Fire Chief.

The developer shall obtain encroachment permits from the Nevada Department of Transportation for all improvements within the State's right-of-way, prior to commencing any work in the State's right-of-way.

Planned Development Submittal

The Legends at Sparks Marina

Chapter 2 Development Standards

I. Project General Standards

General project standards shall conform to the City of Sparks Design Standards Manual, the City of Sparks Master Plan, including the Transit Oriented Development Corridor Area Plan, and Sparks Municipal Code for TC zoning, except as designated herein.

II. Specific Standards—Sparks Marina Destination Development

Permitted Uses
Uses permitted in this project include all uses permitted in the TC zoning district, including comparison goods, shopping facilities, offices and clinics, retail and wholesale stores and personal service shops conducted wholly within a building, entertainment facilities, accessory uses, automobile service stations, restaurants, bars and health clubs, hotels and motels having one hundred-fifty rooms or less, and drive-through businesses (in accordance with Sparks Municipal Code).

The following additional uses will also be permitted:

- Luxury motor coach dealer
- Motorcycle showroom and sales
- Recreational boat sales

<u>Luxury Motor Coach Dealer</u> Luxury Motor Coach sales will be a permitted use, with the following stipulations:

Only one Luxury Motor Coach Dealer may be built in the entire Legends at Sparks Marina development and shall be generally located in the northwest portion of the proposed development.

Definition: Retailer whose primary purpose is to sell new and pre-owned recreational vehicles, parts, accessories, and service to support the sales of recreational vehicles. Prominently displayed products will be drivable, high-end motor coaches similar but not limited to product manufactured by Fleetwood, Gulf Stream, Jaco, Coachman, and Winnebago. The outdoor product display areas shall be attractive, pedestrian friendly landscaped areas to promote the intended high quality development. The architectural design shall provide a high quality building similar to the main shopping center to complement the outside display areas similar to the attached building elevations (see Exhibits A-5.21 through A-5.24).

Motor coaches on the site shall be screened from adjacent residential property in the northwest corner of the site by landscaping and a buffer wall conforming with the Planned Development landscaping standards. Buffer wall shall be constructed of masonry and be a

minimum of six feet in height and a maximum of eight feet. Landscaping shall be included on both sides of the buffer wall, and shall include evergreen trees on the residential side of the wall (at a minimum spacing of 20 feet on center), and shall be a minimum of 6 feet in height at time of planting. See Exhibits A-9.1 and A9.2.

An outdoor feature display area or areas for motor coaches may be included which incorporates features such as landscaping, covered areas, and other pedestrian friendly amenities. The feature display areas may not exceed 10% of the total luxury motor coach dealer site area or contain more than 10 vehicles. Additional paved vehicle display areas may be visible from adjacent right-of-ways, provided that the street frontage landscape noted in the landscape standards below are maintained.

Any additional vehicle storage areas shall be screened from adjacent properties and right-of-ways by landscaping. Any vehicle storage areas adjacent to existing residential area northwest of the site shall be screened as noted above.

All service of vehicles shall occur within the service area of the building, except for minor incidental service as may be required. Outdoor storage of vehicles that are in states of visible disassembly or disrepair is prohibited.

Motorcycle Showroom and Sales Motorcycle sales will be a permitted use in the development, with the following stipulations:

A special use permit shall be required for a development on this site including a motorcycle showroom and sales.

Outdoor displays of new merchandise and a limited amount of pre-owned vehicles (50% or less of displayed vehicles) shall be restricted to a designated area or areas not exceeding 10% the total motorcycle showroom site area. Antique motorcycles may also be displayed outdoors.

The building(s) on the site and any outdoor display or storage areas shall be screened from adjacent residential property (if applicable) in the northwest corner of the site by landscaping and a buffer wall. Buffer wall shall be constructed of masonry and be a minimum of six feet in height and a maximum of eight feet. Landscaping shall be included on both sides of the buffer wall and shall include evergreen trees on the residential side of the wall at a (minimum spacing of 20 feet on center) and shall be a minimum of 6 feet in height at time of planting.

Recreational Boat Sales and Display Recreational boat sales and display will be a permitted use in the development, with the following stipulations:

A special use permit shall be required for a development on this site including a boat showroom and sales. The use will be allowable without a Special Use Permit as an ancillary use to an outdoor recreation retailer (less than 20% of gross sales).

Outdoor feature display area or areas will be permitted for a total of up to 30 boats in areas specifically designated for display (maximum of 4) which may be visible from any adjacent right-of-way, provided that the street frontage landscape noted in the landscape standards below are maintained. Display areas shall be attractive, integrating landscaping, decorative paving, and similar amenities. New merchandise and a limited amount of pre-owned boats (50% or less of displayed boats) will be permitted to be displayed outdoors. For an ancillary use, a total of 12 boats in 2 locations will be allowable.

All boats and trailers on the site shall be screened from adjacent residential (if applicable) in the northwest corner of the site by landscaping and a buffer wall. Buffer wall shall be constructed of masonry and be a minimum of six feet in height and a maximum of eight feet. Landscaping shall be included on both sides of the buffer wall and shall include evergreen trees on the residential side of the wall (at a minimum spacing of 20 feet on center) and shall be a minimum of 6 feet in height at time of planting.

Uses Requiring a Special Use Permit
The following uses from the City of Sparks TC Zoning classification may be constructed with a Special Use Permit:

Multi-family residential dwellings, outdoor sales and service operations (except as noted above for Luxury Motor Coach Dealers and Boat Sales and Display), hotels and motels with more than one hundred-fifty rooms, non-restricted gaming operations, child care facilities requiring more than one care giver, private sports and recreation facilities, and drive-through businesses (as required by Sparks Municipal Code).

Any structures proposed to be built within 300' of an adjacent existing residential structure shall be subject to review and approval of operating hours and/or buffering and noise mitigation measures to the approval of the Administrator.

Lot and Setback Requirements
There shall be no minimum site area.

There shall be no minimum lot width.

There shall be no front, side, or rear setback requirement, except as follows: A minimum separation of 75' shall be maintained between any structure located within the development and any residential dwelling located on adjacent properties. Building setbacks along the interstate right-of-way shall be 40' minimum.

There is no building height restriction.

A site plan with dimensions is attached as Exhibit C1.1 and C1.2.

Other Restrictions for All Allowable Uses
Parking Lot Sales: There shall be no parking lot sales including temporary sales or auto sales unless issued a temporary use permit by the City of Sparks.

Site Maintenance: Noise generating maintenance such as parking lot sweeping or snow removal shall be limited from 7:00 am to 10:00 pm, seven days a week. An exception will be made for areas included in zones A, B & C.

Delivery Hours: Deliveries shall be limited from 7:00 am to 10:00 pm, seven days a week. No delivery truck idling shall be permitted on site outside of these hours.

Storage Containers: There shall be no temporary storage containers used anywhere on the development site unless in conjunction with an approved special event.

Special events, outdoor sales and display, or any other events covered under the provisions of the Sparks Municipal Code, are subject to any requirements contained therein. Extensions or waivers to the number of duration of events are subject to review and approval of the Administrator.

III. Street and Pedestrian System

Streets and vehicular access
Street improvements, site access, and traffic control shall be determined jointly by the master developer and City of Sparks based on the traffic study (See Appendix 4). Proposed street improvements and site access are shown in Exhibits C1.1, C1.2, and C7.1-C7.3. In general, vehicular circulation within the development shall follow the City of Sparks Design Standards Manual.

The developer shall submit improvement plans with roadway cross-sections that comply with the city's pavement standards to the approval of the Engineering Manager for Community Development. The installed pavement sections shall comply with the approved improvement plans. The plans shall also include all necessary improvements to provide irrigation to landscape medians and islands within the right-of-way to the approval of the Engineering Manager for Community Development, the Parks & Recreation Director and the Administrator.

The developer shall comply with the requirements of the Nevada Department of Transportation to the approval of the Administrator and Public Works Director.

The developer shall complete improvements to the intersection of Sparks Boulevard and East Lincoln Way prior to the issuance of the first certificate of occupancy for the project; to the approval of the Public Works Director, that provide the following features:

A) A minimum of one exclusive left turn lane (minimum of 150 feet of storage length) and one shared through/right turn lane on the east approach.

B) Dual left turn lanes (minimum storage length of 360 feet) and one through lane and one free flow right turn lane on the west approach. The free flow right turn lane should extend to the east site driveway.
C) The free flow right turn lane should extend to the east site driveway.
D) One left turn lane, three through lanes, and one free flow right turn lane on the north approach.
E) The free flow right turn lane should extend to the north to the site driveway.
F) Triple left turn lanes (minimum storage length of 680 feet), two through lanes and one exclusive right turn lane on the south approach.

The developer shall complete improvements to the south access intersection with Sparks Boulevard, prior to issuance of the first certificate of occupancy, to the approval of the Public Works Director, that limits traffic to right-in/right-out movements only and provides an exclusive right turn lane at the north approach with a minimum of 150 feet of deceleration length.

The developer shall complete improvements to the north access intersection with Sparks Boulevard, prior to issuance of the first certificate of occupancy, to the approval of the Public Works Director, that limits traffic to right-in/right-out movements only and provides an exclusive right turn lane at the north approach with a minimum of 150 feet of deceleration length.

The developer shall complete construction/modification of East Lincoln Way from the intersection with Sparks Boulevard westerly to the east access roundabout as a 6-lane roadway with a right turn lane extending to the east site driveway, per the Regional Transportation Commission's standard roadway section, to the approval of the Public Works Director, prior to the issuance of a certificate of occupancy for the project.

The developer shall complete construction of East Lincoln Way from the east access roundabout to Marina Gateway Drive as a four-lane roadway, per the Regional Transportation Commission's standard roadway section, to the approval of the public works director, prior to the issuance of the first certificate of occupancy for the project.

The developer shall widen Sparks Boulevard to six lanes from the intersection with East Lincoln Way to the intersection with Interstate 80, prior to the issuance of the first certificate of occupancy, to the approval of the Public Works Director.

The developer shall construct a 5-legged multi-lane roundabout at the intersection of East Lincoln Way and the project's main access, prior to the issuance of the first certificate of occupancy for the project, to the approval of the public works director that provides the following features:

A) One left turn lane and one shared left turn-through-right turn lane for the north leg of the roundabout.

B) One shared left-turn through lane and one right turn lane for the south leg of the roundabout.
C) An exclusive left-turn lane, one shared left/through lane, and one shared though/right-turn lane for the east leg of the roundabout.
D) One shared left-turn/through lane, one shared through/right-turn lane, and flared right-turn lane for the northwest leg of the roundabout.
E) One shared left-turn through lane and one right turn lane for the southwest leg of the roundabout.

The developer shall construct a two lane roundabout at the intersection of East Lincoln Way and the project's west access, prior to the issuance of the first certificate of occupancy, to the approval of the public works director that provides the following features:

A) One shared through/left-turn lane and one shared through-right turn lane for the west leg of the roundabout.
B) One shared through/left-turn lane and one shared through-right turn lane for the east leg of the roundabout.
C) One shared through/left-turn and one right turn lane for the south leg of the roundabout.
D) A single lane approach for the north leg of the roundabout.

The developer shall contribute $500,000 to assist with funding of alternative and environmental studies to support long term improvements of the Sparks Boulevard/Interstate 80 interchange prior to the issuance of the first certificate of occupancy on the project.

The developer shall enter into a Capital Contribution Front Ending Agreement (CCFEA) with the City of Sparks and the Regional Transportation Commission for qualified improvements to the regional road network prior to commencing any related improvements, to the approval of the Public Works Director.

The developer shall comply with the requirements of the Regional Transportation Commission, including but not limited to the items discussed in the letters dated May 17, 2005 & October 4, 2005, to the approval of the Administrator and Public Works Director.

The developer shall work with the regional transportation commission to augment the transit service before and after events utilizing the stadium. The developer shall establish an acceptable plan which promotes the use of the public transit system to the approval of the administrator and the regional transportation commission.

Prior to issuance of any certificate of occupancy for any business employing more than one hundred people, the developer shall have an employee trip reduction (ETR) program approved by the administrator upon recommendation from RTC. The ETR program should include some combination of transit use, bicycling, walking, carpooling, parking management, flexible work schedules and telecommuting and shall appoint a designated ETR coordinator on-site.

The approval of any SUP required for any project not indicated on the approved plan may require additional street improvements beyond the limits of the development to handle project-specific projections of increased traffic identified by the traffic study included with the SUP.

Transit Facility
The developer shall work with RTC to incorporate a transit facility into the design of the project. The transit facility shall be located within the project site. The transit facility shall include shelters, be located conveniently, and provide sufficient space for several buses to the approval of the Administrator and Regional Transportation Commission prior to the issuance of the certificate of occupancy for the first building constructed.

Parking Lot Design (DSM 3.B.1)
All circulation between different parking areas shall be within the property. Dead-end aisles are prohibited.

A clear hierarchy of circulation will be evident. Major entry drives shall be four or more lanes, have landscaped medians, and have no parking. Major circulating drives shall have no parking and limited direct access from major parking fields. Pedestrian circulation through the parking areas shall be marked by landscaping, separate walking surfaces, and pedestrian scale lighting, directional signage, etc.

A very limited amount of parking (within the 10% allowable) may be located in rear service areas of buildings not part of the main retail center. Parking areas and drives shall be separated from buildings by sidewalks and landscape strips of widths no less than required by the Design Standards (DSM 3.B.1.e). Shopping cart corrals shall be provided adjacent to retailers using shopping carts. The developer and/or tenants must demonstrate an effective shopping cart retrieval program to the satisfaction of the administrator prior to issuance of certificate of occupancy. For businesses that use shopping carts, shopping cart storage shall be located within an enclosed portion of the building, screening it from view from the outside and designed as part of the building architecture.

Refer to Exhibit A-4, Development Site Plan, and Exhibit C1.1 and C1.2—Site Dimension Plans for parking lot layout.

Project Entry Design (DSM 3.B.2 and 4)
The spacing of the two major entries has been maximized given the site configuration and the need to provide proper separation from the signalized intersection at Sparks Boulevard. The entries shall be coordinated with median breaks in the improved East Lincoln Way roadway and with access to sites across East Lincoln Way. The entry drives shall be sufficiently long to prevent traffic back-ups onto East Lincoln Way. Refer to Exhibit A-4, Development Site Plan, and Exhibit C1.1 and C1.2—Site Dimension Plans for entry drive layout.

Pedestrian Circulation

Photo 1--Existing Pedestrian Path

There shall be an attractive continuation of the existing walking trail around the lake (see Photo 1) which will permit the general public to access all parts of the walking trail around the lake from either on or off-site and will permit and encourage the use of the walking trail to provide access to the amenities of the development including the common open space, the commercial activity, and the cultural and entertainment functions within the development.

Pedestrian connections from the Sparks Marina Lake to the businesses located along Sparks Boulevard shall be provided. The plan provides for these connections via four landscaped pedestrian circulation links between the main shopping plaza area and the businesses located on the east side of the site, with continuing access to and along the Sparks Boulevard frontage.

Pedestrian Access (DSM 3.B.5)
The development shall place a high priority on the safety, comfort, and enjoyment of pedestrians. A major portion of the development shall be committed to a pedestrian way that will include landscaping, pedestrian amenities such as benches and shade/cover, and water features including fountains and pools. Overhead overhangs, trellises, and other sun screening devices will provide cover and shade for pedestrians (photo 2).

Photo 2—Example pedestrian area with overhang and enhanced paving

The parking lot layout provides for pedestrians to cross parking aisles or landscape islands to access pedestrian circulation paths, building entries and pedestrian plaza entries. A clear, safe

pedestrian way from the transit stop to the development shall be provided. Several drop-off points within the development at key entry areas will also be provided. Refer to Exhibit A-4.

Bicycle racks shall be provided in the development to accommodate cyclists accessing the project.

All areas of the project shall be designed for access by disabled persons. Disabled parking facilities shall meet ADA and Building code standards. Circulation areas within the development and between different areas of the development shall be designed to be fully accessible.

Several connections to off-site public sidewalks will be provided, primarily at the two main entry drives off East Lincoln Way, the east main entry drive off East Lincoln Way for the northeast retail site, the pedestrian circulation path leading to the transit stop, at the two right-in, right-out drives at Sparks Boulevard, and at the public pedestrian trail around Sparks Marina. All of these pedestrian connections shall meet the standards outlined in the design guide for a 'main entry sidewalk' (DSM 3.B.5.g).

The proposed plan complements the waterfront setting by orienting dining establishments in close proximity to the water to provide views of the lake and create a low scale, high quality aesthetic adjacent to the natural environment. The largely vacant existing site will be transformed into a pleasant environment with extensive high quality landscaping and pedestrian oriented amenities (Photo 3). The integrity of the public Marina pedestrian path around the lake shall be maintained.

Photo 3—Example landscaping and pedestrian amenities

Interconnectivity between different areas of the development shall also be provided. Other pedestrian paths tying the development to adjacent residential areas shall be provided. Refer to Exhibits A-7.1 and A-7.2 for further information.

IV. Parking Standards

All parking for the development shall conform to SMC Section 20.49, except as follows:

- Required parking spaces may be located on any parcel within the Planned Development.
- Parking spaces shall be provided at the rate as follows:
    - at the rate of no less than 4.25 spaces per 1000 square feet of gross floor area for all uses in the development (excluding hotels and automotive service areas) ,
    - the Luxury Motor Coach Dealer and automotive service businesses shall require 4.00 spaces per 1000 square feet of office/sales area and 1 space per 1000 square feet of service area,
    - Hotels shall park at 1 space per room and 1 space per 800 square feet of restaurant or public meeting area.
- Irrevocable reciprocal parking and access easements between all parcels in the proposed development will be required.
- Parking space and drive aisle dimensions shall conform to Sparks Municipal Code, except that small diamond-shaped tree islands as noted under VII—Landscape Architecture shall be allowed to occupy a portion of standard parking stalls.
- Parking on gravel lots in order to accommodate visitor/guest/exhibitor parking for events sanctioned Legends shall be permitted. All necessary dust abatement will be implemented and any resulting dirt/mud carried to adjoining hard surface streets will immediately be abated.

Overnight parking shall only be permitted for exhibitors participating in Legends sanctioned events. In no case shall overnight camping be permitted including in RV's.

The developer shall monitor the cumulative tenant uses and on-site parking requirements to assure overall parking for the project meets the parking requirements noted above throughout the life of the project. The developer/owner shall submit the most current cumulative parking calculations (total number of parking spaces on-site; parking spaces already dedicated to existing tenants; parking spaces dedicated to new tenant; and remaining number of undedicated parking spaces) to the Community Development Department upon request.

If additional allowable uses requiring a Special Use Permit, not indicated on the approved plans, are proposed, the Special Use Permit application shall document that the additional use adequately accommodates its calculated parking requirements in addition to those provided for the remainder of the development.

V. Architecture

Design Standards
Architectural design of the main retail center shall conform to the spirit and intent of the City of Sparks Design Standards Manual, as to architectural quality and detail, with dimensional variation throughout. The architectural design and exterior building materials shall be consistent with the architectural design as submitted herein and material sample boards submitted under

separate cover (photos submitted herein). Architectural elevations shall be subject to review and approval by the planning commission, excluding interior plaza storefronts.

Architectural design of surrounding retailers and out-parcel structures shall conform to the City of Sparks Design Standards Manual. The architectural design and exterior building materials shall be consistent with the architectural design of the main retail center. Architectural elevations shall be subject to review and approval by the planning commission.

Height

Building heights shall be properly scaled for context off site as well as within the development in regard to sunlight, ventilation, and public views. Building heights shall be appropriate to the setbacks required adjacent to existing residential uses.

Exterior Walls

Building walls shall be articulated on all sides using differing wall planes, material changes, color differentiation, and architectural detail. The various buildings in the development shall have a common palette of colors and materials, but may be varied in terms of heights, quantities of materials, architectural treatments, and relationship with site elements. Exterior finish materials shall primarily be stucco, manufactured stone, architectural cast stone, glass, and brick, in varying combinations. Accents to exterior walls shall include, but shall not be limited to, decorative metal beams, grilles, panels, and trellises (See photos 4 through 7 and Exhibits A-6.1 through A-6.7).

Photos 4 and 5—Example images of architectural detail

Roofs

Roof lines shall be broken to avoid continuous roof lines longer than 150 feet. A common theme of shed roofs and sloped roofs, utilizing a variety of materials as indicated in Exhibits A-6.1 through A-6.7, will be interspersed with parapet screen walls and lower sloped roof sections. Shed roofs and parapet walls shall be designed to screen mechanical equipment mounted on lower sloped roof sections, allowing mechanical equipment to be completely screened from ground level views. Parapet walls of varying heights shall return toward the interior of the building to provide the impression of substantial building depth, avoiding the appearance of two-dimensional facades. Generally, exposed roof drains and downspouts shall not be included in the development. Any exceptions to this will be integrated architecturally with the design of the building.

Photos 6 and 7 – Example images of architectural detail and variances in roof lines

Color
Color will be an integral part of the overall building articulation strategy for the development. Generally, lighter, more neutral colors will be the predominate color, with accent areas of bolder, more saturated colors. Trim, signage, and pedestrian oriented amenities will also tend to have more intense colors. Color families will generally be limited in any one area of the project. The colors proposed are similar to those found in commercial development in the surrounding area (see Exhibits A-5.1 thru A-5.33 and A-6.1 thru A6.7).

Frontage on Park and Public Right-of-way
Buildings shall provide architectural articulation on facades facing the public right-of-way and Sparks Marina. Restaurants along the waterfront will provide attractive, interesting architectural detail on all facades, including the lakeside frontage and outdoor dining areas (see photo 8). Other buildings along that frontage or a public right-of-way shall have architectural articulation and will provide screening for any service or loading areas or mechanical/electrical equipment on all facades facing the right-of-way or lakefront.

Photo 8—Example Outdoor Dining

Architectural Images
Architectural images developed for this project are included in Exhibits A-5.1 through A-5.33 and serve to delineate the general architectural style and typical materials intended for the development. These images are conceptual in nature and are subject to change, but are representative of the style, quality, and character intended for the development.

VI. Exterior Mechanical, Electrical Equipment, Services Areas, and Trash Enclosure Screening

Screening
All screening of mechanical and electrical equipment, service areas, and trash enclosures shall meet the applicable local codes and City of Sparks design standards. Chapter 2, Section II details additional screening requirements associated with specific uses noted in that section.

Trash/Loading/Storage Areas
All trash areas in the project shall be enclosed with permanent, high quality architectural enclosures. Architectural enclosures shall be integrated into the architectural design of the buildings. The architectural screens shall include landscaping with varied heights, colors and species. Where gates are necessary (at smaller enclosures), the gates shall be constructed of durable materials, typically painted steel, with 80% or greater opacity. Durable pavement sections shall be provided as necessary to prevent damage from trash trucks. Refer to Photo 9. Enclosures shall comply with the following:

1. All trash enclosures shall consist of masonry walls which complement the architecture of the building(s). Enclosures are to be at least as tall as the trash dumpster(s) or compactor(s) being enclosed.
2. Trash enclosures shall be screened by landscaping such that there is 90 percent screening of walls within 3 years. Plant types shall be primarily evergreen, the same size as the overall landscape, with closer spacing. Planter areas shall have a minimum dimension of 4 feet in width on all sides of trash enclosures which do not contain access doors. Access doors shall be 90% solid and be architecturally compatible with the building and enclosure design (no chain link fencing allowed).
3. Loading docks shall be no less than 90% screened from direct view from any point off of the site at a height of 5' (eye level) by landscaping and/or architectural enclosures. They shall be screened from view onsite in the same manner to the extent practical.

There shall be no outdoor storage. All seasonal merchandise, overstocked merchandise and/or nursery stock shall be stored in an enclosed area and not on the parking lot or in front of the store areas. There shall be no outdoor storage of pallets, strapping, boxes, or other materials.

The developer shall obtain a "will serve" letter from Waste Management/Sparks Sanitation prior to the issuance of a building permit for the project.

Loading docks and service areas including trash compaction and/or dumpster(s) shall be substantially screened from view utilizing landscaping or screen walls. The loading dock screening shall be high enough to screen the trucks. The screening shall be reviewed and approved by the Administrator prior to the issuance of a building permit.

Trash Enclosure requirements
The developer shall locate and install trash enclosures to the approval of disposal services and the administrator including the appropriate size and location. The trash enclosures shall match the project's architecture and building materials to the approval of the Administrator prior to issuance of the certificate of occupancy for the individual buildings. The trash enclosures shall be sized to include sufficient space for rendering vats where necessary for food service/restaurants by the Public Works Director.

Photo 9—Typical Trash/Service area screen

Utility and Mechanical Equipment Screening (DSM 3.A.5)
Utility equipment will typically be located in the same trash and loading areas noted above, and therefore will be screened from view. Any equipment which needs to be located outside of the larger service areas (a transformer for a pad site restaurant, for instance), shall be screened from view using landscaping, architectural screening (walls), or both, while allowing for the necessary access required by utility companies.

All utilities within the site shall be routed underground.

All mechanical equipment located on the ground shall be screened in the same manner as noted above for utility equipment. All mechanical equipment located on the roofs of buildings shall be screened by roofs or parapets, as noted above.

The mechanical equipment shall be screened from view of the parking lot, adjacent public streets, and public areas by using an architectural element of the building consisting of same materials and be durable materials to the approval of the Administrator.

The developer shall work in conjunction with the utility companies to locate utility equipment such as electrical, gas and water junction boxes, including transformer boxes, vaults and electrical panels, in areas where the equipment is screened by either architectural elements that match the main structure's colors and materials or by adjusting the approved landscaping to compensate to the approval of the Administrator prior to the issuance of a certificate of occupancy and the site plan review permit for each individual building.

VII. Landscape Architecture

General Site Landscape Requirements

1. Landscaping for items specifically not addressed in the Planned Development Handbook standards shall comply with the Resource Efficient Landscaping Standards (Chapter 20.32), the Non-Residential Development Design Standards, and Planned Development Review (Chapter 20.18) for the City of Sparks. In the case of conflicting standards, the Planned Development Handbook standards shall apply.

2. A minimum of 20 percent of the site area shall include landscaping. This area includes the right-of-way areas along East Lincoln Way, Sparks Boulevard, and I-80. Of that total, up to 10 percent of the landscape can be non-living ground covers or decorative paving outside of the main shopping plaza area. Non-living ground covers shall be a combination of 4" river rock, decomposed granite, or bark mulch (min. 4" depth). Boulders may also be incorporated into the design. Water features, including the lake, may be counted in the landscaped area per Sparks Municipal Code as 'other amenities'. All areas of landscaping, water features, decks, boardwalks, and decorative paving in the main shopping plaza area may be counted as 'landscaped area.' If the baseball stadium is not developed as part of the project, and additional retail and outdoor plaza area is developed instead, a minimum of 50% of the plaza area shall meet this standard as counting toward the overall minimum of 20% of the site to be 'landscaped area'. If the baseball stadium is developed as part of the project, 100% of the plaza, including both landscape and hardscape areas, shall be permitted to count toward the overall minimum of 20% of the site to be 'landscaped area'. Parking structures provided under an approved SUP shall provide landscape planters or "benches", integrated in the parking structure's elevated decks, that shall count toward the overall landscape coverage requirement. Refer to Exhibits A-7, A-7.1 and A-7.2 for the landscape concept plan for the development.

3. The plant material types were selected from Table 3 – Suggested Plant Materials, page 2.114 of the obsolete Marina Area Plan. Specific plant types, including additional plant materials under consideration for possible use in the project, are listed at the end of this section.

4. All irrigation shall be automatic drip/spray, with a dual program controller and wind shut-off. All plants shall be grouped into similar water zones.

5. The overall tree count shall be a minimum of 1 tree per 500 s.f. of required landscape, plus parking lot standards. The overall shrub counts in all areas shall be a minimum of 6 shrubs per each tree required. Low ground covers below shrubs shall be used to achieve 90% coverage of mulched bed area over three years. No bare dirt areas shall be allowed.

6. The conceptual landscaping for the development shall be reviewed by zone during the site plan review process, approved by the administrator prior to the issuance of a site

improvement permit for that zone, and installed per the approved plans prior to the issuance of the certificate of occupancy for any building within that zone.

7. The entire perimeter landscaping and irrigation shall be installed per the approved plans prior to issuance of the certificate of occupancy for the first building constructed.

Minor Entry Driveways at Lincoln Way and Sparks Boulevard

1. The entry drives off of Lincoln Way and Sparks Boulevard shall include sidewalks on one side, located within a minimum 10-foot width landscape area. The entry drives shall include a minimum 7-foot width paved crosswalk at the public sidewalks. Refer to Exhibits A-7.1 and A-7.2

Major Entry Areas (at round-a-bouts) of Lincoln Way

1. Pedestrian entry into the project from Lincoln Way is illustrated in Exhibit A-8.4. The 5-foot minimum width sidewalk at the street shall be widened at the corner locations adjacent to entry signs to a maximum of 25 feet. The two main entry drives off of East Lincoln Way shall include sidewalks to provide a pedestrian connection to the buildings, configured generally as shown in Exhibits A-7.1 and A-7.2. The sidewalks shall be located within a minimum 15' wide landscaped parkway. Where combined with the bikeway, the sidewalk shall be widened to meet the standards outlined in the AASHTO Handbook. The entry drives shall include a minimum 7' width decorative paving crosswalk at the public sidewalks. Decorative paving crosswalks shall be included in major pedestrian circulation areas within the parking area as well. Behind the site identity signage vertical evergreen trees form a backdrop and accentuate the corners of each round-a-bout. At each corner in front of the signage low perennials and boulder groupings are proposed. Visibility triangles at the intersections shall be maintained. Medium height (3-4 foot) shrubs on top of 24-inch tall berming in back of each sign will enhance the backdrop for the sign and help screen car parking from the intersection.

Photo 10—Typical entry drive with landscape buffer and sidewalk.

2. The Lincoln Way medians shall consist of medium height (3-4 foot) ground covers/shrubs, with vertical upright trees (2 inch caliper) at 25 feet on center. At end

islands a low ground cover (12-18 inches tall) is proposed for sight visibility within the round-a-bout. The use of ground covers, not to include turf, below shrubs shall be utilized to achieve 90% coverage over the mulched ground surfaces within 3 years.

Photo 11—Public Street Median

3. The center medians at the round-a-bouts shall include tall vertical accent trees (3 inch caliper) at the center. At the median center, berming is proposed (3-feet tall) with medium height (3-4 foot) shrubs, and low ground-cover, including turf. Around the perimeter a 20-foot wide band of low shrubs/perennials are proposed to meet local and regional standards for sight distance within round-a-bouts. Other hardscape features such as water features or sculpture may also be included.

Photo 12 – Roundabout Landscaping

4. At the north and south driveway medians off of Lincoln Way, small accent trees (2 inch caliper) at 20 feet on center and medium height ground covers are proposed. Again, end islands will include low ground covers and perennials.

Photo 13—Entry Median

5. The overall planting palette will be fairly simple in terms of plant varieties used and will be planted close together at corner locations and focal points to provide maximum impact. Larger trees and shrubs required shall be located at entry points and along pedestrian corridors.

Lincoln Way/Sparks Boulevard Corner Location

1. Exhibit A-8.2 illustrates the corner at the Sparks Boulevard project entry. A 20 to 25-foot wide decorative concrete paving area is proposed on the front side of the signage to match those at the round-a-bouts. Evergreen trees and medium to tall shrubs are proposed behind the signage as a backdrop.

2. Accent trees, either tall vertical or lower flowering trees are proposed on either side of the sign to frame views to the sign from motorists. Along both streets 18 to 24-inch tall berming is required. A combination of low ground covers and low to medium height shrubs are proposed to screen cars, while allowing views to the retail building facades beyond.

3. The southwest corner of Lincoln Way and Sparks Boulevard will not be covered by this additional landscaping, as indicated in Exhibit A-7.1, due to the properties not being included in the development.

Lincoln Way/Sparks Boulevard Streetscape

1. Concrete sidewalks shall be a minimum of 5 feet in width. Where combined with the bikeway, the sidewalk shall be widened to to meet the standards outlined in the AASHTO Handbook. Berming shall be incorporated into the landscape area at the parking side of the sidewalk. Berming shall vary between 18 and 24 inches in height.

Photo 14—Streetscape Berm and Plantings

2. Ground covers and shrubs shall be a combination of deciduous and evergreen types, with a mature height of 12 to 18 inches. A minimum of 60 percent of the shrubs shall be planted at a #5 size and 40 percent at a #1 size. Plant types to be repeated throughout the landscape and will be used in large massing for simplicity. The use of low ground covers below shrubs shall be utilized to achieve 90% coverage over the mulched ground surfaces within 3 years.

3. Non-living ground covers shall be a combination of river rock, decomposed granite, or bark mulch. Boulders may also be incorporated into the berming. Non-living ground covers shall be a minimum depth of 4 inches.

Photo 15—Low shrubs and rock ground cover

4. Trees shall be planted at a rate of 1 tree per 40 lineal feet of street frontage, grouped with secondary trees to provide visibility into the center. The predominant tree at the street edge will be Red Oak, 3-inch caliper size. Secondary trees shall be a mix of small and medium deciduous 3-inch caliper size trees grouped with the street tree placements, and planted at an overall rate of 1 tree per 60 lineal feet of street frontage, with clusters of

trees permitted to allow for visibility of the retail tenants in the development and to enhance parking lot security.

5. Lawn area will be allowed between the sidewalk and parking lot, with a minimum lawn width of 10 feet. Grading shall accommodate swales at bottom of slope to avoid run-off onto sidewalk areas.

Photo 16—Typical Berm and Lawn Area

6. Adjacent to the service areas along Sparks Boulevard, evergreen trees shall be planted at an overall rate of 1 tree per 25 lineal feet of site perimeter, with grouping permitted to allow visibility into the center while providing effective screening of the service areas. The trees shall be a mix of 50 percent 6-foot tall and 50 percent 8-foot tall. If grading allows berming shall be added, with all slopes 3:1 or flatter covered with mulch. Medium (3-4 foot) to tall (4-6 foot) shrubs shall be used in all areas adjacent to service and loading as an understory to the evergreen trees.

7. The existing trees within the Sparks Boulevard right-of-way shall be retained where possible, except where re-grading and new lane additions require removal.

8. The Sparks Boulevard entries will be similar in design to the corner location illustrated in Exhibit A8-2. A 5-foot wide sidewalk shall extend into the center along the north side of the driveway.

9. A 35' wide landscaped corridor shall be provided along the entire length of the Sparks Boulevard frontage, except where the existing gas station, located on a parcel outside of this development, is located. The landscape width at each side of Lincoln Way is generally 22'; the width is narrower at the existing fitness center and gas station, where a right turn lane is required at eastbound East Lincoln Way at Sparks Boulevard. See Exhibits A-7.1, A-7.2, A-8.2 and A-8.4.

I-80/South Property Perimeter

1. The landscaping along the I-80 freeway corridor at the south property line shall be a minimum of 10 feet wide.

2. Trees shall be a combination of deciduous and evergreen (30-70% mix), planted at the outside edge of the 10-foot landscape strip at an average of 1 tree per 25 lineal feet, per the Design Standards Manual. The trees shall be grouped in clusters, primarily at the highway entrance ramp frontage. The intent is to allow visibility to the main shopping center from the freeway. Evergreens shall be a minimum 8-foot tall along this property line, grouped together in clusters. Primary deciduous trees shall be Red Oaks.

3. Shrubs shall be a combination of low growing ground covers at the curb edge and tall (4-6 feet tall) shrubs at the fence line.

4. Shrubs and tree types shall be consistent with the existing tree plantings in the NDOT right-of-way.

5. As grading allows, as much berming as possible shall be added to the perimeter planters. (In some areas the planters may be a cut or fill slope to match existing grade.) The maximum berm height will be approximately 30 inches at 3:1 slope. See Exhibits A-7.2 and A-8.6.

Parking Lot Landscaping

1. A mix of deciduous (80%) and evergreen (20%) trees shall be provided in the parking lot.

2. Tree types shall be organized into the following types:
   a) Ornamental upright trees at drive aisles with pedestrian pathways.
   b) Shade trees in freestanding planters among the car stalls.
   c) Evergreen trees in larger planters, with berming to visually reduce the size of the parking lot.

3. Parking lot islands among the parking stalls shall be a minimum size of 70 s.f. and shall be planted in diamond shaped islands with 9' sides and 9' across the smaller internal dimension. A minimum of 8 feet clear landscaping width shall be provided for free-standing planters at the ends of parking rows. All islands shall include a 6-inch curb within parking areas. Diamond shaped islands shall be placed at a rate of 1 island for every eight spaces in the center areas of parking rows. Ends of rows may include up to 8 spaces between end-cap islands and the first diamond islands. Each island shall include one tree, as well as additional plantings. Refer to Exhibit A-8.3.

4. Deciduous trees shall be 50 percent 1.5 inch caliper and 50 percent 2 inch caliper size. Evergreens shall be 50 percent 6-foot height and 50 percent 8-foot height. Accent trees shall be clustered around driveway entries and paved areas. Trees shall be planted at an overall rate of 1 tree per each 7 parking spaces (parking spaces for employees and customers only—no inventory storage spaces to be included). The remaining trees shall be in planters at the end of rows and at perimeter areas surrounding the parking lot.

5. Within the parking lot, shrubs shall be a maximum of 30 inches tall for visibility. Plants along drive aisles and pedestrian pathways may be taller.

6. Larger planters shall include berming of 6 to 12 inches tall for interest and drainage.

7. The use of low ground covers below shrubs shall be utilized to achieve 90% coverage over the mulched ground surfaces within 3 years.

8. Non-living ground covers shall be a combination of river rock, decomposed granite, bark mulch, and boulders. Non-living ground covers shall be a minimum of 4 inches in depth. Lawn areas may be allowed in larger planter areas with a minimum width of 10 feet. All lawn shall include a mow strip at the perimeter and a swale at the low end adjacent to paving for water infiltration.

Photo 17—Perimeter landscaping at parking area

Photo 18 – Perimeter landscaping at parking area

Freestanding Building Pads and Perimeter of Main Retail Center

1. Along building frontages landscaping shall consist of small to medium ornamental trees, a minimum of 3-inch caliper. Trees shall be deciduous, either multi-branching or upright types for interest. Evergreens with a minimum of 6-foot height may also be used.

2. Shrubs shall be a mix of evergreen and deciduous, with 70 percent #5 size and 30 percent #1 size. Shrubs shall be selected for visual interest, with use of ornamental grasses and perennials.

3. Berming up to 12 inches tall and boulders of a minimum dimension of 30 inches shall be used to make plantings more visible and blend with the building's rock veneer.

4. The use of low ground covers below shrubs shall be utilized to achieve 90% coverage over the mulched ground surfaces within 3 years.

5. The minimum planter dimension shall be 5 feet in width.

6. Freestanding planters shall be added along building walls and as a spatial definition around specialty use areas such as restaurants.

7. Site amenities shall include bicycle racks, benches, trellis covers, trash receptacles and ash urns, all grouped together in an organized fashion.

8. Non-living ground covers shall be a combination of river rock, decomposed granite, bark mulch, and boulders. Non-living ground coveres shall be a minimum of 4 inches in depth. Lawn areas may be allowed in larger planter areas with a minimum width of 10 feet. All lawn to include a mow strip at the perimeter and a swale at the low end adjacent to paving for water infiltration.

9. A minimum of 50 percent of each building frontage must include landscaping as defined in this section. If this standard cannot be met, the minimum is 30% for any given building frontage with a cumulative minimum total of 60%.

Main Shopping Plaza

1. The landscaping at the main shopping plaza is illustrated in Exhibits A-8.1, A-8.3 and A-8.5. These areas are the highlight of the development and will feature extensive landscaping, pedestrian amenities, and integrated water features.

Photos 19 and 20—Example shopping plaza landscaping and pedestrian amenities

2. The central theme to the plaza is a series of fountains, with a sight-line terminus at the west end to the lake. Between fountains are landscaped planters at the center of the plaza. Plantings within these planter islands shall be a combination of deciduous and evergreen ground covers, vines, shrubs, and trees. Shrub sizes shall be a combination of #1 and #5

plants, depending on species. The intent is for complete ground coverage within 3 to 5 years. Trees shall be a minimum 3-inch caliper size, with larger trees used in focal areas. Section A-A shown on Exhibit A-8.1 illustrates the intent of the plantings and water features. The planters will accept some stormwater flows and will require overflow piping to the storm drains.

3. Planters at buildings will be added along building frontages without storefront type windows, such as restaurants and services areas (see Exhibit A-8.5). Berming of 2-3 feet will be included for visual interest. There may be areas of lawn within the plaza, with appropriate berming and swales for drainage.

4. Specialty paving will be used throughout the plaza, including, but not limited to, masonry pavers, colored concrete, textured/stamped/colored concrete and stone. A 6-inch concrete curb will be placed around all planter areas for drainage, planter protection, and pedestrian circulation. Colors will be earth tones with color accents for architectural compatibility.

5. Site amenities proposed include bike racks, benches, tables, trash receptacles, ash urns, and freestanding planters. Freestanding planters will include perennials and annuals that are changed seasonally. Boulders inset into concrete/planters will offer additional seating, and some planter designs will offer other forms of integrated seating. Overhead trellises, tent structures, and other sun-screening architectural treatments will be detailed for pedestrian shade.

6. Non-living ground covers shall be a combination of river rock, decomposed granite, bark mulch, and boulders. Non-living ground covers shall be a minimum of 4 inches in depth.

Trash Enclosures and Loading Areas

1. Trash enclosures shall be screened with a combination of evergreen and deciduous plant material, including trees, shrubs, and vines. Planters shall be a minimum width of 5-feet wide.

2. Non-living ground covers shall be a combination of river rock, decomposed granite, bark mulch, and boulders. Non-living ground covers shall be a minimum of 4 inches in depth.

3. Loading areas shall be screened with the use of evergreen trees at the wall perimeter(s) and berming in areas with adequate space. Shrubs shall be 80 percent evergreen and 20 percent deciduous, primarily medium to tall in height. Evergreen tree selection and placement shall be sensitive to icing issues around loading areas during winter months. All trash and loading areas shall be screened such that there is 90 percent screening of walls and truck docks within 3 years.

Landscaping submittals and installation

The developer shall submit landscaping and irrigation plans for the project for a site improvement permit. Landscape design shall be in conformance with the standards noted above for review and approval by the administrator prior to issuance of a building permit for the project.

The landscaping and irrigation of the entire development perimeter, I-80 frontage, Sparks Boulevard, and East Lincoln Way shall be installed per the approved plans prior to issuance of the certificate of occupancy for the first building constructed, per the approval of the Administrator. Plans for such landscaping shall reflect the requirements of the standards noted above. Building sites not immediately constructed shall be revegetated with grasses, ground covers, and/or wildflowers and temporary irrigation shall be provided and maintained, including dust control, until construction begins on those sites.

Parking lot landscaping and irrigation shall be installed per the approved plans, including providing planter islands as noted above. Parking lot landscaping shall be installed within a site zone, per the approval of the Administrator, prior to the issuance of a certificate of occupancy for the first building within such zone.

The landscaping for each building pad shall be reviewed during the building permit review process, approved by the administrator prior to the issuance of a building permit for that site, and installed per the approved plans prior to the issuance of the certificate of occupancy for that building.

Proposed Plant Materials that may be included in the Project

<u>Shade Trees</u>

Acer Rubrum	Red Maple
Acer saccharum	Sugar Maple
Betula nigra	River Birch
Carpinus betulus 'Fastigiata'	Columnar Hornbeam
Fraxinus Americana 'Autumn Purple'	Autumn Purple Ash
Gleditsia triacanthos 'iermis'	Honeylocust
Platanus x acerifolia 'Yarwood'	Yarwood London Planetree
Quercus Rubra	Red Oak

<u>Ornamental Trees</u>

Amelanchier spp	Serviceberry
Cercis occidentalis	Western Redbud
Celtis reticulate	Netleaf Hackberry
Cornus florida	Flowering Dogwood
Fraxinus oxycarpa 'Raywood'	Raywood Ash
Koelreuterua paniculata	Goldenrain Tree

Malus spp.	Crabapple

Evergreen Trees

Pinus aristata	Bristlecone Pine
Pinus flexilis 'Vanderwolf'	Vanderwolf Pine
Pinus monophylla	Singleleaf Pinon Pine
Pinus sylvestris	Scotch Pine

Shrubs

Berberis thunbergii	Japanese Barberry
Berberis mentorensis	Mentor Barberry
Buddleia davidii	Butterfly Bush, Orange-eye
Buxus microphylla var. koreana "Wintergreen"	Wintergreen Boxwood
Calamagrostis sp.	Feather Reed Grass
Cotoneaster acultifolius	Peking Cotoneaster
Cotoneaster divaricatus	Spreading Cotoneaster
Cornus stolonifera	Redtwig Dogwood
Itea virginica 'Little Henry'	Little Henry Sweetspire
Juniperus chinesis "Armstrong"	Armstrong Globe Juniper
Juniperus chinenesis 'Sea Green'	Sea Green Juniper
Juniperus horizontalis 'Hughes'	Hughes Juniper
Juniperus Sabina 'Broadmoor'	Broadmoor Juniper
Lonicera Japonica "halliana"	Halls' Honeysuckle
Mahonia aquifolium	Oregon grapeholly
Miscanthus sinsensis	Maiden Grass
Photinia serrulata	Chinese Photinia
Pinus Mugo	Mugo Pine
Potentialla verna "nana"	Dwarf Potentilla
Rosa rugosa	Rose
Rosa 'Knockout'	Knockout Shrub Rose
Spiraea bumalda 'Anthony Waterer'	Anthony Waterer Spiraea
Syringa petula 'Miss Kim'	Miss Kim Lilac
Taxus cuspidate 'Capitata'	Dense Yew
Taxus x media 'Hicksii'	Hick Columnar Yew
Viburnum spp.	Viburnum
Yucca filamentosa	Adamsneedle Yucca

Ground Covers/Grasses/Perennials

Achillea tomentosa	Wooly Yarrow
Artemisia schmidtiana	Silver Mound
Coreopsis spp.	Coreopsis
Cotoneaster "Lowfast"	Cotoneaster
Echinacea purpurea	Purple Coneflower
Festuca ovina "Glauca"	Blue Fescue
Gaillardia x grandiflora	Blanket Flower
Hemerocallis spp.	Daylillies
Heuchera spp.	Coral Bells
Juniperus horizontalis	Bar Harbor Juniper
Lavanula sp.	Lavendar
Lonicera japonica	Honeysuckle
Mahonia repens	Creeping Mahonia
Miscanthus sinensis purpurascens	Purple Maiden Grass
Parthenocissus quinquefolia	Virginia Creeper
Pennisetum alopecuroides 'Hamelin'	Dwarf Fountain Grass
Rudbeckia fulgida speciosa 'Glodstrum'	Goldsturm Black-Eye Susan
Santolina chamaecyparissus	Lavender Cotton
Schizachyrium scoparium	Little Bluestem
Sedum x "Autumn Joy"	Autumn Joy Sedum
Thmus vulgaris	Common Thyme
Vinca minor	Periwinkle

VIII. Signs

Definitions

Additional signs / graphics: Any sign or graphic display not otherwise defined by these requirements. Additional signs / graphics may include, but are not limited to, non-commercial graphic displays, regulatory signs, and directional signs.

Area: Sign area is defined as the actual area of the sign copy or graphic, not including supporting structures or background elements. Where animated video reader boards are permitted as part of a sign, the entire area of the animated video reader board shall be counted as sign area.

Awning Sign: A framed fabric assembly mounted over storefront windows to provide shade, which may be adorned with sign graphics related to the tenant occupying the store, or other graphics.

Building Sign: Any sign attached to a building.

Blade Sign: Relatively small, two-sided sign projecting outward from a building façade, usually at a 90-degree angle to the façade.

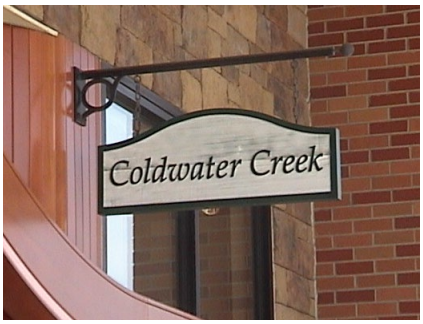


Photo 21 - Example of Blade Sign

Center-Identification Monument Sign: Independently mounted signs that solely identify the Legends at Sparks Marina Development at specific locations identified on the site plan, Exhibits A-4, A-4.1 and A-4.2.

Photo 22 – Example of Center-Identification Monument Sign

Double-stacked Letters: Two rows of text comprising a single sign, wherein the second row is an integral part of the Tenant name identified by the sign.

Façade Sign: Sign mounted directly to a building façade, parallel to the plane of the building façade.

Free-standing Sign: Independently mounted signs that exceed 6 feet in height above the grade, that identify the Legends at Sparks Marina Development and significant tenants in combination.

Marquee Sign: An assembly supporting sign graphics or lettering that projects from a building façade and provides sign visibility from angles other than 90-degrees to the building façade.

Monument Sign: Independently mounted signs associated with specific retail tenants that do not exceed 6 feet in height above the grade.

Non-tenant-identification graphic panel: A panel which conveys general graphic artwork or information related to the development as a whole, not related to any single specific tenant. (See "Photo 23.")

Primary Sign: The sign which identifies the name of the store or tenant at the primary storefront entry to the store.

Projecting Sign: Signs incorporating a three-dimensional form, which can be sculptural or representational in nature, and generally of a larger size than blade-signs.

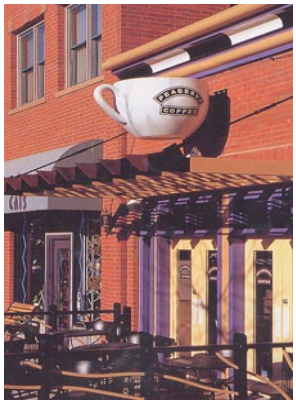


Photo 22 - Example of Projecting Sign

Over-door Transom Sign: A sign located immediately over a storefront entry door and applied using adhesive decal lettering directly to the inside surface of a glazed transom panel.

Rooftop Sign: A sign or graphic mounted on a framework anchored to and located above a building roof.
Storefront safety glazing decals: Decals applied to the interior of storefront safety glazing, conveying additional information about the store not conveyed by the primary Tenant sign (such as store operating hours).

Tag Line: An additional line of text in a sign, wherein the additional line provides further clarification or explanation of the primary Tenant or store name, though legally a part of the store operator identification.

Wall Sign: A sign mounted directly to a wall. Wall signs include façade signs, projecting signs, and marquee signs. Blade signs are considered, for the purposes of these standards, to be a separate category of sign.

Regulation
Sign designs shall be submitted 1) for design approval under these requirements; 2) for permit per ordinance. Multiple signs may be covered under one design approval submittal and under one permit providing full details of all signs covered are submitted for review and approval.

The Master Developer shall regulate the design of signs under the requirements of these standards prior to regulatory review by the City. All Signage is to be submitted as a complete package for review and approval. Incomplete submittals will not be approved. The tenant shall provide a copy of the Master Developer's written approval to the City of Sparks as part of the regulatory review package. All signs, except signs within the pedestrian plaza of the main retail

center (as designated in Exhibit A-10), are subject to the design approval of the Master Developer or its Architect, and of the City of Sparks. Signs within the pedestrian plaza of the main retail center (as designated in Exhiit A-10) are only subject to the design approval of the Master Developer or its Architect.

All signs must be shown to scale on the approved storefront elevation. Submittal requirements for regulatory review of sign design by the Master Developer and the City shall include the following by sign type:

1. Wall Signs (Façade sign, projecting, or marquee sign): Provide a storefront elevation of each sign proposed as well as detailed shop drawing elevation and section through sign indicating sign makeup, method of illumination, dimensions, materials and colors.
2. Over-door transom sign: Provide a storefront elevation of each sign proposed as well as detailed shop drawing elevation indicating sign dimensions, materials and colors.
3. Storefront safety glazing decals: Provide a storefront elevation of each sign proposed as well as detailed shop drawing elevation indicating dimensions, materials and colors.
4. Blade sign: Provide a storefront elevation and section of each proposed sign indicating mounting location and height. Provide a detailed shop drawing section and elevation indicating sign makeup, method of illumination, dimensions, materials and colors for sign and decorative bracket.
5. Additional signs / graphics inside stores within four feet of the storefront: Provide a storefront elevation, plan, and or section indicating any additional proposed signage as well as detailed shop drawings indicating sign makeup, dimensions, materials and colors. All such additional signage shall be reviewed on an individual basis.
6. Rooftop Signs: Provide an elevation of each sign proposed as well as detailed shop drawing elevations and sections indicating sign makeup, method of illumination, dimensions, materials, colors, and means of structural support. Include structural calculations if support is not included as part of the building design to which the sign is affixed.
7. Monument Signs: Provide an elevation of each side and each end of each sign proposed, as well as detailed shop drawing elevations and sections indicating sign makeup, method of illumination, dimensions, materials, colors, and means of structural support. Include structural calculations of means of structural support.
8. Free-Standing Signs: Provide an elevation of each side and each end of each sign proposed, as well as detailed shop drawing elevations and sections indicating sign makeup, method of illumination, dimensions, materials, colors, and means of structural support. Include structural calculations of means of structural support.

All additional signage shall be submitted to the Master Developer or the Master Developer's Coordinating Architect for approval. Any minor deviations to these criteria will be reviewed on an individual basis and subject to the approval of the Master Developer and the Administrator.

Sign Classification
Signs shall be classified according to the tenant types with which they are associated:

1. Small Shop Retail Tenant: Leaseable area 0 - 14,999 square feet
2. Sub-Major Retail Tenant: Leaseable area 15,000 – 119,999 square feet
3. Major Retail Tenant: Leaseable area 120,000 square feet and above
4. Out-Parcel (Single) Tenant: Stand-alone structures 15,000 square feet and below comprised of a single tenant.
5. Casino / Hotel
6. Movie Theater
7. Baseball Stadium

General Sign Requirements
All signs shall be made up of individual illuminated letters or nationally branded shapes; conventional box (cabinet) signs will not be approved. Box signs with raised, ~~or~~ individual letters, or nationally branded shapes will be considered on an individual basis. All sign returns shall either match face color of sign or blend with adjacent building color. All metal letters shall be fabricated using fully-welded construction, with all welds ground smooth so as not to be visible. Visible fasteners will not be permitted. Acrycap or trimcap retainers used at the perimeter of sign letter faces shall match in color and finish the face or the sides of the sign. Threaded rods or anchor bolts shall be used to mount sign letters standing off of the building face. Angle clips attached to letter sides will not be permitted. All mounting attachments shall be sleeved and painted.

Minimum height of all signage shall not be less than 80 percent of the maximum allowable letter height without prior written approval. Signs shall be readable against the designed background of the architectural façade of the building and shall not require a secondary backer panel to make the lettering or graphics read.

Lettering on all store signs shall be limited to business or trade name of the premises as it appears on the lease. Logo signs will be reviewed on an individual basis.

Manufacturers' labels, underwriters' labels, clips, brackets, or any other form of extraneous advertising attachment or lighting devices shall be fully concealed from public view. Labels installed on sign returns are not permitted.

Except as provided herein, no advertising placards, banners, pennants, names, insignia, trademarks, or other descriptive materials shall be affixed or maintained upon the glass panes

and supports of the storefront windows and doors, or anywhere inside the store within four feet of the storefront, without prior written approval of the Master Developer.

At no time will hand-lettered, non-professional signs, or newspaper advertisements be displayed on the storefronts or inside stores within four feet of the storefront. Decals or other signs indicating products lines or credit card acceptability shall not be permitted on the storefront glazing or anywhere inside the store within four feet of the storefront, other than stores operating hours.

No exposed lamps or tubing will be permitted. Neon tubing or other exposed light sources will be reviewed on an individual basis. No exposed raceways, crossovers or conduits will be permitted. All cabinets, conductors, transformers and other equipment shall be concealed from public areas. All electric signs and installation methods must meet Underwriter's Laboratories (UL) test criteria and contain a UL label. Tenants are required to provide a concealed access panel from within the Tenant's leasable area, if applicable, to service and install exterior building signage

Either direct or indirect lighting of signs is required and shall be at hours as required by Master Developer. The use of time clocks for sign and show window lighting is required and should be adjusted and coordinated with the shopping center's normal business hours.

Signs not Permitted

The following types of signs shall not be permitted:

- Signs such as die cut vinyl, gold or silver leaf, or paint.
- Boxed pillow or cabinet type (except as otherwise noted herein).
- Formed plastic or injection molded plastic signs.
- Temporary tenant banners or pennants.
- Secondary signs (window sign or sign plate indicating name of shop or good sold) in addition to primary signs, except as otherwise permitted herein.
- Cloth, paper, cardboard and similar stickers or decals around or on surfaces on the storefront, or inside the store within four feet of the storefront, without prior written approval.
- Noise making, except as otherwise permitted herein.
- Additional signs of any kind inside stores within four feet of storefront windows.

- Awning signs, with the exception of awning signs provided in lieu of an over-door transom sign with the explicit approval of the Master Developer.

Special Sign Provisions
No illuminated sign may face toward the existing adjacent residential area at the northwest corner of the development when located within 300 feet of the development's west property line; except that signs located in the main retail center may face a direction ranging from directly west to 45 degrees north of west only. Nor may such sign face directly north toward existing adjacent residential area at the northeast corner of the development, unless located at least 300' from the nearest off-site residential structure.

Signs which produce odor, sound, smoke, flame or other emissions shall be allowable if approved by the Master Developer and the City. Proper safety precautions shall be taken for any such signs and shall be approved by the City of Sparks.

The Developer may locate one building, tower or beacon feature signs communicating the development name at a maximum height of 150', within the development courtyard, that will be seen from a distance on adjacent local roadways. Such features may include up to six components identifying select tenants of the development.
Beacons may be an allowable light source provided they are not directed at adjoining properties or roadways. No more than (4) individual sources shall be allowed in the development, with a maximum candlepower of 300 million each. Such light sources may move but may not flash or blink. Such light sources must be approved by the FAA if applicable.

Development graphic banners may be incorporated into the design of roadway light standards within the Development along Sparks Boulevard and East Lincoln Way, but not located within the public right-of-way. The size of such banners shall be limited to 20 square feet.

Roof signs shall be allowable within the area of the main retail center.

Non-tenant-identification graphic panels or window graphics shall be allowable within the area of the main retail center. Such panels shall be limited to a maximum of thirty percent (30%) of the area of the building façade on which they are located.

"Photo 23" - Example of non-tenant-identification graphic panels and roof sign
(utilized at The Legends at Village West Development)

Sign Standards by Tenant Type

Sign provisions shall be applied based on the following building parameters for each tenant type:

1. Small Shop Tenant Signs

   Wall Signs: Wall signs may consist of either façade signs, projecting signs, or marquee signs. Wall signs are required for each Tenant and shall be installed within sign areas indicated on the building drawings. The maximum average height for letters shall be 30 inches. A maximum of one wall sign per storefront with a maximum of two per tenant shall be permitted. One additional wall sign may be considered for "endcap" tenants subject to approved of the Master Developer. Façade signs shall not extend more than 8 inches beyond the face of the surface to which the sign is mounted. Façade signs shall be individually illuminated channel letter with colored translucent face, or reverse- channel, halo-lit illuminated individual letters mounted to the building face. One additional 8-inch over-door transom sign is allowed per storefront. Direct illuminated signs and double-stacked lettering shall be considered for approval by the Master Developer but shall be reviewed on an individual basis. Double stacked letters shall be a maximum 24" high individual letters and shall comfortably fit within the designated sign area of the storefront as determined by the Landlord or the Landlord's Representative. Tag lines shall be allowed on an individual basis only and are subject to Landlord approval. Any allowable tag lines shall be individual illuminated letters (no box signs) and shall not exceed 10" in height. The width of the tag line, if approved, shall not exceed the width established for the primary signage.

   Marquee Signs: One marquee sign shall be permitted per storefront (with a maximum of two allowed signs) in lieu of a façade sign. Maximum size shall be 15 square feet. Letters shall be a maximum of 16 inches in height. Sign shall be individually illuminated letters, pin mounted to projected metal marquees. All exposed conduit shall be concealed

from public view and painted to match marquee structure. Exposed raceways behind letters are not permitted.

Projecting Signs: Projecting signs may be sized to complement the architectural elements on which they are placed and will be determined at the sole discretion of the Master Developer.

Blade Signs: One blade sign is required at each storefront. The maximum allowable size for a blade sign shall be seven square feet. Decorative brackets and sign design are to reflect the qualities of the tenant and the shopping center in a greater entirety.

2. Sub-Major Tenant Signs

Wall Signs: Wall signs may consist of either façade signs, projecting signs, or marquee signs. Wall signs are required for each Tenant and shall be installed within sign areas indicated on the building drawing. The maximum height for letters in the body of the sign shall not exceed 48 inches. The sign areas shall not exceed ten percent (10%) of the area of the storefront. A maximum of one sign per storefront with a maximum of three per tenant shall be permitted. Façade signs shall not extend more than 8 inches beyond the face of the surface to which the sign is mounted. Façade signs shall be individually illuminated channel letter with colored translucent face, or reverse- channel, halo-lit illuminated individual letters mounted to the building face. One additional 8-inch over-door transom sign is allowed per storefront. Direct illuminated signs and tag lines shall be considered for approval by the Master Developer but shall be reviewed on an individual basis. Any allowable tag lines shall be individually-illuminated letters (no box signs) and shall not exceed 16" in height. The width of the tag line, if approved, shall not exceed the width established for the primary signage.

Marquee Signs: One marquee sign shall be permitted per storefront (with a maximum of two allowed signs) in lieu of a façade sign. Maximum size shall be 15 square feet. Letters shall be a maximum of 20 inches in height. Sign shall be individually illuminated letters, pin mounted to projected metal marquees. All exposed conduit shall be concealed from public view and painted to match marquee structure. Exposed raceways behind letters are not permitted.

Projecting Signs: Projecting signs may be sized to complement the architectural elements on which they are placed and will be determined at the sole discretion of the Master Developer.

Blade Signs: One blade sign is required at each storefront. The maximum allowable size for a blade sign shall be seven square feet. Decorative brackets and sign design are to reflect the qualities of the tenant and the shopping center in a greater entirety.

3. Major Tenant Signs

Wall signs: Wall signs are required and shall be installed within sign areas indicated on the building drawings. Tenant sign area shall be on the building faces and as part of the building design. The sign areas shall not exceed ten percent (10%) of the building elevation. A maximum of one primary sign shall be permitted per storefront, with a maximum of four total signs permitted. In addition to the primary signs, tenants shall be permitted to include up to three additional secondary signs, on primary elevation only, describing product sold within the store. Total area of all signs together is not to exceed ten percent (10%) of the building elevation. Façade signs shall not extend more than 8" beyond the face of the surface to which the sign is mounted. Façade signs shall be individually illuminated channel letter with colored translucent face, or reverse- channel, halo-lit illuminated individual letters mounted to the building face. One additional 8" over door transom sign is allowed per storefront. Direct illuminated signs will be considered for approval but will be reviewed on an individual basis.

4. Out Parcel Tenant

Wall Signs: Wall signs are required and shall be installed within sign areas indicated on the building drawings. The maximum height for letters in the body of the sign shall not exceed on average 36 inches in height. The sign areas shall not exceed ten percent (10%) of the area of the storefront. A maximum of four (4) wall signs and one monument sign are allowed. Refer to "Monument Signage" for monument sign information. Signage shall be illuminated individual letters mounted on the building's opaque background or as approved by the Master Developer. The use of a colored or frosted Plexiglas face is required for channel-letter signs.

5. Out Parcel Tenant (with drive-thru)

Drive-thru users are allowed directional signs and menu board signs. The directional signs shall be a maximum of 2 square feet each. The menu board signs shall be a maximum of 65 square feet total. Multiple signs are allowed but shall not exceed the 65 square feet total. Both the directional and menu board signs shall have a stone or brick base to match the building materials.

6. Casino / Hotel Signs

Casino / Hotel building signs located within the shopping center plaza, and free-standing and monument signs located anywhere in the development, shall be subject to the requirements of the Planned Development Handbook. Casino / Hotel signs located outside of the shopping center plaza shall be subject to the criteria as outlined in the Planned Development Handbook.

7. Movie Theater Signs

Movie theater building signs located within the shopping center plaza, and free-standing and monument signs located anywhere in the development, shall be subject to the requirements of the Planned Development Handbook. Theater signs located outside of the shopping center plaza shall be subject to the criteria as outlined in the Planned Development Handbook.

Theater sign standards are as follows:

- (1) "IMAX" wall sign, 9'-0" high letters x 45'-6" (approx.) long letter grouping, internally illuminated shoebox-type letters.
- (1) "IMAX" wall signs, 3'-0" high letters x 17'-0" long letter grouping, internally illuminated shoebox-type letters.
- (2) "LEGENDS" vertical tower signs, 2'-0" wide x 27'-0" long letter grouping, internally illuminated individual shoebox-type letters.
- (2) "THEATRE" canopy signs, 3'-3" high x 17'-6" long letter grouping, internally illuminated individual script letters.
- (4) LCD Reader Board marquee monitors, 4'-0" high x 8'-0" long.
- "LEGENDS" billboard sign, 2'-6" high x 16'-0" long letter grouping, internally illuminated individual script letters.
- (10) Wall mounted internally illuminated Poster Cases, 3'-3" wide x 4'-11" tall.
- (5) Studio signs, 12'-0" wide x 10'-6" mounted on the structural steel billboard.
- (2) "LUXURY +" canopy signs, 2'-8 1/2" average height letters, "L" to be approx. 5'-4" high and "+" to be approx. 3'-11". Sign to be 31'-1" long letter grouping, Open face channel script letters with 8" returns and scintillating lights with dimmer.

8. Baseball Stadium Signs

   Baseball Stadium building signs located within the baseball stadium and facing toward the interior of the stadium, and free-standing and monument signs located anywhere in the development, shall be subject to the requirements of the Planned Development Handbook. Baseball Stadium signs located at the perimeter of the baseball stadium and facing away from the interior of the stadium shall be subject to the City of Sparks Municipal Code as pertains to signs for Tourist Commercial (TC) zoning.

9. Electronic Message Signs: Text, photos or copy only permitted, video and animation are not permitted. All messages will remain visible for a minimum of 15 seconds with changes taking no more than 3 seconds to accomplish.

Free standing and monument signs
For all monument and free-standing signs (identified as to approximate location on Exhibit A-4), the following criteria shall apply:

Monument signs shall not be closer than 40' to another monument sign.

Monument signs associated with a specific retail tenant shall be limited to 6 feet in height and a maximum area of 150 square feet.

Center-identification monument signs shall be of a customized design integrated with the surrounding landscape elements and shall be subject to individual review and approval by the Administrator.

A maximum of eight free-standing signs shall be permitted. Free-standing signs shall not exceed 65' in height and 500 square feet in sign area, except that up to two free-standing signs located along the I-80 frontage shall be permitted up to 90 feet in height and 1,000 square feet in sign area. Free standing signs shall not be closer than 60 feet to another free standing sign. One free-standing sign 90 feet in height shall be permitted to include an animated video reader board fitting within the sign area limit for that sign. The same sign may also provide up to three lower panels, within the sign area limit, promoting activities in the development. Any sign containing a video board must be set a minimum of 1,000 feet away from the lake shore along the I-80 frontage. Video boards must be dimmed at night to a maximum 30,000 lumens measured 3 feet in front of the board.

One additional 25' freestanding tenant directional sign will be allowed as indicated on Exhibit A-4. This directional sign is to identify tenants in Buildings B, I and Pads 1, 2 and 3.

One additional video board monument sign will be allowed to identify the Chamber of Commerce at a location indicated on Exhibit A-4.

The minimum constant display for this sign shall be eight (8) seconds with a maximum time allowed for messages to change is one (1) second. The use of animation, fade, dissolve, travel, flashing, message sequencing, scrolling, or blinking characters is prohibited. The maximum luminance shall be 6,300 nits (candelas per square meter) from dawn to dusk and 300 nits between dusk to dawn as measured from the sign's face at maximum brightness. The sign shall include photo-sensors to provide automatic intensity adjustment based on ambient lighting conditions. Size of sign shall be a maximum of 12.5' in height and not to exceed 80 square feet.

All free standing and monument signs shall be set back from property lines a minimum of 5 feet, from interstate highway right-of-way a minimum of 2 feet, and from public street right-of-way a minimum of 5 feet. Signs may not overhang the property line into the right-of-way. Safe sight lines shall be maintained in accordance with AASHTO standards.

Free-standing or monument signs may incorporate product displays in a dedicated display area, such as a vehicle of any type, which may be displayed on a supporting platform designed for the purpose, where located within 50 feet of an interstate highway frontage. The product display shall not be included in the overall sign area.

On all free standing or monument signs, twenty five percent (25%) of the display surface must be opaque on all internally lit channel-letter signs within this development. Translucent areas shall be limited to individual lettering, logos, or similar graphics.

No illuminated free-standing sign may face toward the existing adjacent residential area at the northwest corner of the development when located within 300 feet of the development's west property line; except that signs located in the main retail center may face a direction ranging from directly west to 45 degrees north of west only.

Additional Signs
Service doors to Tenant spaces throughout the project shall be standard 4 inch, identification only (name and address number, or additional as required by local jurisdiction) and shall be installed by the Landlord. The Tenant shall not apply any signs or other wording to service doors.

Directional signs smaller than 5 square feet shall be allowable and shall not count against the total sign count per façade or the total sign area.

Miscellaneous Provisions
Roundabouts may include an illuminated sculpture or a feature item celebrating the nature of the development or its conceptual theme. Such elements shall not be located in public right-of-way area.

Entry drives may include a sculptural feature with illumination. Each entry may incorporate an illuminated entry monument sign featuring the name of the development with a maximum height of 12’-0”. Such elements shall not be located in public right-of-way area.

Exterior artwork such as sculptures, statues, or fountains shall not be counted as signs.

Existing billboards currently located on the development property shall be removed prior to the issuance of the first certificate-of-occupancy for the project.

IX. Lighting and Sound Systems

Lighting for the development shall conform to the City of Sparks Design Standards Manual, including requirements for cut-off fixtures and prohibition of illumination on adjacent property.

Parking Lot and Pedestrian Area Lighting (DSM 3.B.8)
Parking lot lighting shall be provided by high quality, full cut-off fixtures. Fixtures shall be placed to prevent any light trespass onto adjoining properties and to prevent direct glare on adjacent properties or streets. The height of the lighting poles may vary depending on location (for instance, shorter poles near adjacent residential). Pedestrian areas of the project shall have lower scale, more detailed fixtures as a visual cue for pedestrians. Pedestrian areas adjacent to the waterfront shall utilize the same fixture currently in use around the Sparks Marina to

continue that theme at the pedestrian connection to other parts of the Marina Area. The color of the Marina Area themed lighting may vary from the existing color to provide a transition into The Legends at Sparks Marina. Photometric diagrams shall be submitted with the lighting plans for site improvement permit review. Refer to the concept Site Lighting Plan Exhibits C-5.7 and C-5.8.

Public Street Lighting
Light fixtures along the public street shall match the existing fixture type of the Marina District, although a different color or finish may be used to transition the style through The Legends at Sparks Marina development. The parking lot lighting shall be high quality decorative lighting and poles appropriate to the use and consistent with the architectural design of the project.

Parking Lot Lighting
Due to the proposed scale of the development and height of the buildings, a slightly taller light pole configuration is appropriate. If approved by developer, parking lot lighting poles can be a maximum of 40' high, provided the prohibition of illumination on adjacent property is maintained. Poles must be placed away from residential areas to avoid light trespass, and cut-off fixtures are required, per the Design Standards Manual. Poles must be sized appropriately, considering the location on the site and the height of adjacent structures. Poles in parking areas may incorporate a 3' high concrete base for protection. The style of the poles and fixtures shall be decorative and complement the architecture of the buildings.

The developer shall submit a lighting plan for review and approval by the Administrator prior to the issuance of a building permit. The developer shall comply with the lighting standards and requirements of the Sparks Municipal Code, and City of Sparks Design Standards Manual as noted above. To reduce the impact of glare and minimize the effect of parking lighting, lighting fixtures with external hoods and internal glare reduction louvers will be used.

Baseball Stadium Lighting
Lighting for the baseball stadium will be designed per the required standards of the baseball league and shall incorporate high quality cut-off type fixtures that minimize glare and light trespass on adjacent properties or right-of-ways.

Baseball Stadium Sound System
To minimize community noise impact on residential areas near the ballpark, a distributed loudspeaker type of sound system will be provided. The configuration will place a large number of small loudspeakers behind the seating, directing sound towards the seats and center of the playing field, rather than towards the property lines where adjacent uses may be impacted. The distributed loudspeaker system will lower the acoustical power required from each speaker, due to closer positioning to the listener; substantially decreasing sound levels at nearby properties. In addition, the speakers will purposely be aimed away from adjacent properties, further mitigating the noise impacts to the approval of the administrator prior to the issuance of the certificate of occupancy for the stadium. Concerts and other events not using the stadium sound system that occur Sunday night through Thursday night shall end at 11:00 P.M. or earlier, unless specifically

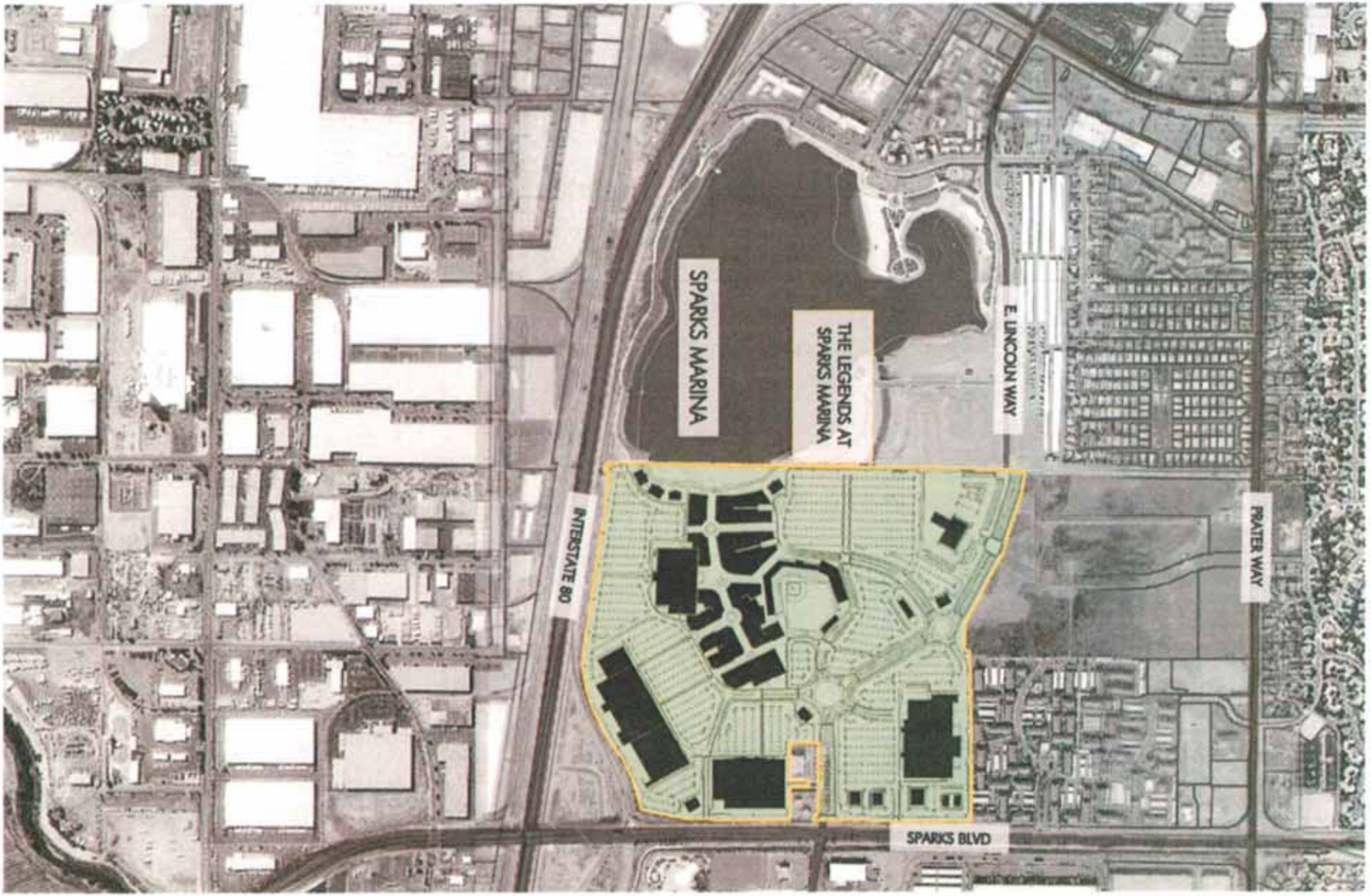
approved by the City of Sparks. Friday and Saturday evening events not using the stadium sound system shall conclude at 12:00 A.M., unless specifically approved by the City of Sparks.

Lighting and Sound Systems for other Uses not included in the Approved Plan
Other potential uses not included in the current approved plan and submitted separately for Special Use Permit shall explicitly define regulations governing lighting and sound systems, particularly with regard to development of water/light/sound features in the area of the Sparks Marina Lake adjacent to the lake shore frontage of the development. Such light systems shall not result in spill-over glare onto adjoining properties outside of the development, except for spill-over onto City-owned property (i.e. the lake surface and public path system) adjacent to the development. Such sound systems shall conform to the same design criteria defined above for the baseball stadium sound system.

X. Buffering/Walls

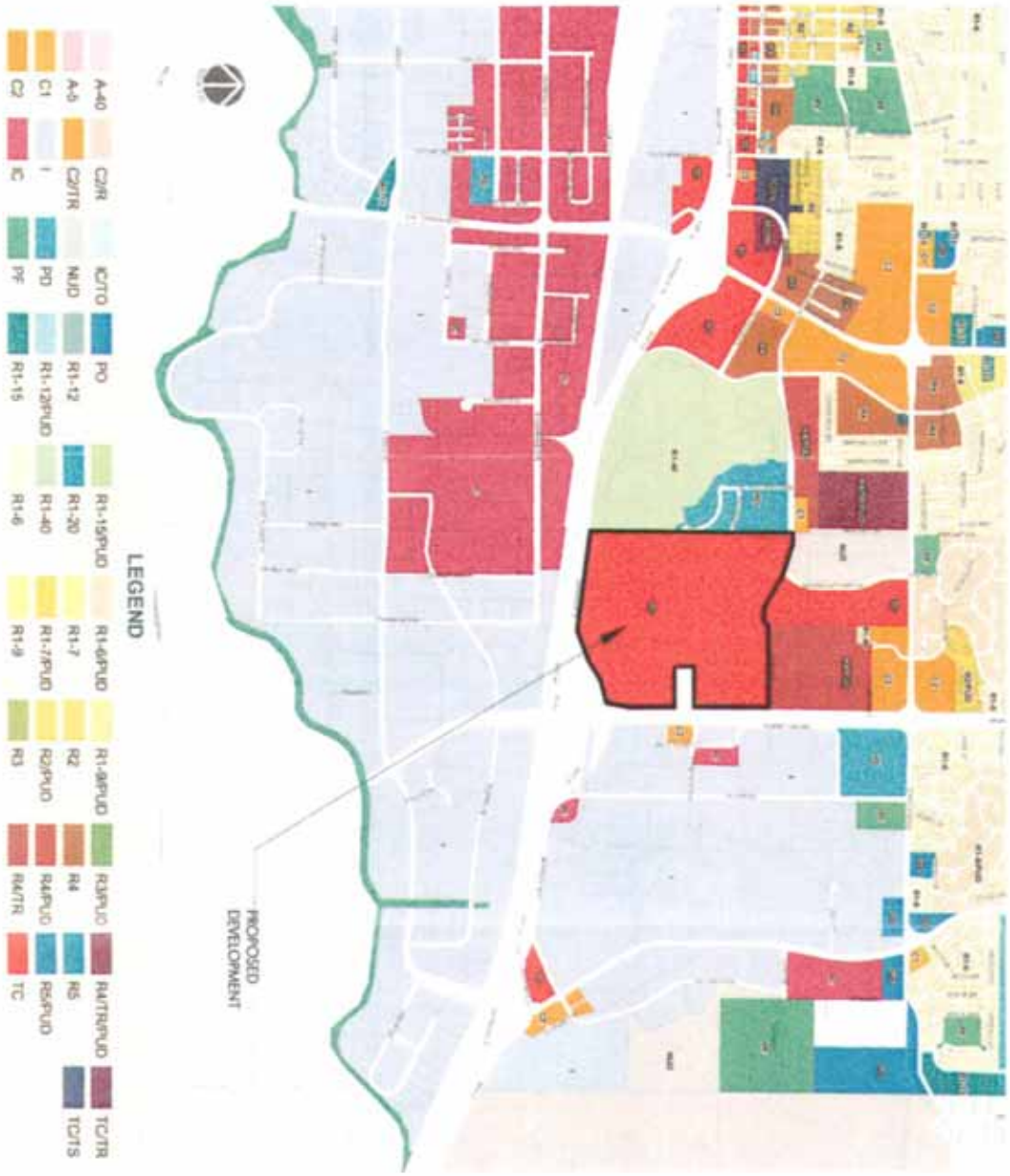
Buffering
The development shall follow the City of Sparks Design Standards Manual for buffering. A separation wall shall be utilized adjacent to residential areas.

All buildings and parking areas on the site shall be screened from adjacent residential property in the northwest corner of the site by berms, landscaping, and a buffer wall. Buffer wall shall be constructed of masonry block or precast concrete panels and be a minimum of six feet in height and a maximum of eight feet, Landscaping shall be included on both sides of the buffer wall, and shall include evergreen trees on the residential side of the wall (at a minimum spacing of 20 feet on center) and shall be a minimum of 6 feet in height at time of planting. The residential side of the buffer wall shall be comprised of a minimum 10-foot wide planting bed. Typical buffer wall details are shown in attached Exhibit A-9.1.



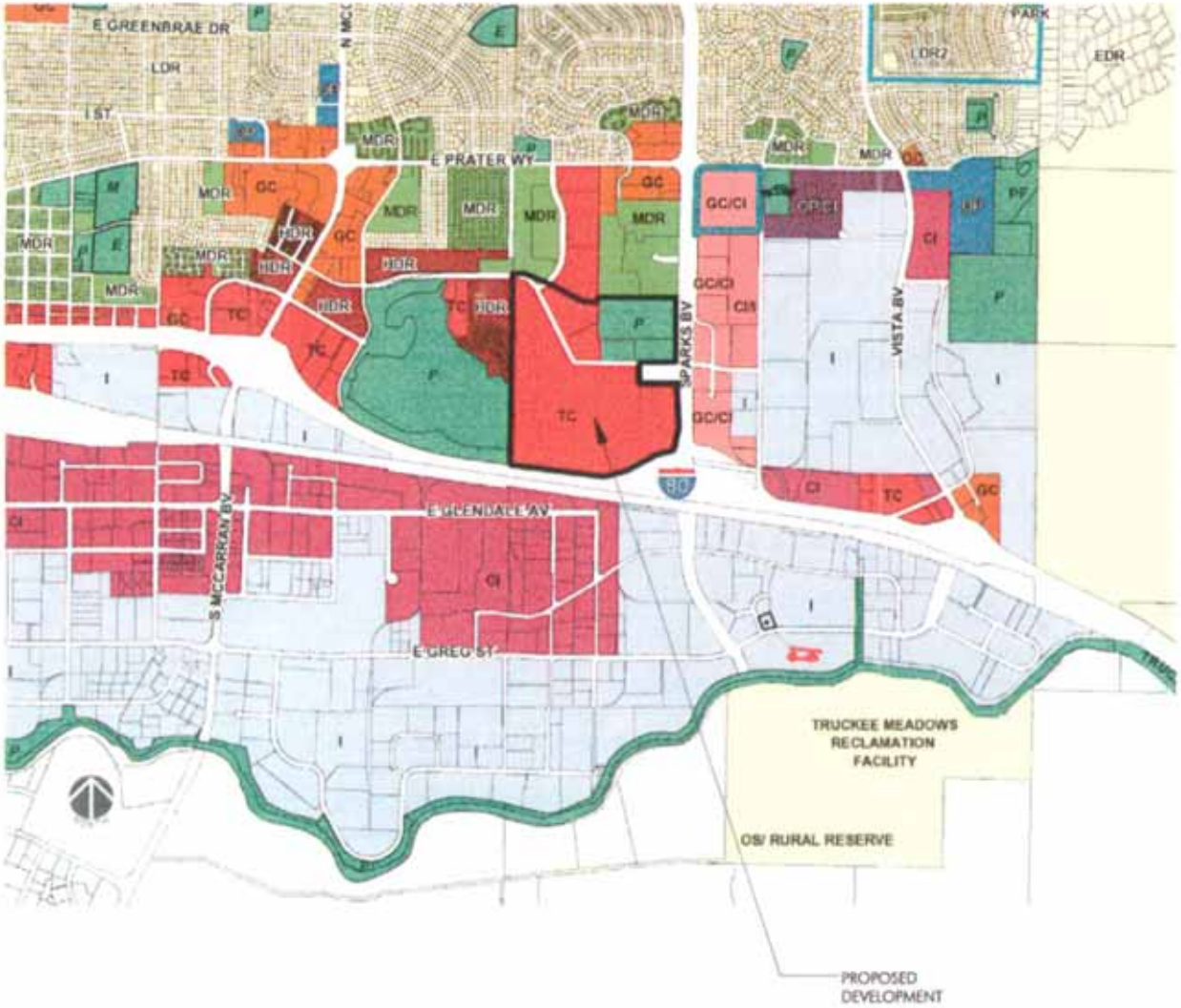
THE LEGENDS AT SPARKS MARINA

EXHIBIT A-1
VICINITY PLAN



THE LEGENDS AT SPARKS MARINA

EXHIBIT A-2
EXISTING ZONING



LEGEND:

- INDUSTRIAL
- PUBLIC FACILITY
- COMM. INDUSTRIAL
- OFFICE PROFESSIONAL / COMM. INDUSTRIAL
- OFFICE PROFESSIONAL
- RURAL RESERVE
- GENERAL COMM.
- TOURIST COMM.
- GENERAL COMM. / COMM. INDUSTRIAL
- COMM. INDUSTRIAL / INDUSTRIAL
- HIGH DENSITY RESIDENTIAL
- MEDIUM DENSITY RESIDENTIAL

EXHIBIT A-3
EXISTING LAND USE

THE LEGENDS AT SPARKS MARINA

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-4
CONCEPTUAL
DEVELOPMENT PLAN

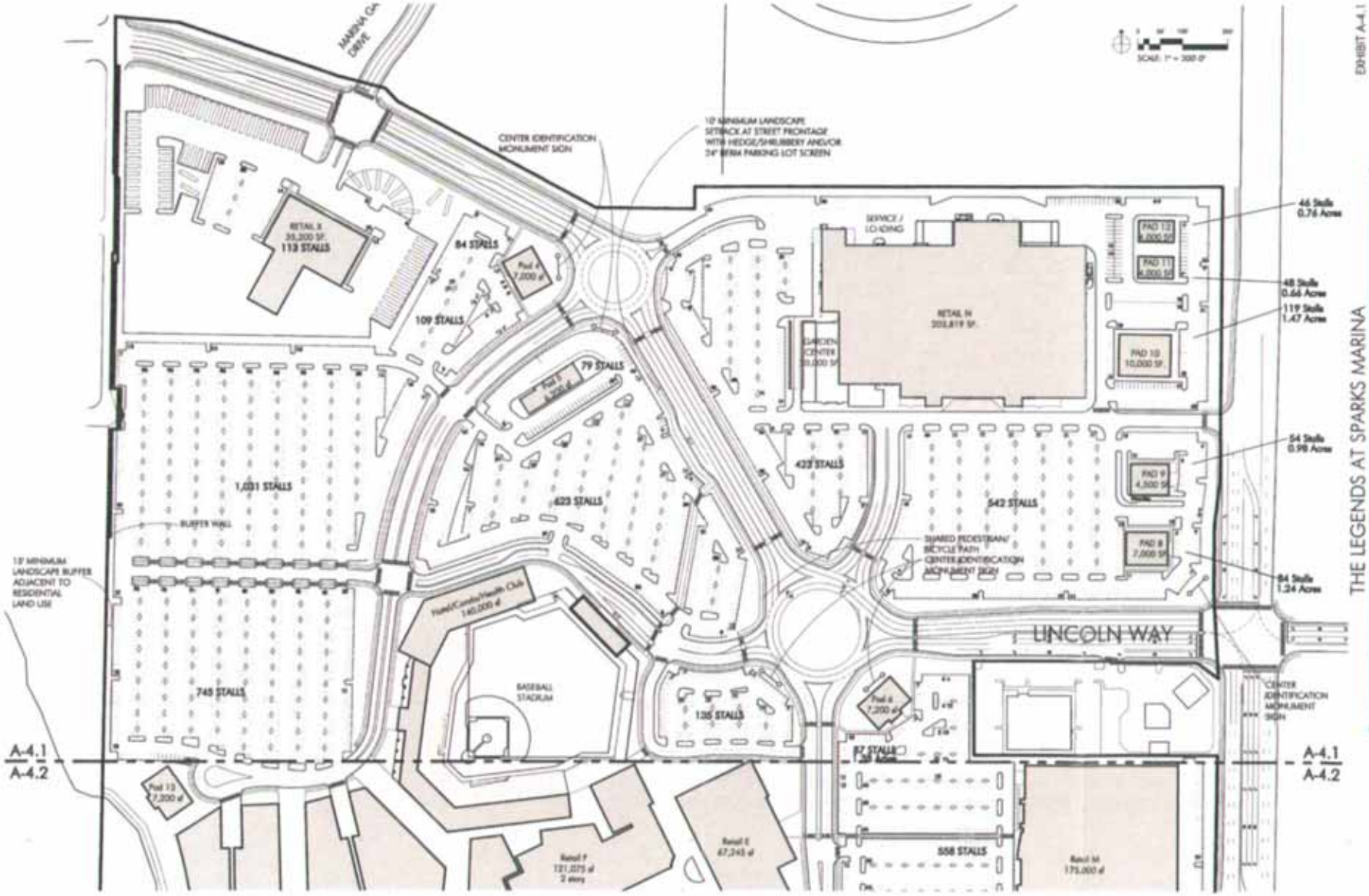


EXHIBIT A-4.1
CONCEPTUAL
DEVELOPMENT PLAN
THE LEGENDS AT SPARKS MARINA
SCALE: 1" = 300'-0"
MARINA GATE DRIVE
CENTER IDENTIFICATION MONUMENT SIGN
10' MINIMUM LANDSCAPE SETBACK AT STREET FRONTAGE WITH HEDGE/SHRUBBERY AND/OR 24" BERM PARKING LOT SCREEN
RETAIL X
35,200 SF
119 STALLS
84 STALLS
Pad 4
7,200 sf
109 STALLS
79 STALLS
Pad 3
6,200 sf
1,031 STALLS
623 STALLS
BUFFER WALL
15' MINIMUM LANDSCAPE BUFFER ADJACENT TO RESIDENTIAL LAND USE
Hotel/Conv/Health Club
140,000 sf
BASEBALL STADIUM
745 STALLS
Pad 13
7,200 sf
A-4.1
A-4.2
SERVICE / LOADING
RETAIL N
203,819 SF
GARDEN CENTER
30,000 SF
423 STALLS
542 STALLS
SHARED PEDESTRIAN/ BICYCLE PATH
CENTER IDENTIFICATION MONUMENT SIGN
PAD 12
6,000 SF
PAD 11
6,000 SF
PAD 10
10,000 SF
PAD 9
4,500 SF
PAD 8
7,000 SF
46 Stalls
0.76 Acres
48 Stalls
0.66 Acres
119 Stalls
1.47 Acres
54 Stalls
0.98 Acres
84 Stalls
1.24 Acres
LINCOLN WAY
CENTER IDENTIFICATION MONUMENT SIGN
Pad 6
7,200 sf
135 STALLS
87 STALLS
558 STALLS
Retail E
47,245 sf
Retail F
121,075 sf
2 story
Retail M
175,000 sf


EXHIBIT A-4.2
CONCEPTUAL
DEVELOPMENT PLAN
THE LEGENDS AT SPARKS MARINA
SPARKS BOULEVARD
A-4.1
A-4.2
BASEBALL STADIUM
745 STALLS
135 STALLS
87 STALLS
558 STALLS
303 STALLS
601 STALLS
PEDESTRIAN PATHS, TYP.
208 STALLS
85 STALLS
88 STALLS
218 STALLS
928 STALLS
201 STALLS
48 STALLS
Pad 13 7,200 sf
Pad 6 7,200 sf
Pad 7 7,000 sf
Pad 3 5,500 sf
Pad 2 7,200 sf
Pad 1 7,200 sf
Retail H 74,290 sf
Retail G 186,845 sf 2 story
Retail F 121,075 sf 2 story
Retail E 67,245 sf
Retail D 53,850 sf
Retail C 44,535 sf
Retail B 17,675 sf
Retail I 21,875 sf
Retail A 225,000 sf 2 Story
Retail M 175,000 sf
Retail L 98,800 sf
Retail K 128,000 sf
Retail J 30,000 sf
RELOCATED BIKE PATH (12' WIDE)
PROPOSED NEW BOAT DOCKS BE TENANT
EXISTING BOAT LAUNCH RAMP
PROPSED PAVEMENT EXTENSION BY TENANT
CENTER IDENTIFICATION MONUMENT SIGN
FREE-STANDING CENTER IDENTIFICATION SIGN - 65'
FREE-STANDING CENTER IDENTIFICATION SIGN - 90'
10' FREEWAY LANDSCAPE BUFFER
FREE-STANDING CENTER IDENTIFICATION SIGN - 65'
FREE-STANDING CENTER IDENTIFICATION SIGN - 90'
FREE-STANDING CENTER IDENTIFICATION SIGN - 65'
SCALE: 1" = 200'-0"

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.1
CONCEPTUAL MAJOR
TENANT EXTERIOR
ELEVATION

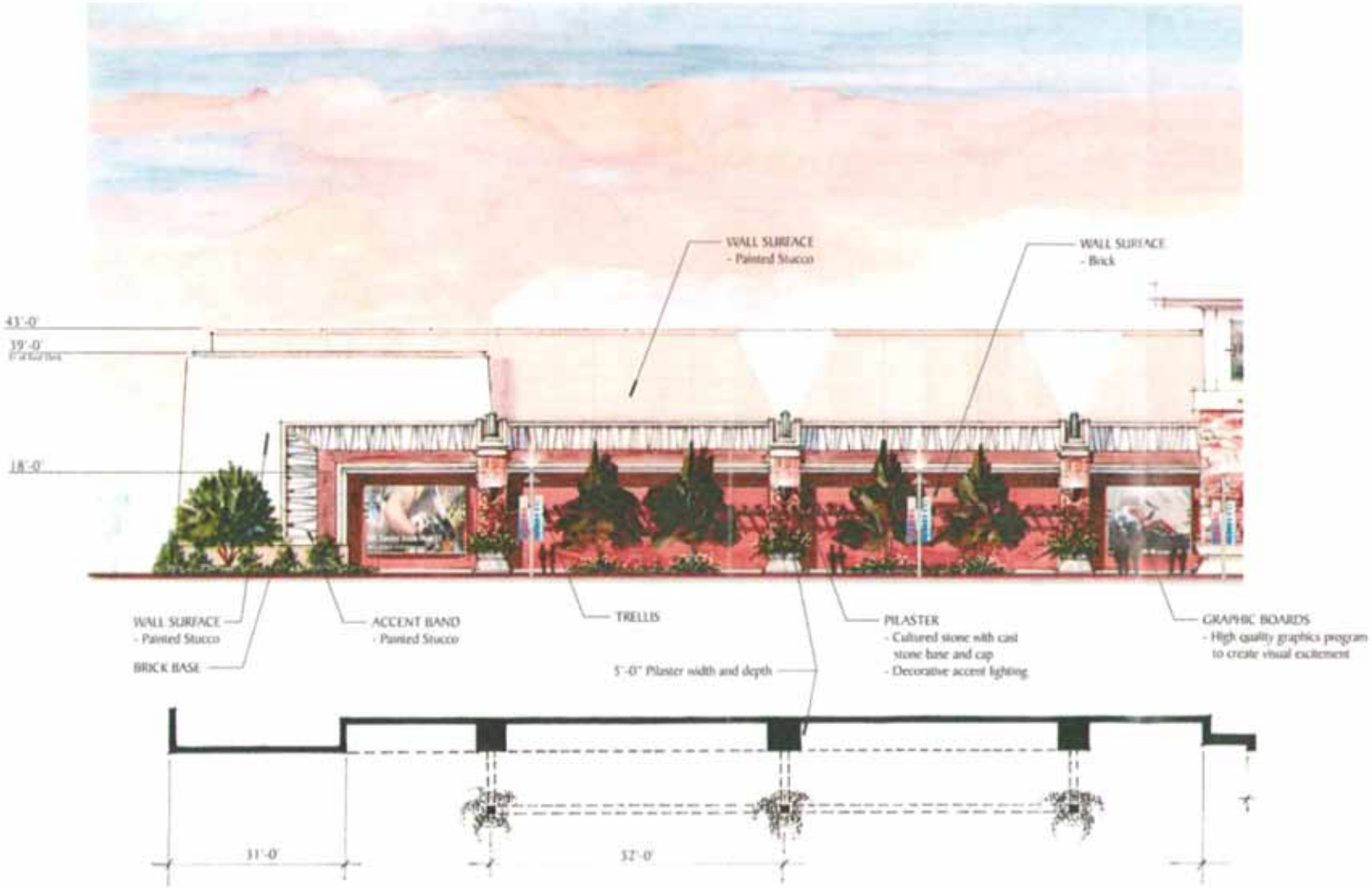


EXHIBIT A-5.2
CONCEPTUAL MAJOR TENANT ENLARGED PARTIAL ELEVATION

THE LEGENDS AT SPARKS MARINA

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.3
CONCEPTUAL MAJOR
TENANT ENLARGED
PARTIAL ELEVATION

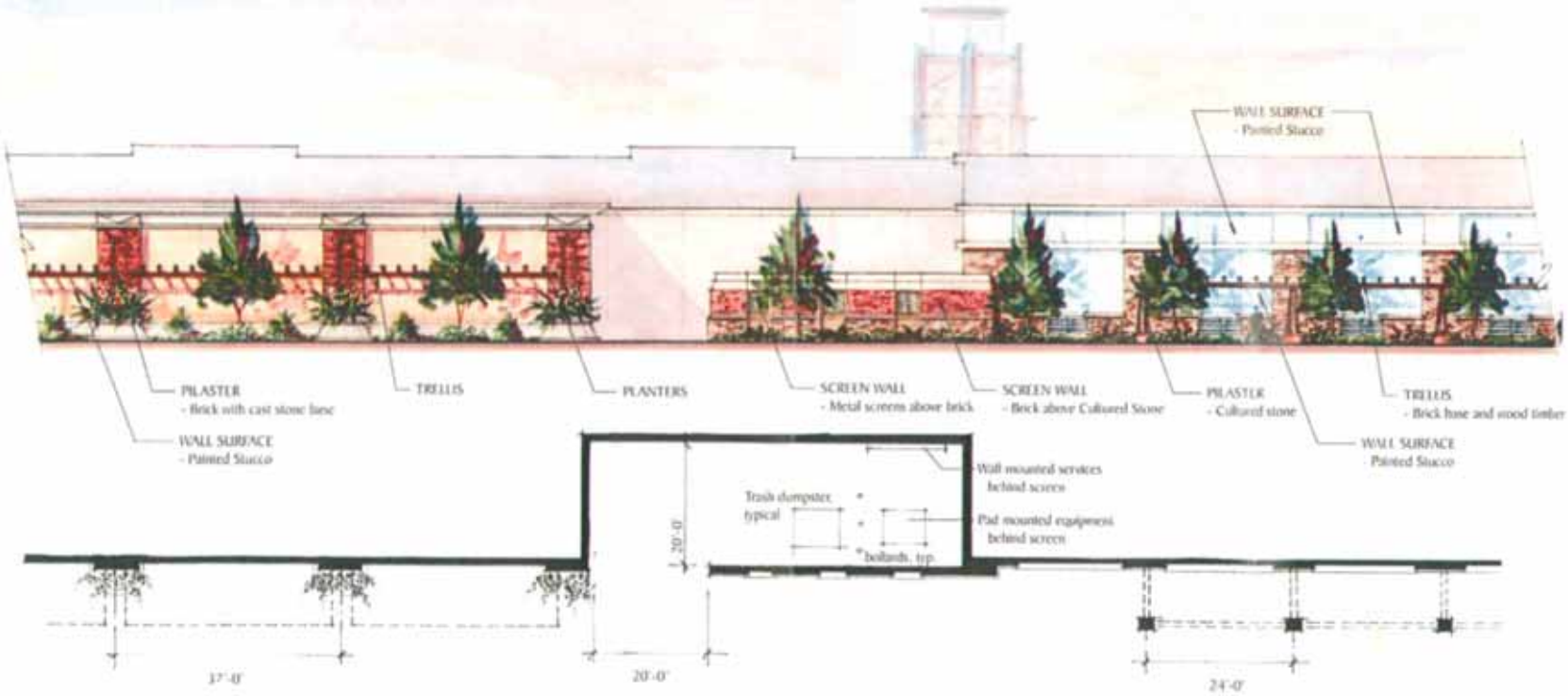
THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.4
CONCEPTUAL MAIN ENTRY/
TYPICAL SMALL SHOPS/
EXTERIOR ELEVATION

EXHIBIT A-5.5
CONCEPTUAL MAIN ENTRY/
TYPICAL SMALL SHOPS

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.6
CONCEPTUAL
EXTERIOR ELEVATION



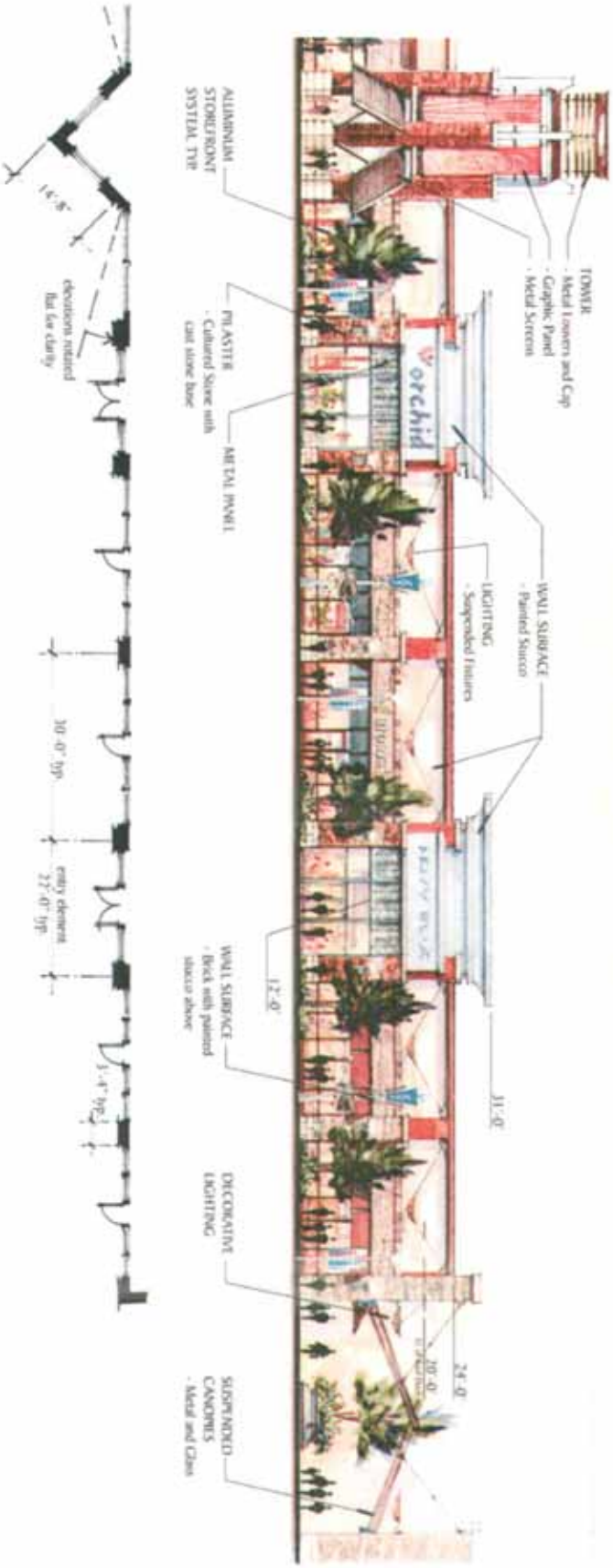
THE LEGENDS AT SPARKS MARINA

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.7
CONCEPTUAL
TYPICAL SMALL SHOPS

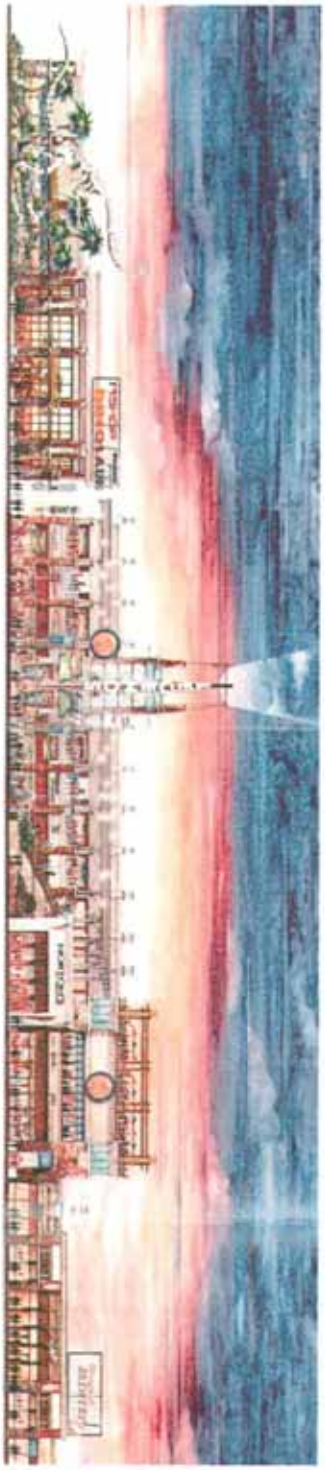
EXHIBIT A-5.8
CONCEPTUAL
TYPICAL SMALL SHOPS

THE LEGENDS AT SPARKS MARINA



THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.9
CONCEPTUAL
TYPICAL SMALL SHOPS



THE LEGENDS AT SPARKS MARINA

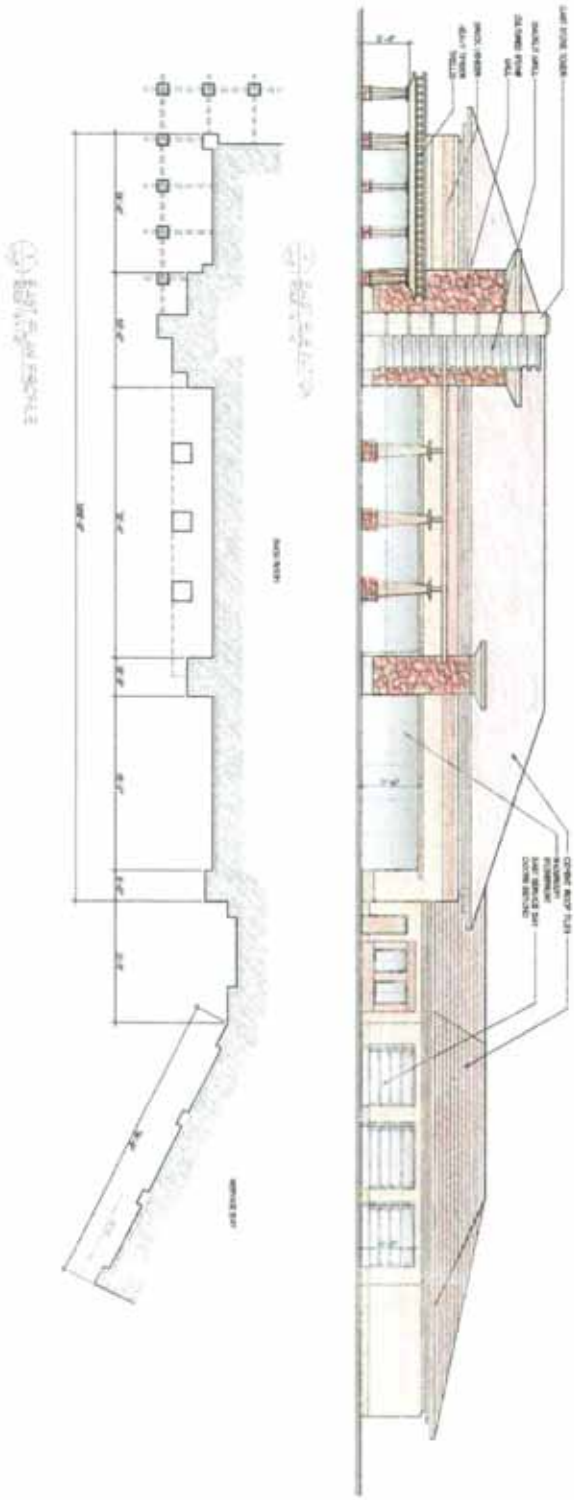
EXHIBIT A-5.10
CONCEPTUAL
PLAZA ELEVATION

EXHIBIT A-5.11
PLAZA ELEVATION

THE LEGENDS AT SPARKS MARINA

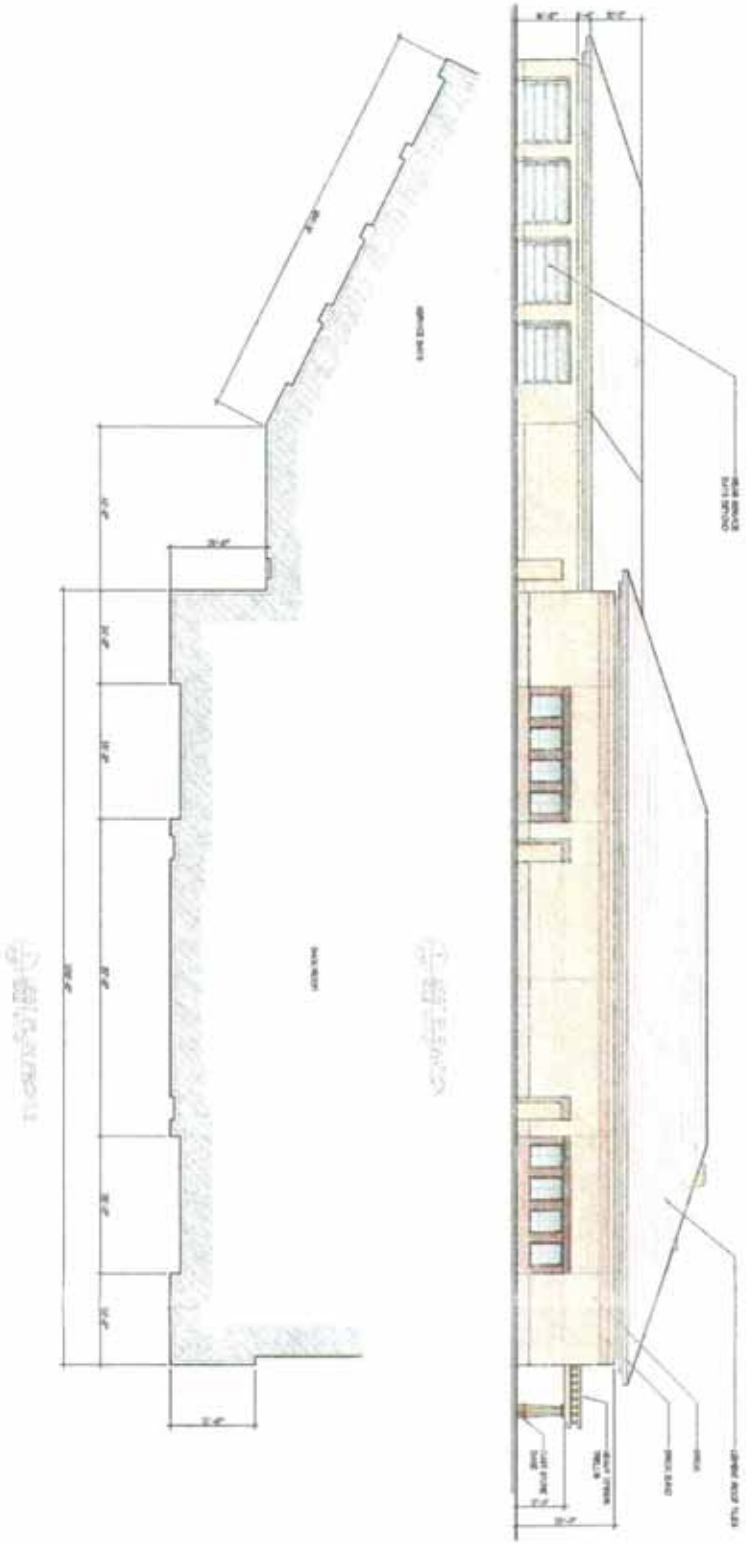
THE LEGENDS AT SPARKS MARINA

EXHIBIT A-5.12
CONCEPTUAL
PLAZA ELEVATION



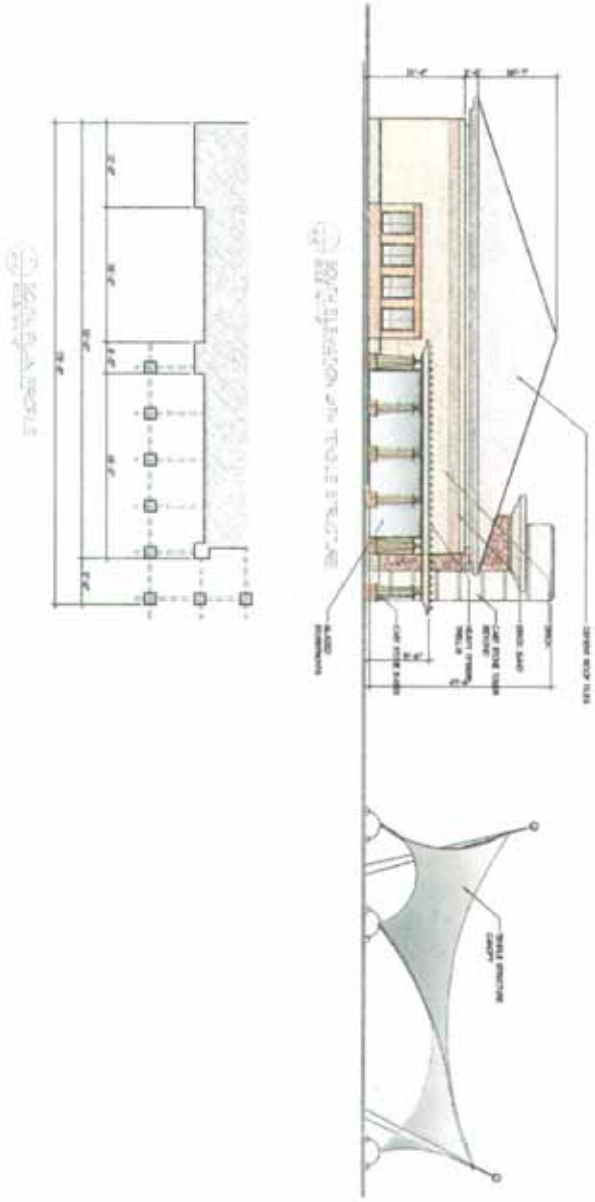
THE LEGENDS AT SPARKS MARINA - NORTHWEST PARCEL

EXHIBIT A-5.21
CONCEPTUAL
EXTERIOR ELEVATIONS



THE LEGENDS AT SPARKS MARINA - NORTHWEST PARCEL

EXHIBIT A-5.22
CONCEPTUAL
EXTERIOR ELEVATIONS

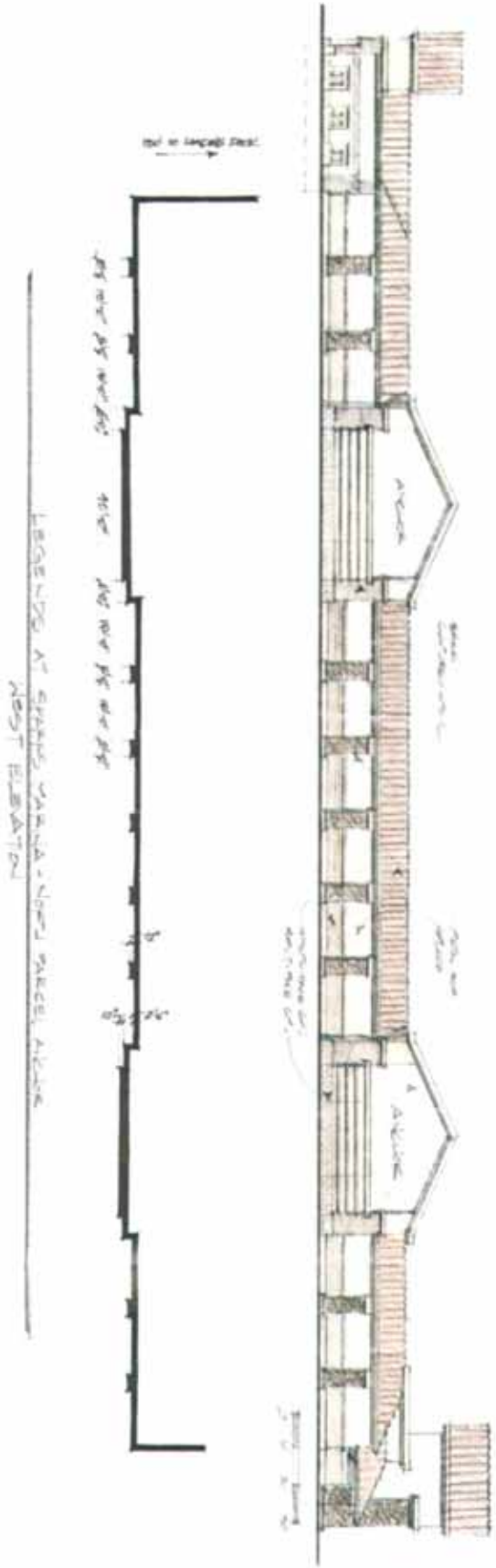


THE LEGENDS AT SPARKS MARINA - NORTHWEST PARCEL

EXHIBIT A-5.23
CONCEPTUAL
EXTERIOR ELEVATIONS

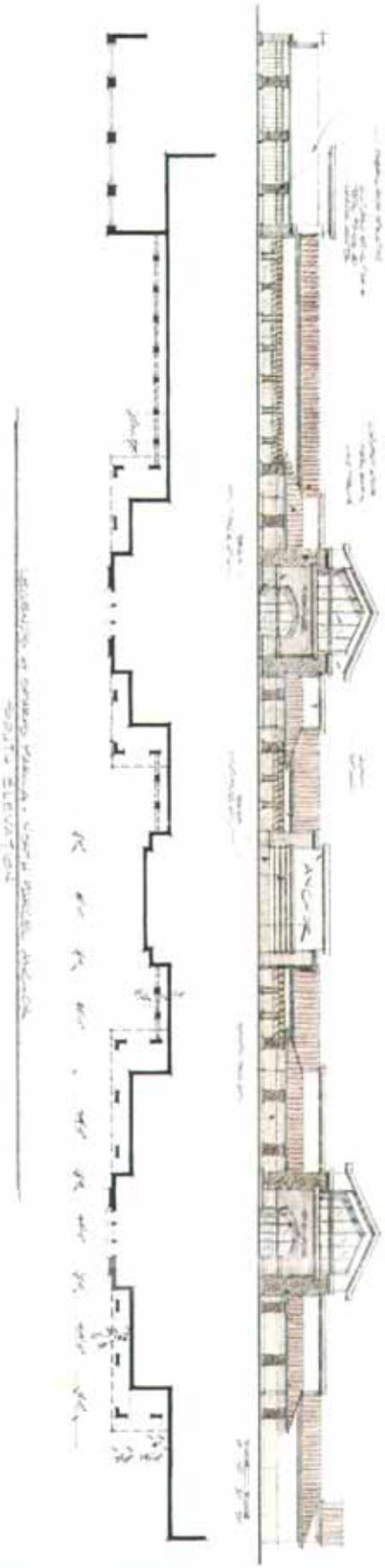
THE LEGENDS AT SPARKS MARINA - NORTHWEST PARCEL

EXHIBIT A-5.24
CONCEPTUAL
EXTERIOR ELEVATIONS



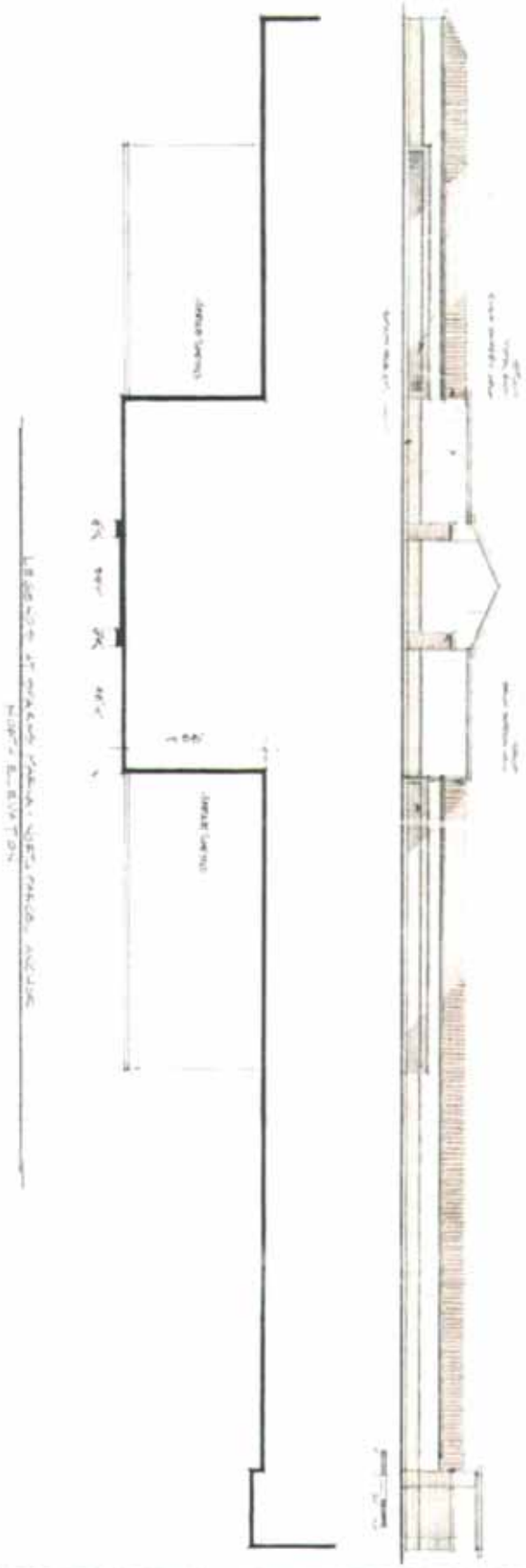
THE LEGENDS AT SPARKS MARINA - NORTHEAST PARCEL

EXHIBIT A-5.31
ANCHOR ELEVATION



THE LEGENDS AT SPARKS MARINA - NORTHEAST PARCEL

EXHIBIT A-5.32
ANCHOR ELEVATION



THE LEGENDS AT SPARKS MARINA - NORTHEAST PARCEL

EXHIBIT A-5.33
ANCHOR ELEVATION

ROOF

Slate Shingle:

Eco Star Stone Red

Metal 1:

Pac-Clad Burgundy

Accent Metal :

Pac-Clad Metallics

Copper Penny

MEDALLION

Pineapple Grove

Coastal Ivory

STOREFRONT

Metal 2:

Pac-Clad Medium Bronze

WALL

Stucco 1:

Dryvit #381

Monastery Brown

Stucco 2:

Dryvit #112

Saddlewood Beige

Paint 1:

Duron 8242W

Northern Plains

Paint 2:

Duron 8232W Lulled Beige

Brick 2:

Sioux City Brick

Doeskin Velour

Brick 1:

Sioux City Brick

Desert Blend

Arcufoam:

Champagne 20 Burned

Arcustone:

PFC 10 Stone French Cream

Cultured Stone :

Boulder Creek Stone

Faststack - Nema Blend

Material Board - Group 2

LEGENDS AT SPARKS MARINA

Sparks, Nevada

ROOF

Slate Shingle:
Eco Star Chestnut Brown

Metal 1:
Pac-Clad Natural Bronze

MEDALLION

Pineapple Grove
Coastal Ivory

STOREFRONT

Metal 2:
Pac-Clad Medium Bronze

WALL

Stucco 1:
Dryvit #281
Monastery Brown

Stucco 2:
Dryvit #108 Manor White

Paint 1:
Dunn 82184 Oxide

Paint 2:
Dunn 87329
Pewter Straw

Brick 2:
Sioux City Brick
Painted Fine Art Velour

Brick 1:
Sioux City Brick
Gunsmoke

Accents:
Champagne 50 Burned

Accentstone:
PSI 66 Stone Sienna

Colored Stone:
Boulder Creek Stone
Country Ledgestone - Brown

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-6.2
CONCEPTUAL
TYPICAL MATERIALS

Material Board - Group 3

LEGENDS AT SPARKS MARINA

Sparks, Nevada

ROOF
Slate Shingle
Eco Star Black
Metal 1:
Pac-Clad Dark Bronze
Accent Metal:
Pac-Clad Metallics
Copper Patina
MEDALLION
Pineapple Grove
Coastal Ivory

STOREFRONT
Metal 2:
Pac-Clad Sierra Tan
WALL
Stucco 1:
Dryvit #142
Spectrum Brown
Stucco 2:
Dryvit #112
Saddlewood Beige
Paint 1:
Dutch BOND
Stratford Brown
Paint 2:
Dutch BOND Dapple
Brick 2:
Sioux City Brick
Fire Art Velour

Brick 1:
Sioux City Brick Resort

Accents:
Champagne 10 Burned
Accentone:
Stone Dover
Cultured Stone:
Boulder Creek Stone
Redstone - Cheyenne

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-6.3
CONCEPTUAL
TYPICAL MATERIALS

Material Board - Group 4

LEGENDS AT SPARKS MARINA

Sparks, Nevada

ROOF

Slate Shingle:
Eco Star Chestnut Brown

Metal 1:
Pac-Clad Burgundy

MEDALLION

Pineapple Grove
Alluvial Sand

STOREFRONT

Metal 2:
Pac-Clad Metallics
Champagne

WALL

Stucco 1:
Dryvit #294 Leathwax

Stucco 2:
Dryvit #112 Amarillo White

Paint 1:
Dunn 024M
Antler Brush

Paint 2:
Dunn 026-D Tuscan Clay

Brick 2:
Sioux City Brick
Mountain Shadow Velour

Brick 1:
Sioux City Brick
Copperstone Velour

Accoloam:
Champagne 20 Burnout

Accustone:
PPC 10 Stone French Cream

Cultured Stone:
Boulder Creek Stone
Eastwood - Sable Blend

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-6.4
CONCEPTUAL
TYPICAL MATERIALS

Material Board - Group 5

LEGENDS AT SPARKS MARINA

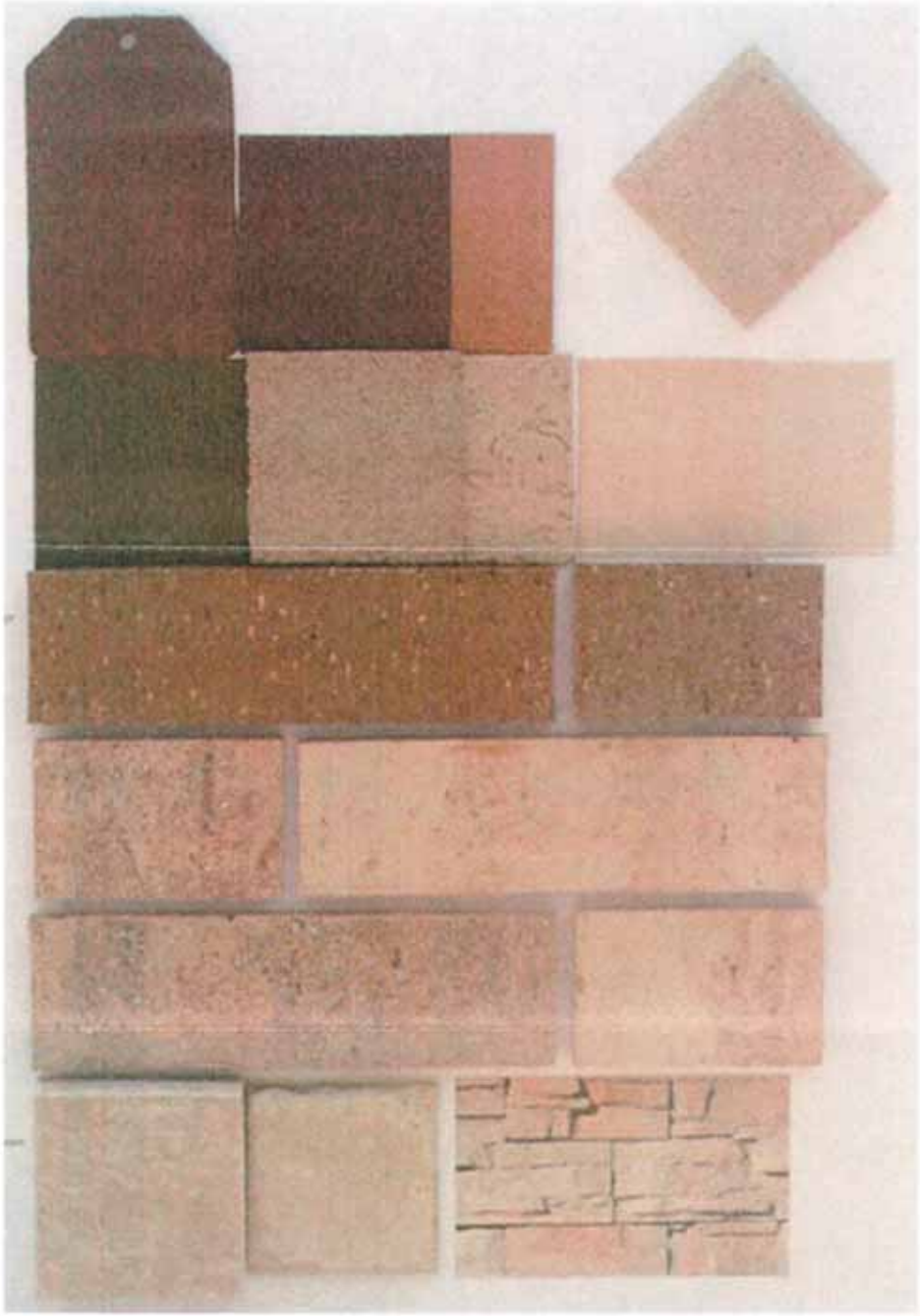
Sparks, Nevada

ROOF
Slate Shingle
Eco Star Stone Red
Metal 1:
Pac-Clad Burgundy
Accent Metal:
Pac-Clad Metallics
Copper Penny
MEDALLION
Pineapple Glow
Coastal Ivory

STOREFRONT
Metal 2:
Pac-Clad Medium Bronze
WALL
Stucco 1:
Dryvit #281
Mountain Brown
Stucco 2:
Dryvit #112
Sandlewood Beige
Paint 1:
Dunn 824JW
Northern Plain
Paint 2:
Dunn 823JW Lulled Beige
Brick 2:
Sioux City Brick
Desolate Velour

Brick 1:
Sioux City Brick
Desert Blend

Arcalium:
Champagne 20 Banded
Arcastone:
PFC 10 Stone French Cream
Cultured Stone:
Boulder Creek Stone
Fieldstone - Nevada Blend



THE LEGENDS AT SPARKS MARINA

EXHIBIT A-6.5
CONCEPTUAL
TYPICAL MATERIALS

Material Board - Group 6

LEGENDS AT SPARKS MARINA

Sparks, Nevada

ROOF

Slate Shingle:
Eco Star Chestnut Brown

Metal 1:
Pac-Clad Natural Bronze

Accent Metal:
Pac-Clad Metallics
Copper Penny

MEDALLION
Pineapple Grove
Pure Pearl

STOREFRONT

Metal 2:
Pac-Clad Medium Bronze

WALL

Stucco 1:
Dryvit #294 Earthware

Stucco 2:
Dryvit #399 Coffee Milk

Paint 1:
Dunn 32940 Tuscan City

Paint 2:
Dunn 82428
Northern Plains

Brick 2:
Interstate Brick
Baja Brown

Brick 1:
Sioux City Brick
Grand Canyon Wheat

Accents:
Seville 33 Sandal

Accentone:
PSE 16 Stone Seville

Cultured Stone:
Boulder Creek Stone
Prairie Bluff - Lanier Blend

Material Board - Group 7

LEGENDS AT SPARKS MARINA

Sparks, Nevada

ROOF
Slate Shingle:
Eco Star Chestnut Brown
Metal 1:
Pac-Clad Hemated Brown
MEDALLION
Pineapple Grove
Mortar Sand

STOREFRONT
Metal 2:
Pac-Clad Metallics
Aged Copper
WALL
Stucco 1:
Dryvit #106 Pearl Ash
Stucco 2:
Dryvit #419 Buckskin
Paint 1:
Duron B64-W Rugged Tan
Paint 2:
Duron CH55-2W
Subtle Natural
Brick 2:
Sioux City Brick
Silverstone Grey

Brick 1:
Sioux City Brick
Marble Stone Grey

Accolade:
Champagne 10 Ravened
Accolade:
PCM 10 Stone Champagne
Colored Stone:
Boulder Creek Stone
Fastrock - Kinta Green

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-6.7
CONCEPTUAL
TYPICAL MATERIALS

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-7
CONCEPTUAL
LANDSCAPE PLAN

EXHIBIT A-7.1
CONCEPTUAL
LANDSCAPE PLAN

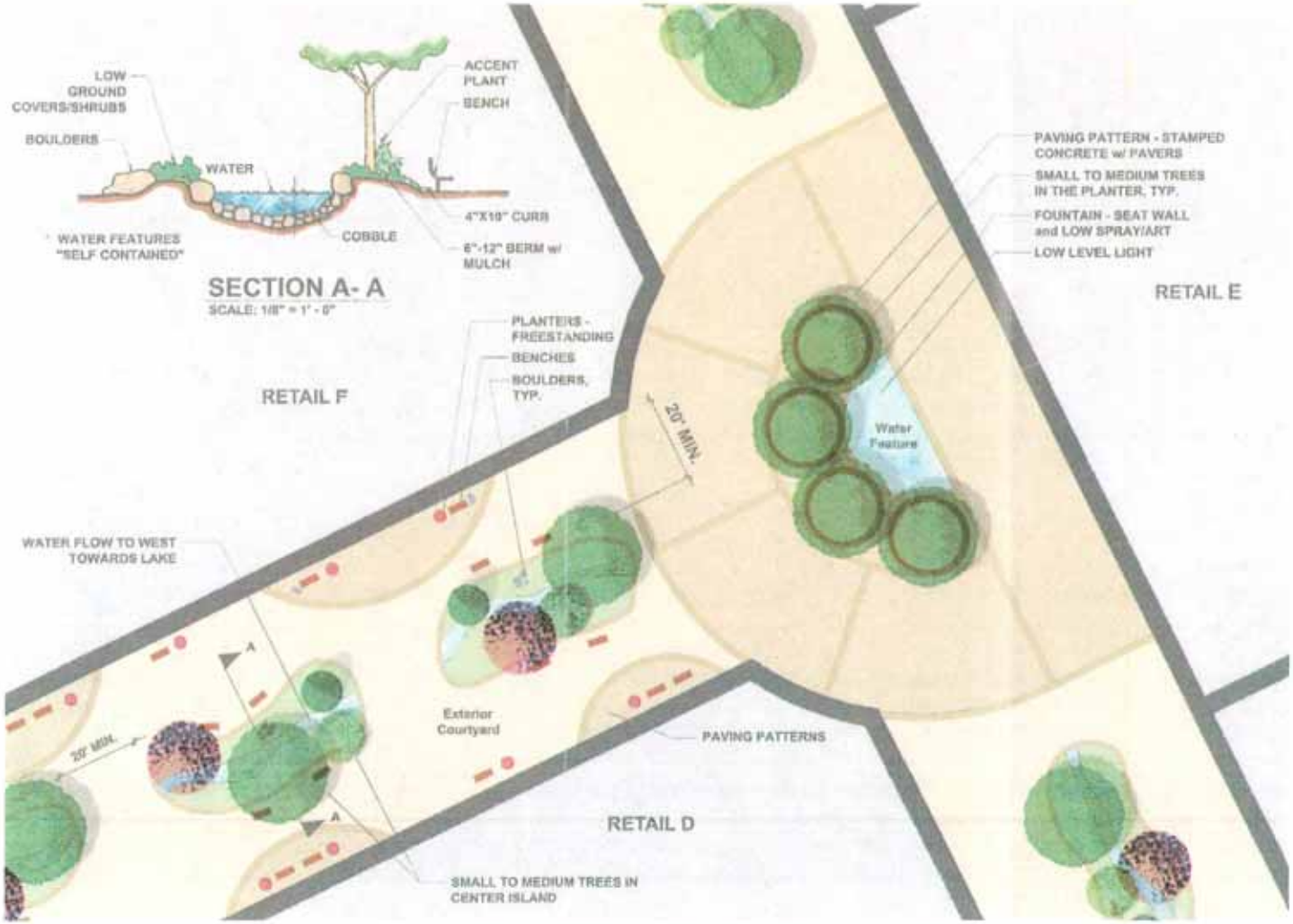
THE LEGENDS AT SPARKS MARINA

LEGENDS AT SPARKS MARINA

CONCEPTUAL LANDSCAPE PLAN

EXHIBIT A-7.2
CONCEPTUAL
LANDSCAPE PLAN

THE LEGENDS AT SPARKS MARINA



ENLARGEMENT AT SIGNIFICANT AREA #1

SCALE: 1" = 20' - 0"

NORTH

EXHIBIT A-8.1
CONCEPTUAL ENLARGED LANDSCAPE PLAN

THE LEGENDS AT SPARKS MARINA

ENLARGEMENT AT SIGNIFICANT AREA #2

SCALE: 1" = 30' - 0"

NORTH

EXHIBIT A-8.2
CONCEPTUAL ENLARGED LANDSCAPE PLAN

THE LEGENDS AT SPARKS MARINA

NORTH

ENLARGEMENT AT SIGNIFICANT AREA #3

SCALE: 1" = 30' - 0"

EXHIBIT A-8.3
CONCEPTUAL ENLARGED LANDSCAPE PLAN

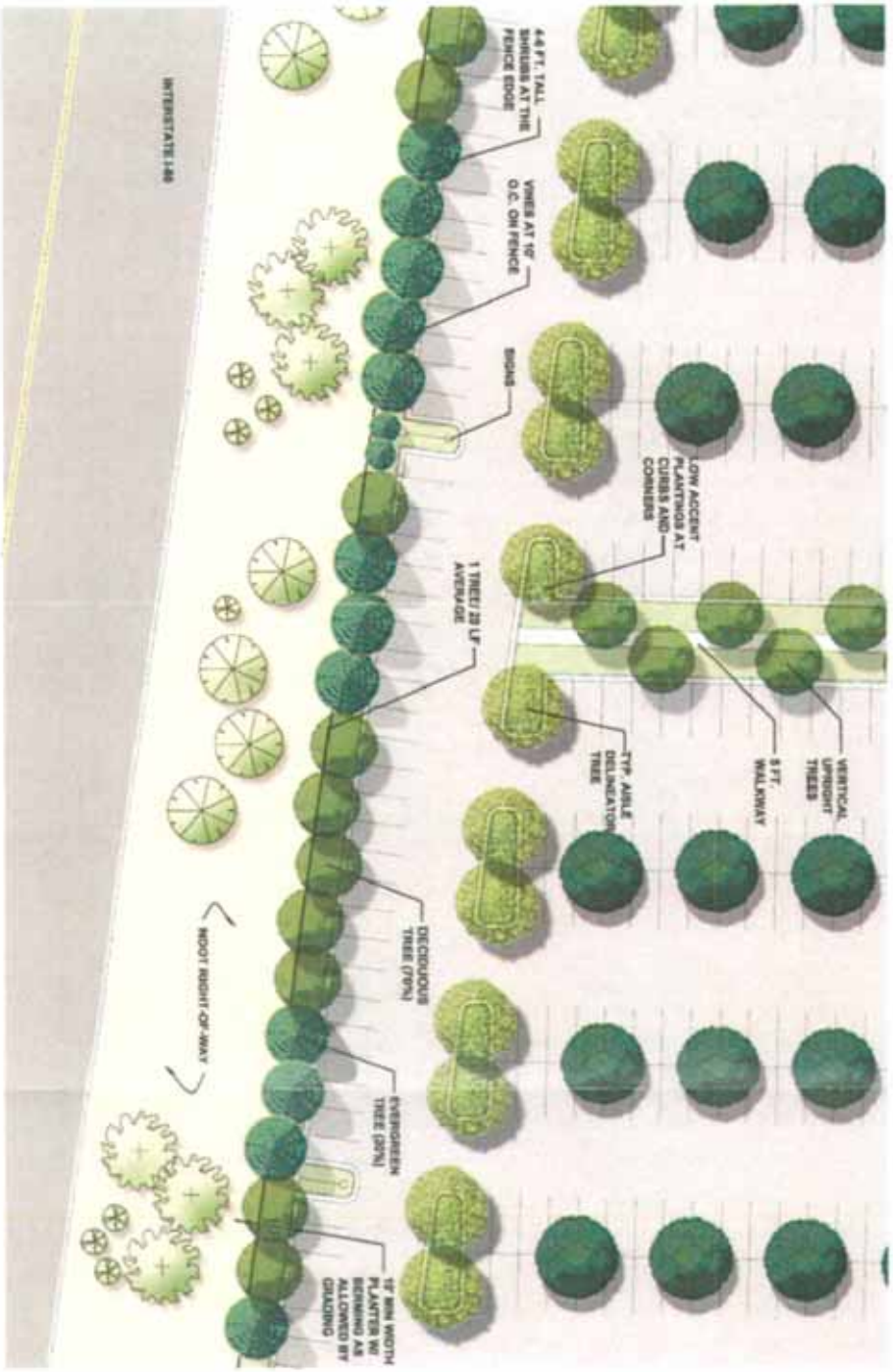
THE LEGENDS AT SPARKS MARINA

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-8.4
CONCEPTUAL ENLARGED LANDSCAPE PLAN

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-8.5
CONCEPTUAL ENLARGED LANDSCAPE PLAN

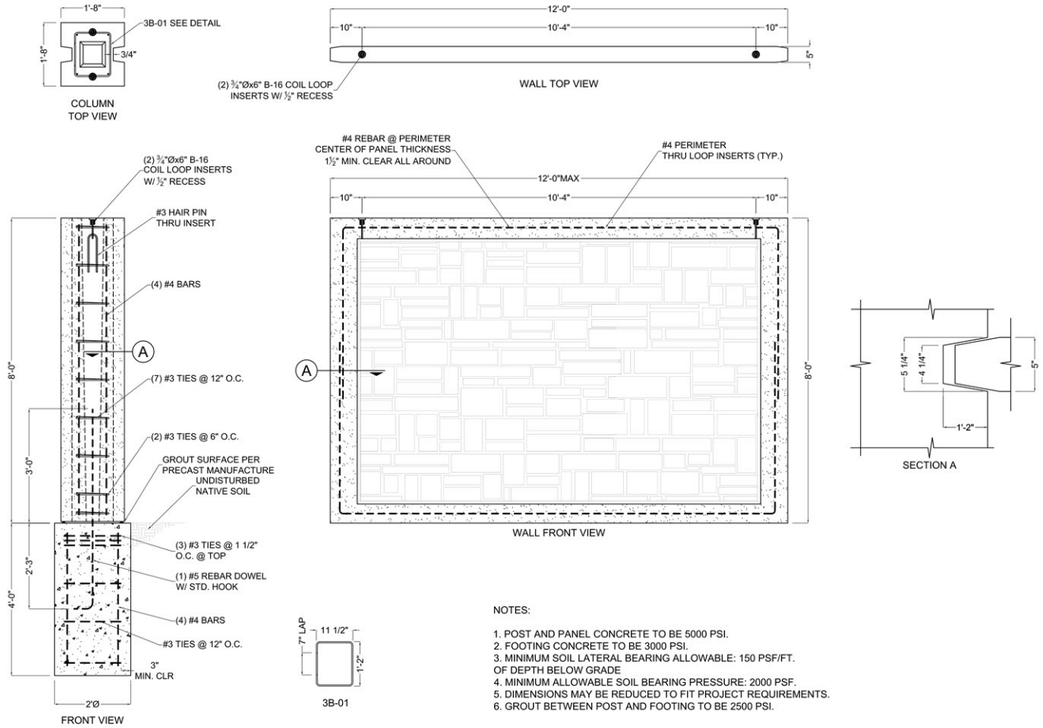


ENLARGEMENT AT SIGNIFICANT AREA #6

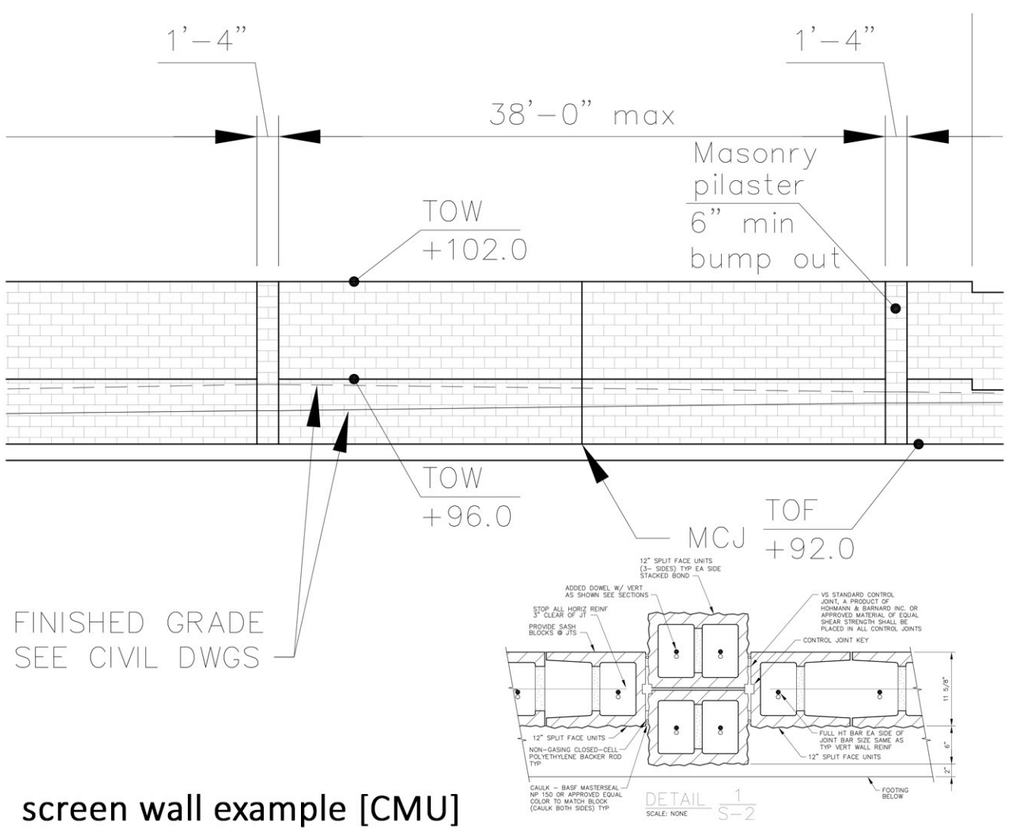
SCALE: 1" = 30' - 0"

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-8.6
CONCEPTUAL ENLARGED
LANDSCAPE PLAN



screen wall example [precast concrete]



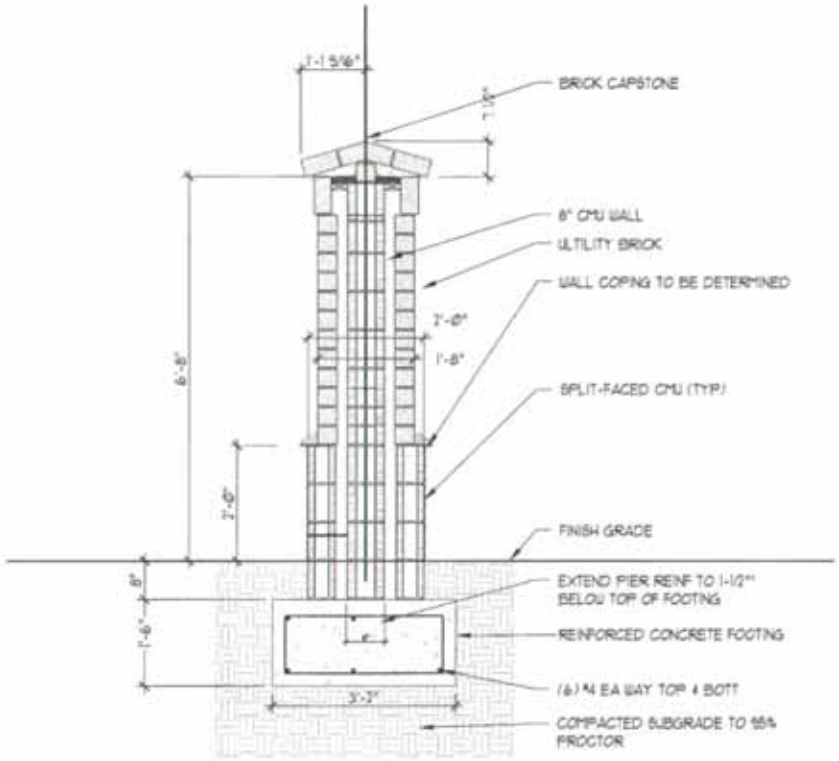
screen wall example [CMU]

EXHIBIT A-9.1
CONCEPTUAL
BUFFER WALL

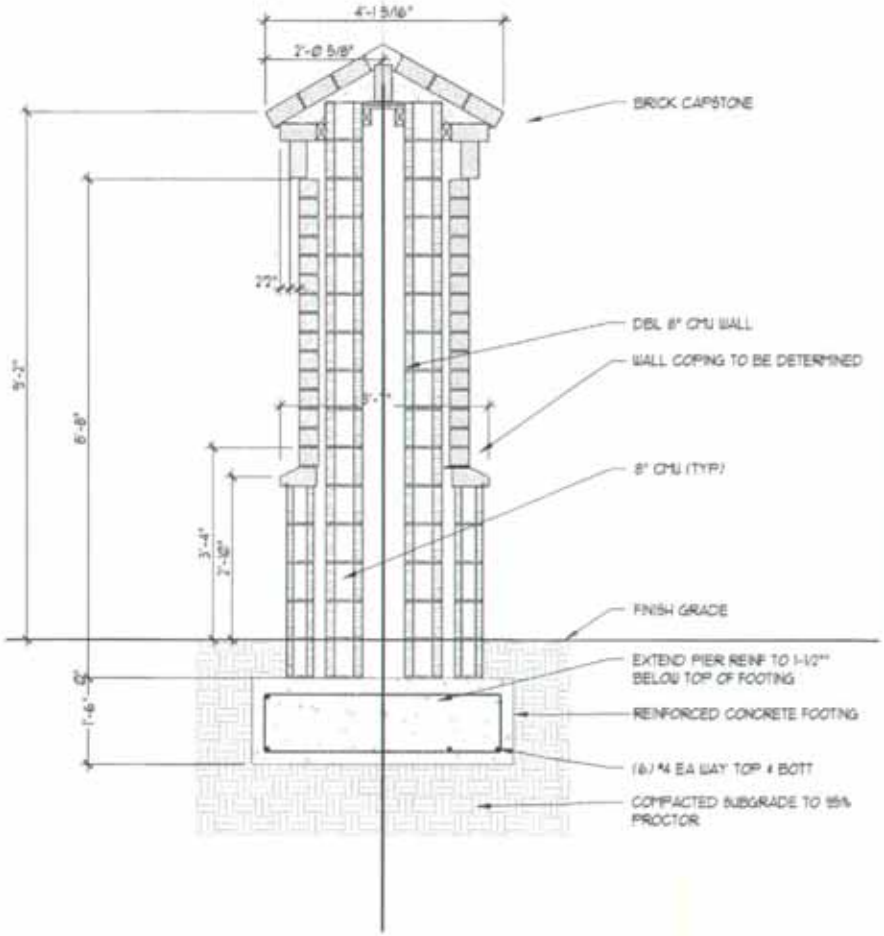
THE LEGENDS AT SPARKS MARINA

Revised 12/02/19

EXHIBIT A-9.2
CONCEPTUAL
BUFFER WALL



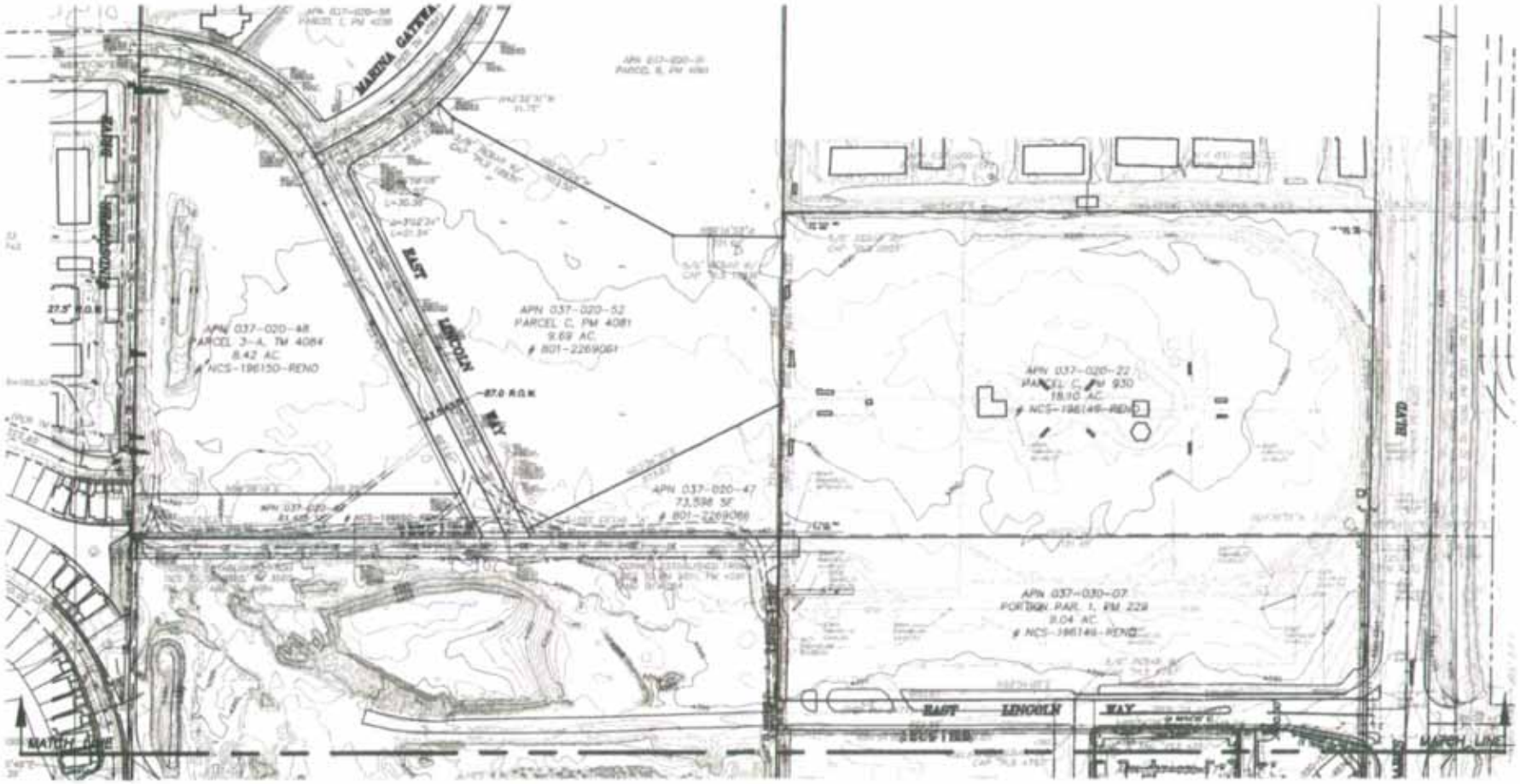
1 / A1-01 SECT THRU WALL
SCALE: 1/2" = 1'-0"



2 / A1-01 SECT THRU PILASTER
SCALE: 1/2" = 1'-0"

THE LEGENDS AT SPARKS MARINA


COURTYARD SIGNS NOT REGULATED BY THE CITY OF SPARKS
Hotel/Condo/Health Club 140,000 sf
BASEBALL STADIUM
LINCOLN WA
Pad 13 7,200 sf
Pad 4 7,200 sf
Retail E 67,245 sf
Retail F 121,075 sf 2 story
Retail G 186,845 sf 2 story
Retail H 74,290 sf
Retail D 33,850 sf
Retail C 44,525 sf
Retail B 17,675 sf
Retail I 21,875 sf
Retail A 225,000 sf 2 Story
Pad 3 5,500
Pad 2 7,200 sf
Pad 1 7,200 sf
Retail M 175,000 sf
Retail L 98,800 sf
Retail K 128,000 sf
Retail J 30,000 sf
EXHIBIT A-10
CONCEPTUAL
COURTYARD SIGN PLAN
THE LEGENDS AT SPARKS MARINA



LINE	BEARING	DISTANCE
L1	N 78°10'28" W	196.24
L2	S 89°34'05" W	80.34
L3	S 89°34'05" W	30.00
L4	N 26°23'29" W	100.48
L7	S 00°56'42" W	10.81

CURVE	DELTA	RADIUS	LENGTH	CHORD
C3	00°00'03"	308.00	0.01	0.01

CO.1
SITE SURVEY

THE LEGENDS AT SPARKS MARINA

LINE	BEARING	DISTANCE
	N 18°00'24" W	
L8	S 81°41'01" E	21.13
L9	N 38°06'40" E	18.71
L10	N 30°30'37" E	43.37
L11	N 53°26'48" E	186.61
L12	N 53°18'18" E	32.47
L13	N 53°18'18" E	104.88
L14	N 80°38'07" E	65.48

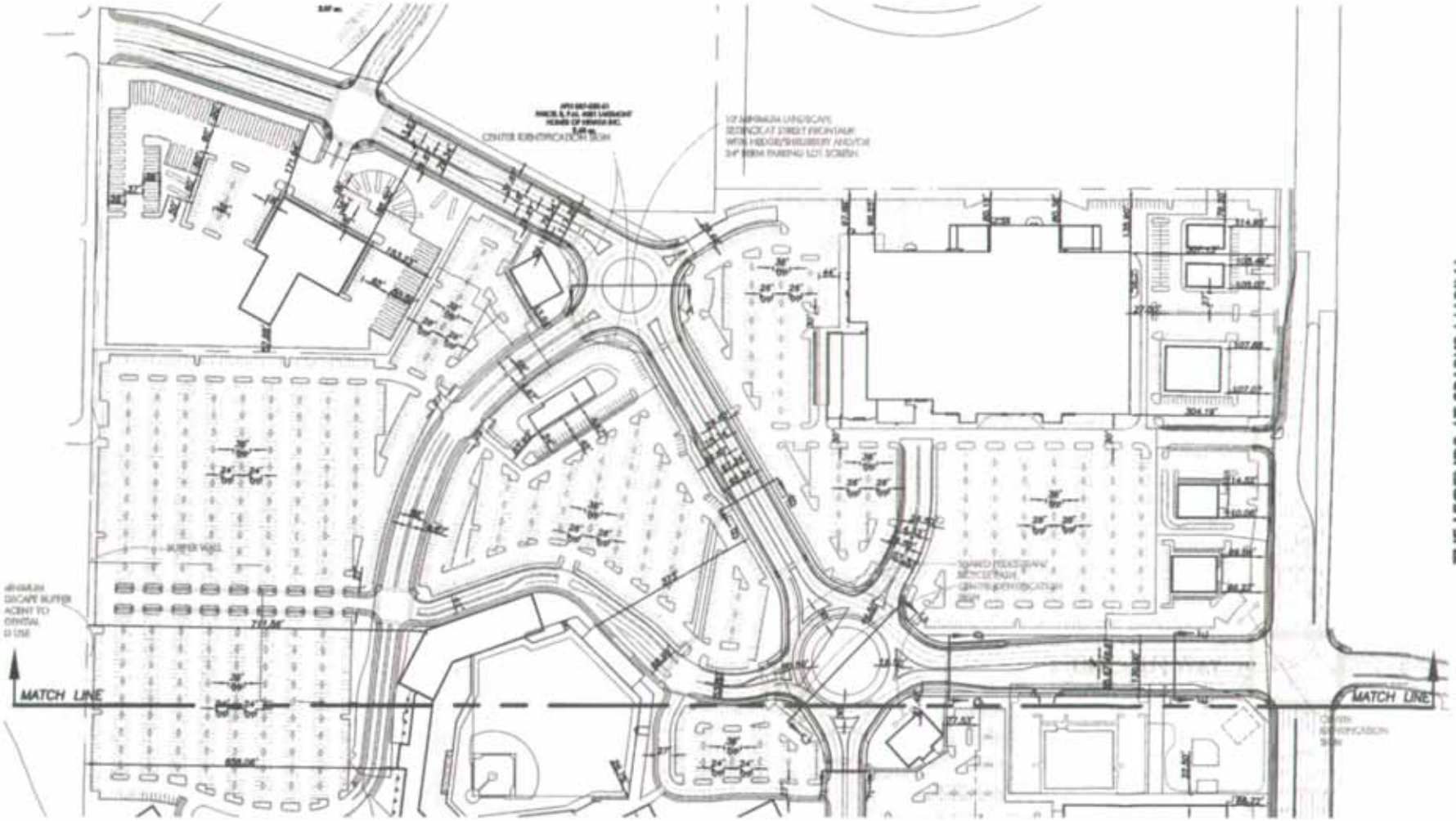
CURVE	DELTA	RADIUS	LENGTH	CHORD
C2	17°11'19"	1080.00	318.00	316.81
C3	00°00'00"	506.00	0.01	0.01

C0.2
SITE SURVEY

THE LEGENDS AT SPARKS MARINA

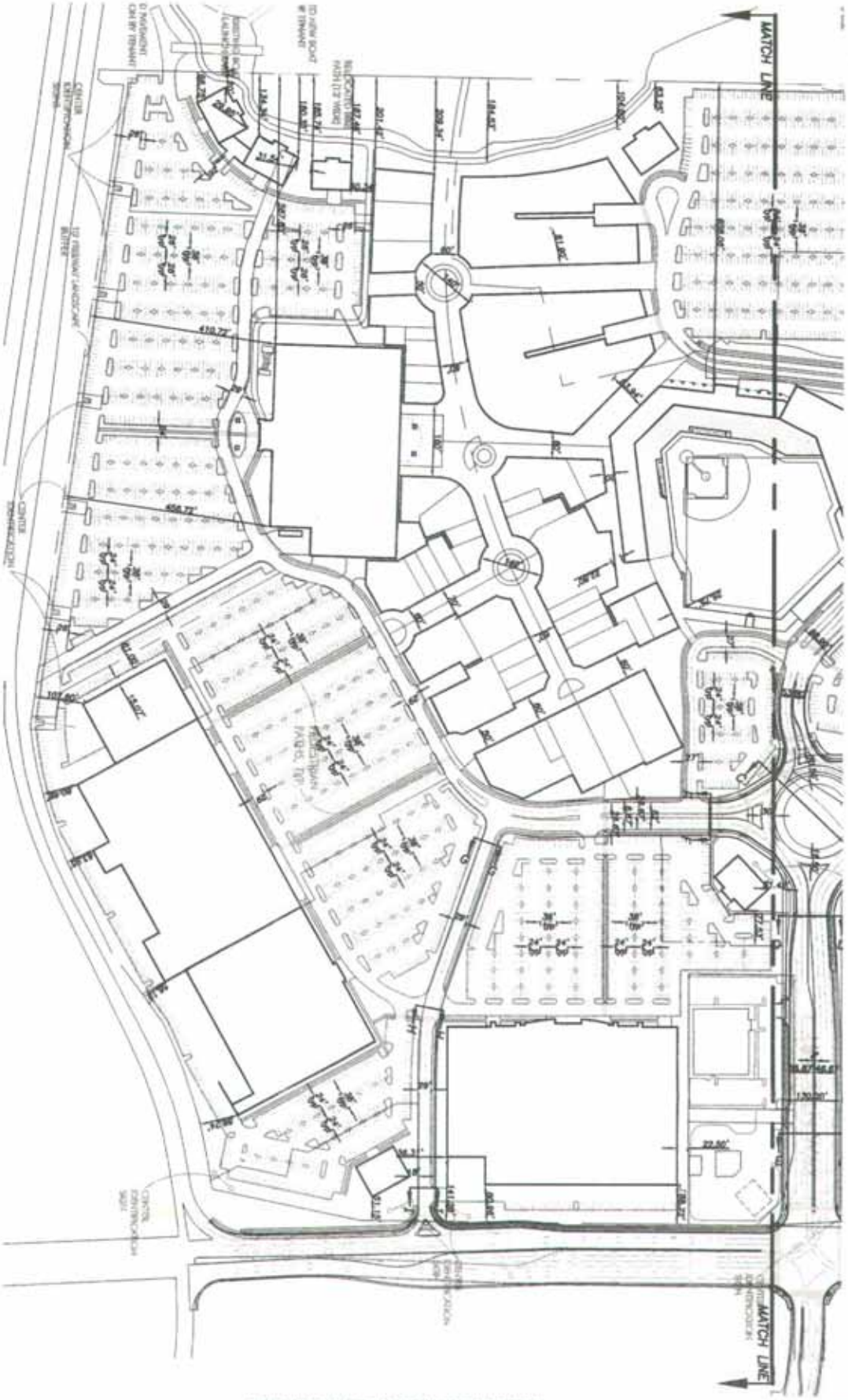
BHC

0 100 200



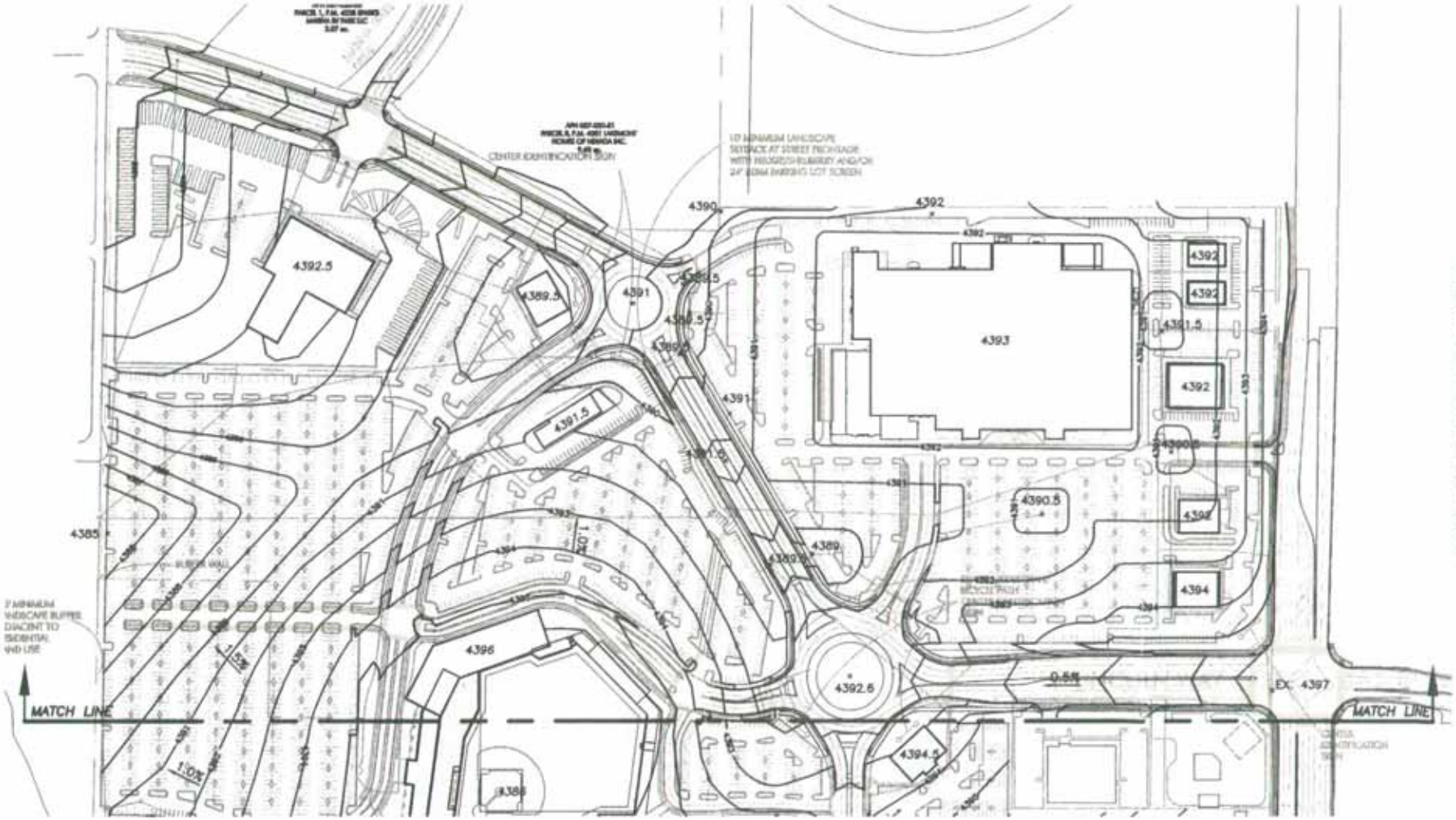
THE LEGENDS AT SPARKS MARINA

C1.1
SITE DIMENSION PLAN



THE LEGENDS AT SPARKS MARINA

C1.2
SITE DIMENSION PLAN



THE LEGENDS AT SPARKS MARINA

C2.1
SITE GRADING PLAN

CL2

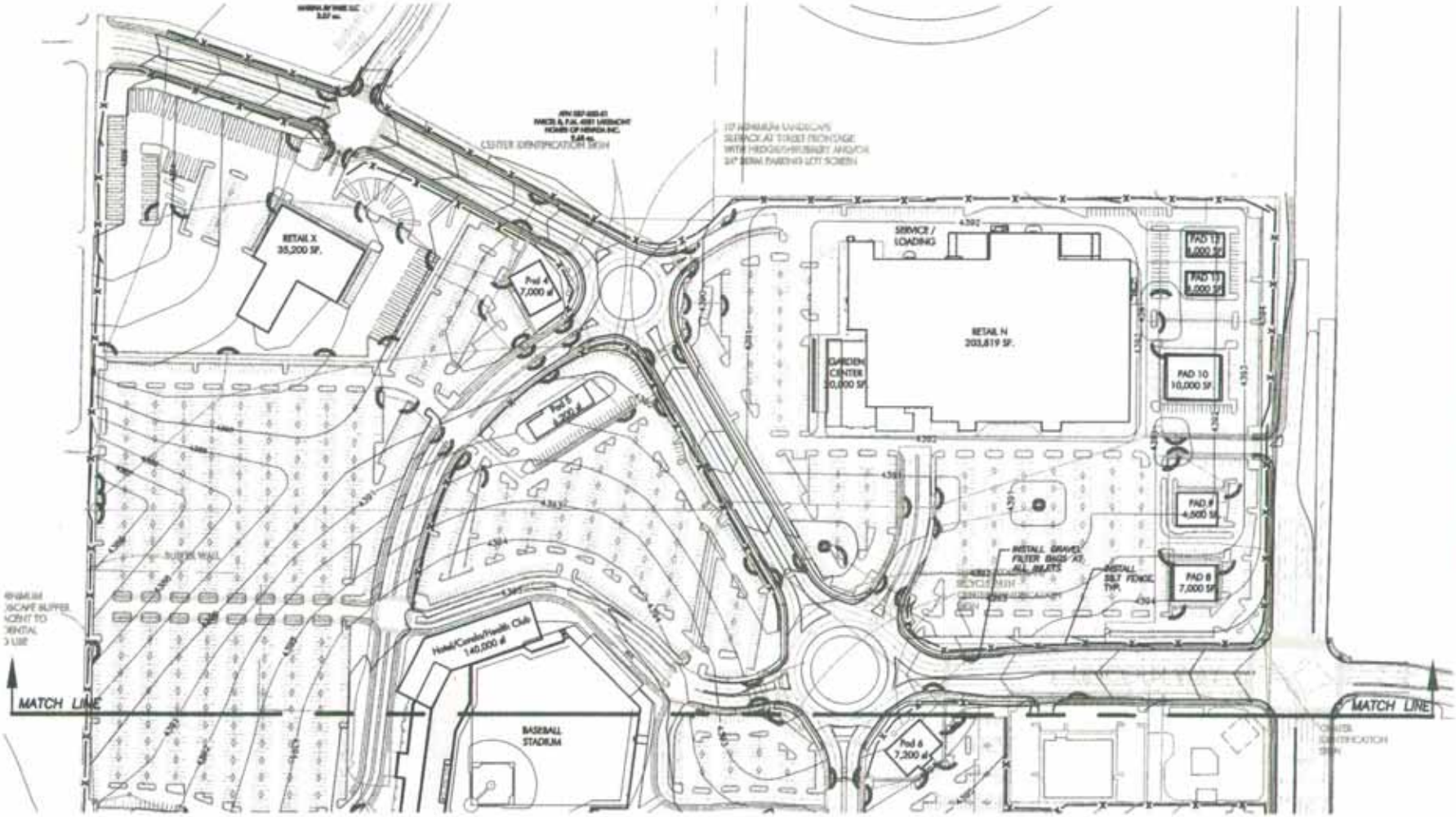
SITE GRADING PLAN

THE LEGENDS AT SPARKS MARINA

BHC

Berger Schatz Hunger

FLEJ



CE.1
SITE EROSION
CONTROL PLAN

THE LEGENDS AT SPARKS MARINA

BHC

P.E.D.

CL2
SITE EROSION CONTROL PLAN

THE LEGENDS AT SPARKS MARINA

BHC

Berger Davis Singer

TIED

PLANNED DEVELOPMENT HANDBOOK

DRAINAGE REPORT

FOR

LEGENDS AT SPARKS MARINA

Prepared for:

SPARKS LEGENDS DEVELOPMENT, INC.
4717 CENTRAL
KANSAS CITY, MISSOURI 64112
PHN 816-777-3500
FAX 816-777-3501

BY

BHC RHODES
CIVIL ENGINEERS • SURVEYORS

BRUNGARDT HONOMICHL & COMPANY, P.A.
10895 GRANDVIEW, SUITE 150
OVERLAND PARK, KANSAS 66210
PHN 913-663-1900 FAX 913-663-1633

SEPTEMBER 15, 2006

Background

The Legends at Sparks Marina is a destination-type development consisting of a mix of retail, restaurants, and a baseball stadium located on approximately 152 acres of property in Sparks, Nevada. The site is generally bounded on the west by the Sparks Marina, on the south by I-80, on the east by Sparks Blvd., and on the north by the existing/proposed Lincoln Way and adjacent development. The map below shows the general site location.

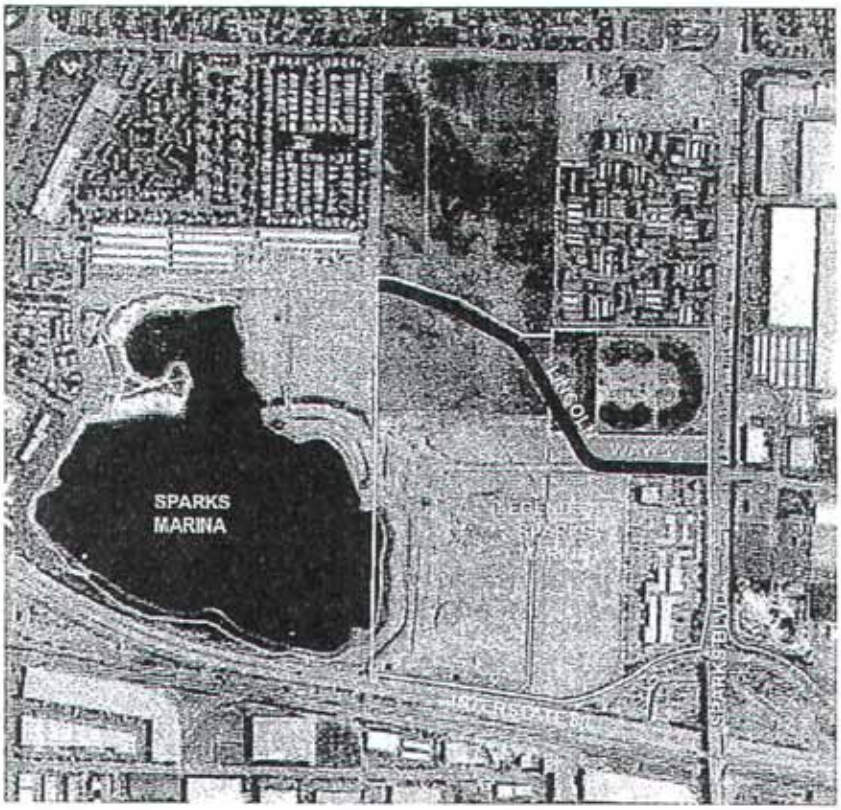


Figure 1 - General Site Location

Existing Conditions

The site is currently partially occupied by a vacant outlet mall, existing gas station, health club, and the Don Mello Sports Complex on the eastern portion of the site along Sparks Blvd. The remainder of the site is undeveloped and has been used as a depository for construction debris, equipment, and other non-household waste. The terrain is typically flat will drainage flowing west into Sparks Marina or south thru a culvert under I-80. All runoff eventually reaches the Truckee River.

A Letter of Map Revision (LOMR) has recently been filed and approved to amend the FEMA Flood Insurance Rate Map in the vicinity of the site. The LOMR indicates that the proposed site no longer lies within a Special Flood Hazard Area.

Proposed Conditions

The proposed Legends at Sparks Marina development will provide approximately 1,671,999 square feet of retail, entertainment, and restaurant space and a 5,788 seat minor-league baseball stadium. The remainder of the site will consist of parking for vehicles, pedestrian walkways, landscaping, and other amenities. See the site plan on next page for more information on the site layout.

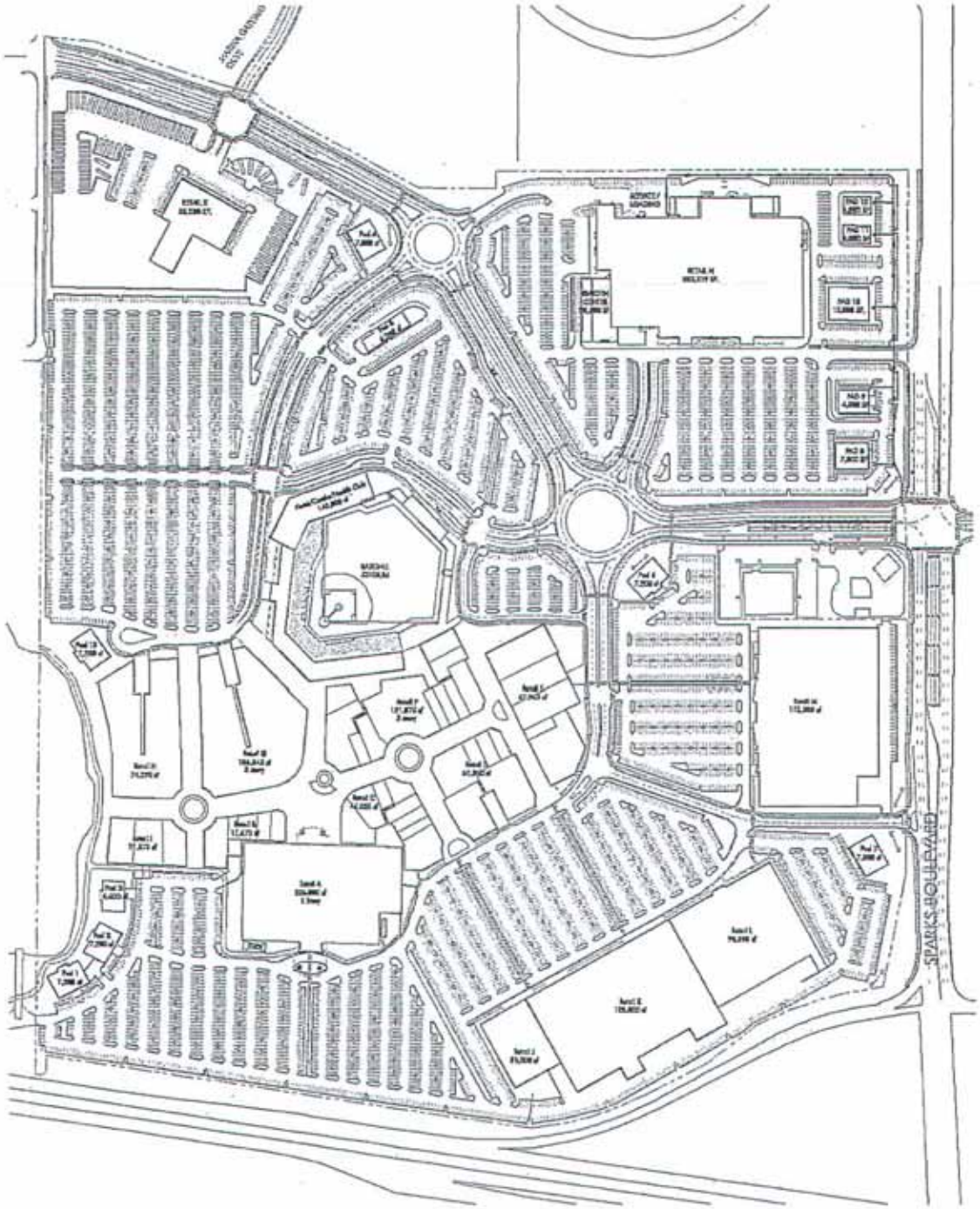


Figure 2 - Site plan

The entire site will be served by underground storm sewer designed to accommodate the 100-year design storm. Runoff from a majority of the site will be collected and discharged into Sparks Marina. Certain portions of the site will flow south into a box culvert that flows under Interstate 80. In total the proposed site would have 7 main storm sewer lines constructed with RCP pipe conveying runoff. The proposed site was delineated into drainage areas based upon this proposed storm sewer layout. A map of the proposed storm sewer layout and drainage areas for each of the main storm sewer lines is on the next page.

Given their relatively small areas and proposed ground cover, all drainage areas except for buildings were assumed to have a runoff coefficient ("C") of 0.85 as is typical for commercial districts (buildings were given a coefficient of 0.90). For the same reasons, the time of concentration was assumed to be 10 minutes for all drainage areas as well. These assumptions provide a conservative design for the proposed storm sewer system.

With the proposed discharge of runoff from a majority of the site directly into Sparks Marina, no stormwater detention facilities are proposed as part of the Legends at Sparks Marina project.

Stormceptor, Vortechnics, or other similar devices, will be installed along the main storm sewer lines and near the outfalls into Sparks Marina to address water quality issues. These structures typically collect a significant portion of floatables (trash), suspended particles (dirt), and petroleum products (oil) transported into the storm sewer system, especially with "first flush" rainfall events. These structures will be sized appropriately to remove contaminants in the more frequent rainfall events (up to 2-year storms) while providing for adequate bypass capacity for the more intense storm events. Inlet structure locations and pipe sizing will be finalized as site design is being completed.

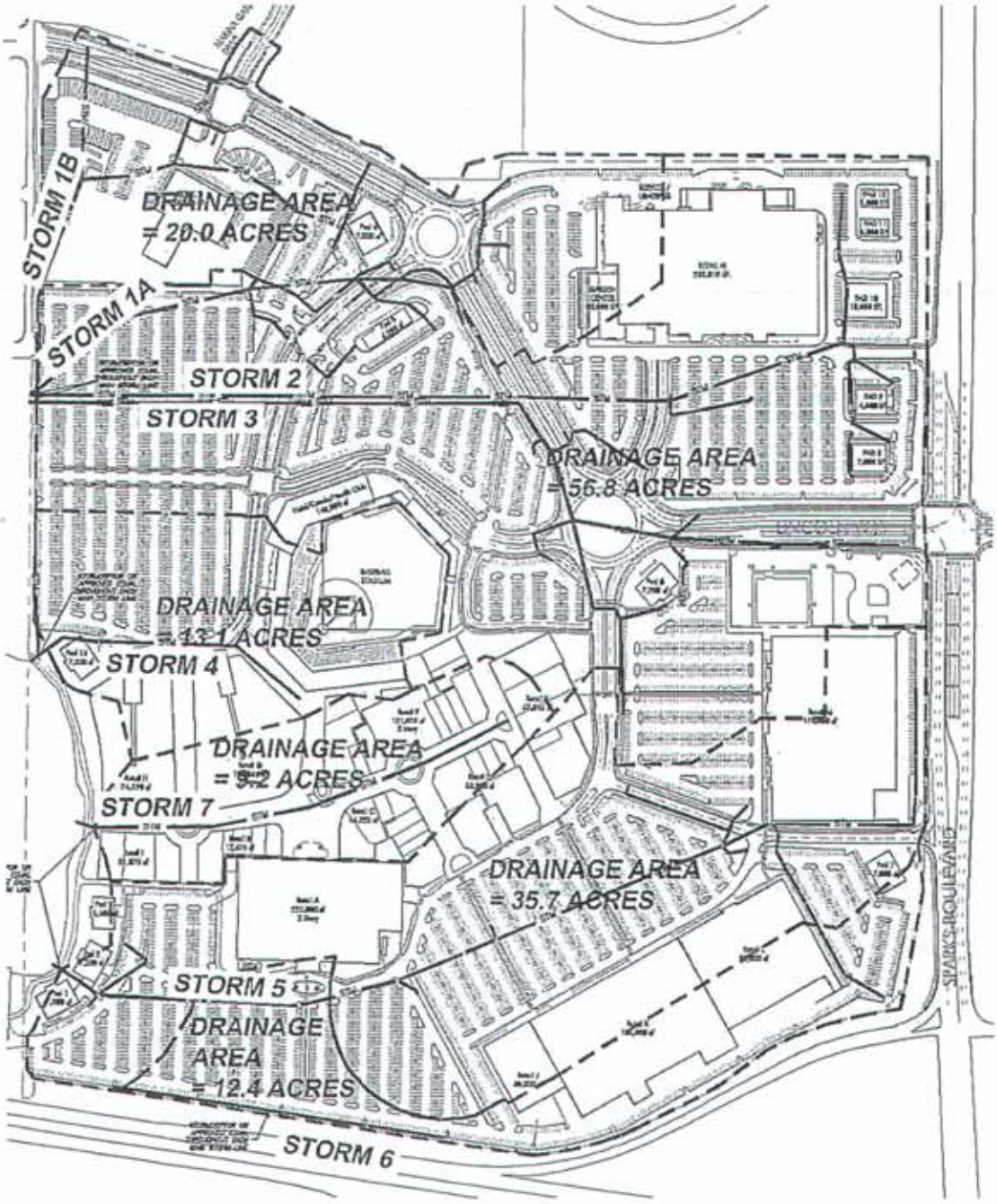


Figure 3 –Major Storm Sewer Lines and Drainage Areas

Methodology

Once drainage areas were delineated peak runoff from each area was calculated using the Rational Method (Q=CiA), where "C" is the runoff coefficient, "i" is rainfall intensity in inches per hour, and "A" is drainage area in acres. As stated, C values were assumed to be 0.85 (0.90 for buildings). Time of concentration is typically calculated as the greater of 10 minutes or [L/(Vx60)], where L is the length of watershed in feet and V is overland flow velocity in feet per second. Again, to be conservative, all drainage areas were assumed to have a time of concentration of 10 minutes. Rainfall intensity was determined using the IDF curve for the City of Reno, Nevada. The intensity values for a time of concentration of 10 minutes were estimated to be 1.40 inches per hour for the 5-year rainfall event and 3.55 inches per hour for the 100-year rainfall event. Peak runoff rates were calculated using a spreadsheet.

Storm sewer system line capacities, velocities, and hydraulic grade lines were determined using Hydraflow Storm Sewers 2005 software. Summaries of storm sewer modeling results for the 100-year event (the most conservative case required) can be found at the back of the report.

Conclusion

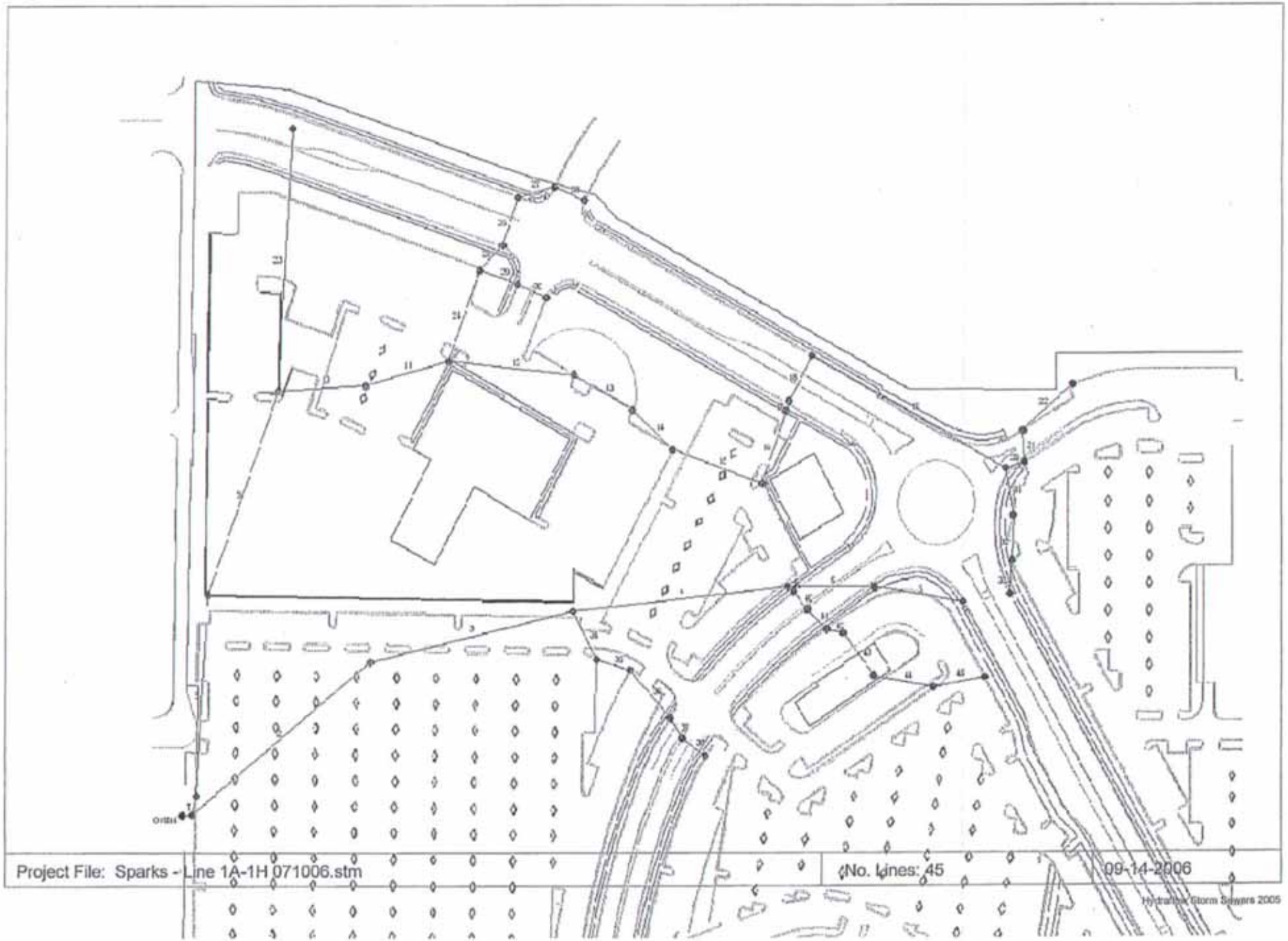
The proposed drainage plan for the Legends at Sparks Marina will provide adequate underground conveyance to collect runoff and transport it to the Sparks Marina or existing box culvert. Water quality devices will be installed as part of the proposed direct outfalls into the Sparks Marina and the Truckee River to help protect these bodies of water. The proposed development will meet the standards and criteria set forth by the City of Sparks.

LEGENDS AT SPARKS MARINA

STORM SEWER MODELING RESULTS

FOR Q_{100}

Hydraf. v Plan View



Project File: Sparks - Line 1A-1H 071006.stm | No. Lines: 45 | 09-14-2006

Hydraflow Storm Sewers 2005

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		215.6	68 x 106 e	14.3	4378.50	4378.54	0.281	4383.60	4383.64	0.51	4384.15	End
2		66.43	68 x 106 e	341.3	4378.54	4379.35	0.237	4384.67	4384.73	0.02	4384.76	1
3		56.70	48 x 76 e	313.1	4381.02	4381.79	0.246	4384.80	4384.99	0.17	4385.16	2
4		39.27	48 x 76 e	324.3	4381.79	4382.59	0.247	4385.33	4385.39	0.08	4385.47	3
5		1.55	19 x 30 e	132.2	4385.00	4385.33	0.250	4385.58	4385.73	n/a	4385.73	4
6		1.55	14 x 23 e	135.1	4385.73	4386.07	0.251	4386.02*	4387.33*	0.01	4387.34	5
7		149.2	68 x 106 e	26.2	4378.54	4378.61	0.267	4384.67*	4384.69*	0.05	4384.73	1
8		149.2	68 x 106 e	279.7	4378.61	4379.30	0.247	4384.96*	4385.13*	0.08	4385.21	7
9		148.9	63 x 98 e	301.5	4379.72	4380.46	0.245	4385.43	4385.71	0.28	4385.99	8
10		136.9	63 x 98 e	130.2	4380.46	4380.79	0.254	4386.30*	4386.40*	0.06	4386.46	9
11		136.9	63 x 98 e	131.1	4380.79	4381.11	0.244	4386.72*	4386.82*	0.22	4387.04	10
12		49.42	58 x 91 e	189.1	4381.52	4381.94	0.222	4387.30*	4387.33*	0.02	4387.35	11
13		47.19	58 x 91 e	100.0	4381.94	4382.16	0.220	4387.39*	4387.41*	0.01	4387.42	12
14		47.19	58 x 91 e	80.8	4382.16	4382.33	0.210	4387.46*	4387.47*	0.02	4387.49	13
15		44.96	58 x 91 e	143.3	4382.33	4382.65	0.223	4387.53*	4387.55*	0.04	4387.59	14
16		40.74	58 x 91 e	104.9	4382.65	4382.88	0.219	4387.63	4387.71	0.00	4387.72	15
17		40.74	58 x 91 e	14.3	4382.88	4382.91	0.212	4387.75*	4387.75*	0.01	4387.76	16
18		27.61	24 x 92 b	72.2	4382.91	4383.06	0.208	4387.76*	4387.79*	0.05	4387.84	17
19		18.86	34 x 53 e	332.2	4384.16	4384.88	0.217	4387.89*	4388.02*	0.05	4388.07	18
20		5.92	24 x 38 e	28.8	4385.71	4385.82	0.381	4388.12*	4388.13*	0.02	4388.15	19
21		3.09	19 x 30 e	43.8	4386.24	4386.35	0.252	4388.17*	4388.18*	0.01	4388.20	20
22		3.09	14 x 23 e	98.8	4387.07	4387.31	0.244	4388.21	4388.30	0.06	4388.36	21
23		10.50	33 c	363.4	4385.28	4388.85	0.983	4386.25	4389.91	n/a	4389.91 j	9
24		84.89	53 x 83 e	132.5	4381.94	4382.24	0.227	4387.30*	4387.40*	0.19	4387.59	11
25		77.67	53 x 83 e	49.2	4382.24	4382.35	0.223	4387.79*	4387.82*	0.08	4387.90	24
26		76.24	53 x 83 e	70.0	4382.35	4382.51	0.228	4388.06*	4388.10*	0.14	4388.24	25
27		74.81	53 x 83 e	56.5	4382.51	4382.64	0.231	4388.39*	4388.43*	0.10	4388.53	26
28		1.21	14 x 23 e	48.9	4385.83	4385.95	0.246	4388.68*	4388.68*	0.01	4388.69	27
29		2.54	14 x 23 e	60.0	4385.46	4385.82	0.600	4387.79*	4387.83*	0.00	4387.83	24
30		2.54	14 x 23 e	46.9	4385.83	4387.22	2.963	4387.87	4387.92	0.08	4388.00	29
31		12.94	29 x 45 e	65.3	4386.16	4386.30	0.214	4388.12	4388.23	0.02	4388.25	19
32		5.43	19 x 30 e	62.9	4387.13	4387.28	0.238	4388.32	4388.39	0.01	4388.40	31

Project File: Sparks - Line 1A-1H 071006.stm | Number of lines: 45 | Run Date: 07-11-2006

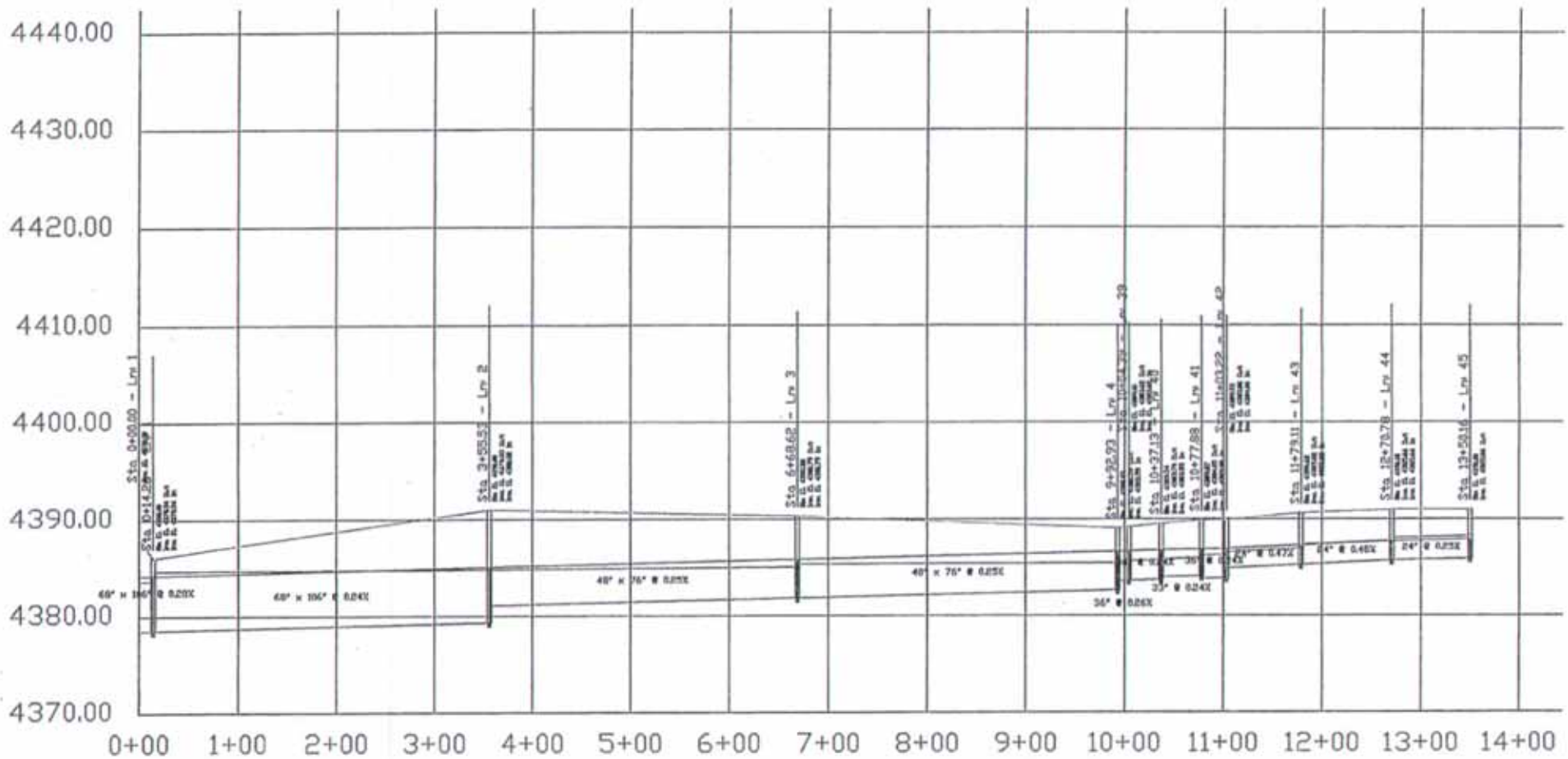
NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown). ; j - Line contains hyd. jump.

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
33		1.02	14 x 23 e	46.3	4387.64	4387.75	0.237	4388.49	4388.51	0.01	4388.52	32
34		14.94	33 c	76.5	4383.04	4383.22	0.236	4385.16	4385.23	0.12	4385.35	3
35		12.87	30 c	52.0	4383.47	4383.59	0.230	4385.35	4385.41	0.10	4385.51	34
36		11.51	30 c	90.3	4383.59	4384.31	0.798	4385.60	4385.59	0.06	4385.65	35
37		7.96	24 c	33.4	4384.81	4385.24	1.290	4385.88	4386.24	n/a	4386.24	36
38		6.15	18 c	43.2	4385.69	4386.50	1.877	4386.45	4387.45	n/a	4387.45 j	37
39		21.01	36 c	11.5	4383.59	4383.62	0.264	4385.47	4385.49	0.09	4385.58	4
40		19.58	36 c	32.7	4383.62	4383.70	0.245	4385.64	4385.68	0.04	4385.72	39
41		18.75	33 c	40.8	4383.95	4384.05	0.244	4385.81	4385.90	0.18	4386.07	40
42		18.75	36 c	25.3	4383.80	4383.86	0.237	4386.22	4386.24	0.11	4386.34	41
43		14.60	24 c	75.9	4384.86	4385.22	0.475	4386.52	4386.88	0.31	4387.19	42
44		14.60	24 c	91.7	4385.22	4385.66	0.480	4387.28*	4387.72*	0.13	4387.85	43
45		9.66	24 c	79.4	4385.66	4385.86	0.252	4388.04*	4388.21*	0.15	4388.36	44

Project File: Sparks - Line 1A-1H 071006.stm | Number of lines: 45 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown). ; j - Line contains hyd. jump.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14+00

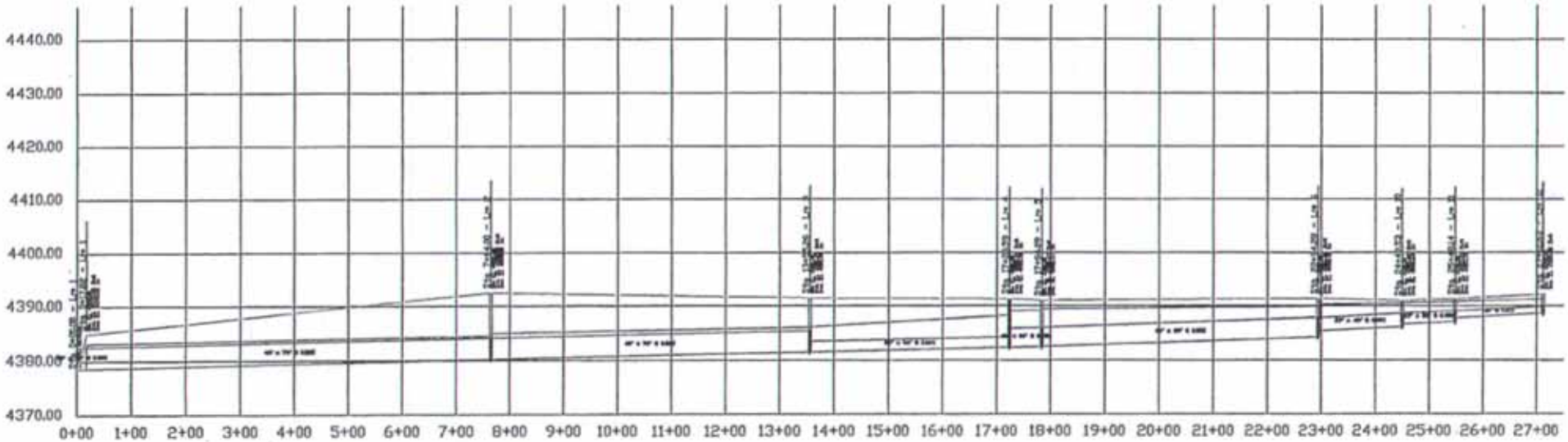
Hydraf. w Plan View

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		103.7	48 x 76 e	17.8	4378.52	4378.56	0.225	4382.52	4382.56	0.21	4382.77	End
2		103.7	48 x 76 e	746.2	4378.56	4380.20	0.220	4383.19*	4384.56*	0.06	4384.63	1
3		103.7	48 x 76 e	591.3	4380.20	4381.49	0.218	4385.05*	4386.14*	0.06	4386.20	2
4		103.7	24 x 92 b	368.3	4381.52	4382.34	0.223	4386.20*	4388.24*	0.11	4388.35	3
5		58.48	43 x 68 e	60.5	4382.32	4382.47	0.249	4389.07*	4389.13*	0.08	4389.20	4
6		57.48	43 x 68 e	510.2	4382.47	4384.26	0.351	4389.41*	4389.93*	0.19	4390.12	5
7		10.30	43 x 68 e	31.7	4384.26	4384.54	0.885	4390.33*	4390.33*	0.01	4390.33	6
8		10.30	29 x 45 e	7.3	4385.71	4385.80	1.221	4390.34*	4390.34*	0.01	4390.36	7
9		6.64	29 x 45 e	33.3	4385.80	4386.13	0.990	4390.39*	4390.39*	0.01	4390.41	8
10		17.68	29 x 45 e	155.3	4385.41	4386.22	0.521	4390.33*	4390.45*	0.04	4390.49	6
11		13.77	24 x 38 e	98.6	4386.63	4387.07	0.447	4390.58*	4390.71*	0.05	4390.76	10
12		3.00	15 c	164.5	4387.78	4388.62	0.511	4390.79*	4391.20*	0.09	4391.29	11

Project File: Sparks - Line 2A-J - 070606.stm | Number of lines: 12 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown).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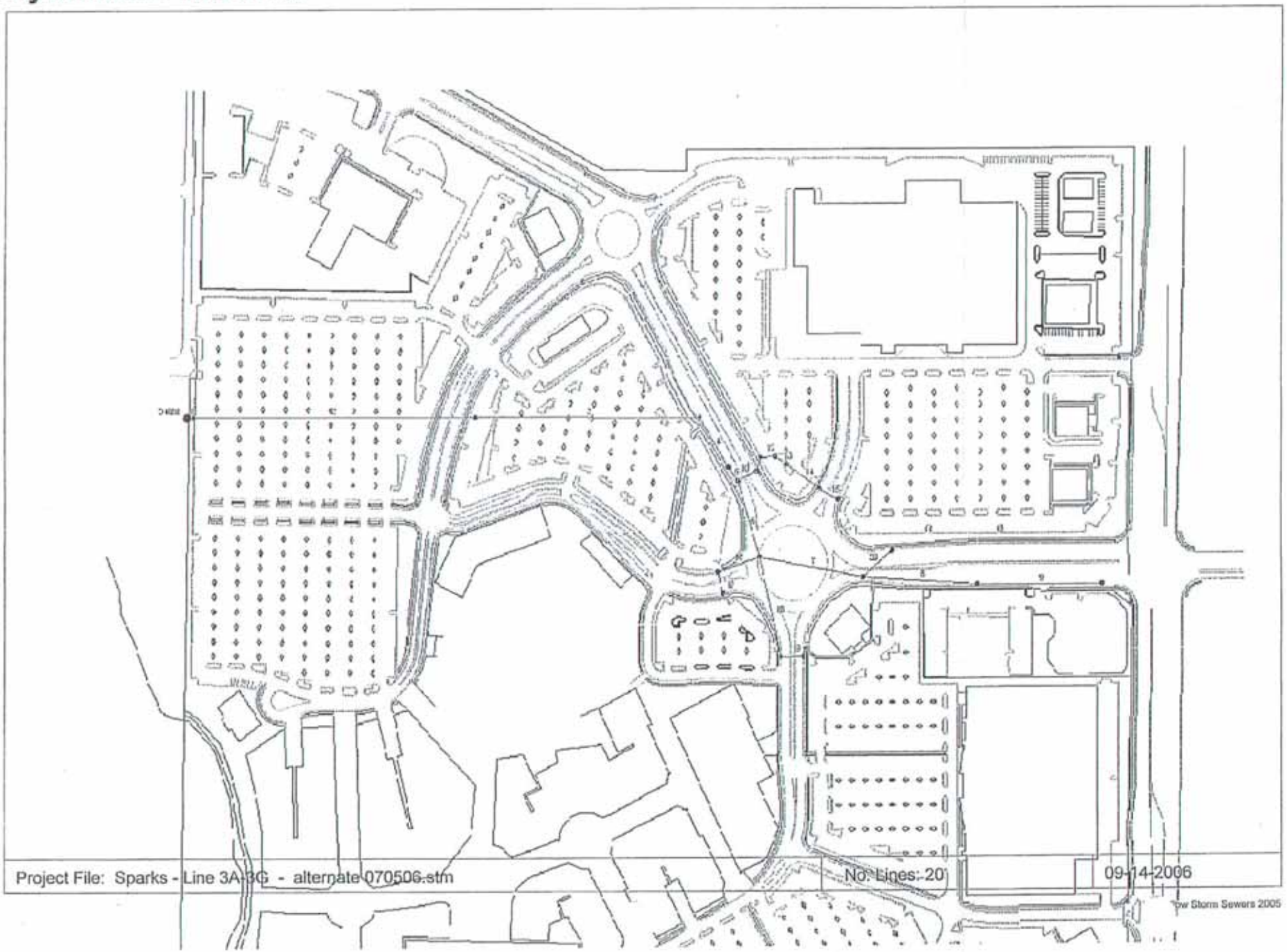
Hydraflow Plan View

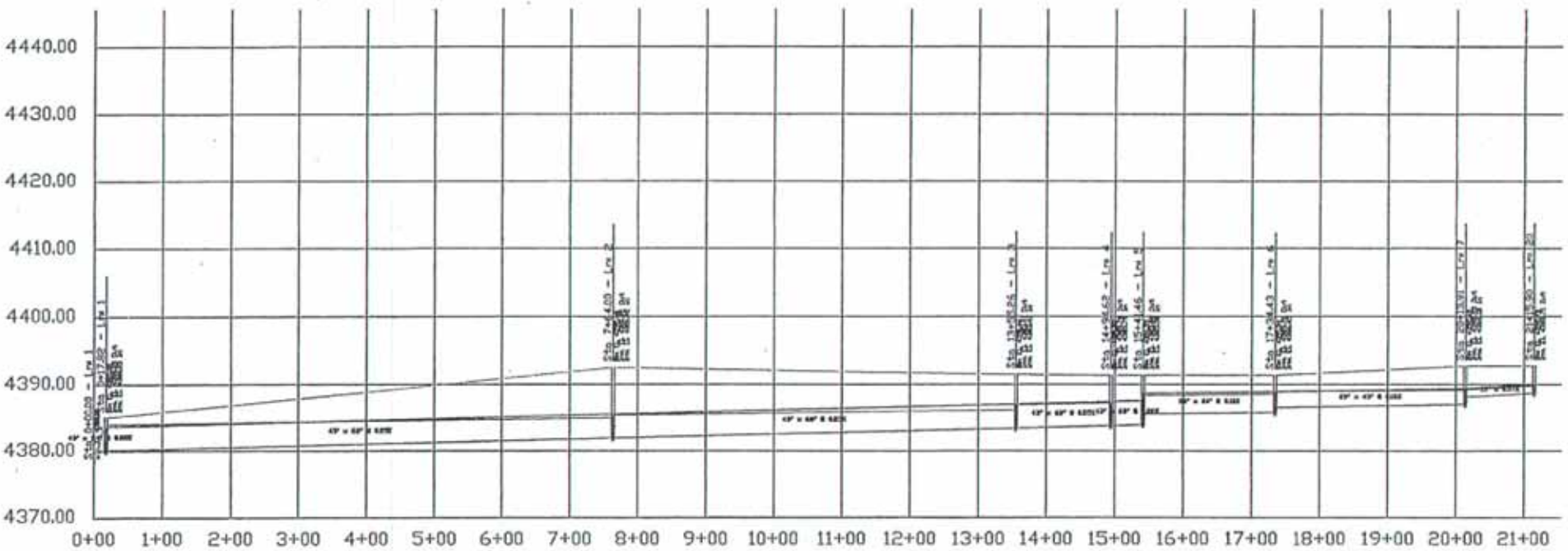


Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		67.72	43 x 54 e	17.8	4380.00	4380.05	0.279	4383.40	4383.45	0.07	4383.52	End
2		67.72	43 x 68 e	746.2	4380.05	4381.92	0.251	4383.99	4384.97	0.05	4385.02	1
3		67.72	43 x 68 e	591.3	4381.92	4383.41	0.252	4385.36	4385.10	0.38	4386.48	2
4		67.72	43 x 68 e	139.4	4383.41	4383.76	0.251	4386.92	4387.16	0.04	4387.21	3
5		63.08	43 x 68 e	46.8	4383.76	4383.88	0.256	4387.50*	4387.56*	0.24	4387.80	4
6		39.56	38 x 60 e	193.0	4385.50	4385.70	0.104	4388.35	4388.55	0.18	4388.72	5
7		20.89	29 x 45 e	279.5	4386.45	4386.95	0.179	4388.90	4389.12	0.12	4389.24	6
8		13.01	24 c	304.1	4387.36	4388.52	0.382	4389.24	4390.25	0.05	4390.30	7
9		11.05	24 c	329.2	4388.54	4389.36	0.249	4390.42	4391.20	0.21	4391.41	8
10		17.11	30 c	55.5	4385.66	4385.80	0.253	4387.80	4387.90	0.18	4388.08	5
11		10.70	24 c	33.7	4386.30	4386.38	0.236	4388.09	4388.16	0.19	4388.36	10
12		8.20	18 c	38.3	4386.88	4387.15	0.706	4388.36	4388.59	0.22	4388.81	11
13		5.70	18 c	36.8	4387.15	4387.28	0.354	4389.00*	4389.12*	0.02	4389.15	12
14		3.20	18 c	104.4	4387.28	4387.54	0.249	4389.26*	4389.37*	0.01	4389.38	13
15		1.60	15 c	55.7	4387.79	4387.95	0.287	4389.40*	4389.44*	0.03	4389.47	14
16		14.15	24 c	115.8	4386.86	4387.38	0.449	4388.72	4389.16	0.36	4389.52	6
17		11.96	24 c	52.2	4387.38	4387.55	0.326	4389.65*	4389.82*	0.23	4390.05	16
18		4.52	15 c	250.4	4387.61	4389.03	0.567	4388.72	4390.05	0.28	4390.32	6
19		2.07	15 c	59.8	4389.03	4389.28	0.418	4390.56*	4390.63*	0.04	4390.67	18
20		4.53	15 c	102.0	4388.13	4388.71	0.569	4389.24	4389.76	0.26	4390.02	7

Project File: Sparks - Line 3A-3G - alternate 070506.stm | Number of lines: 20 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown).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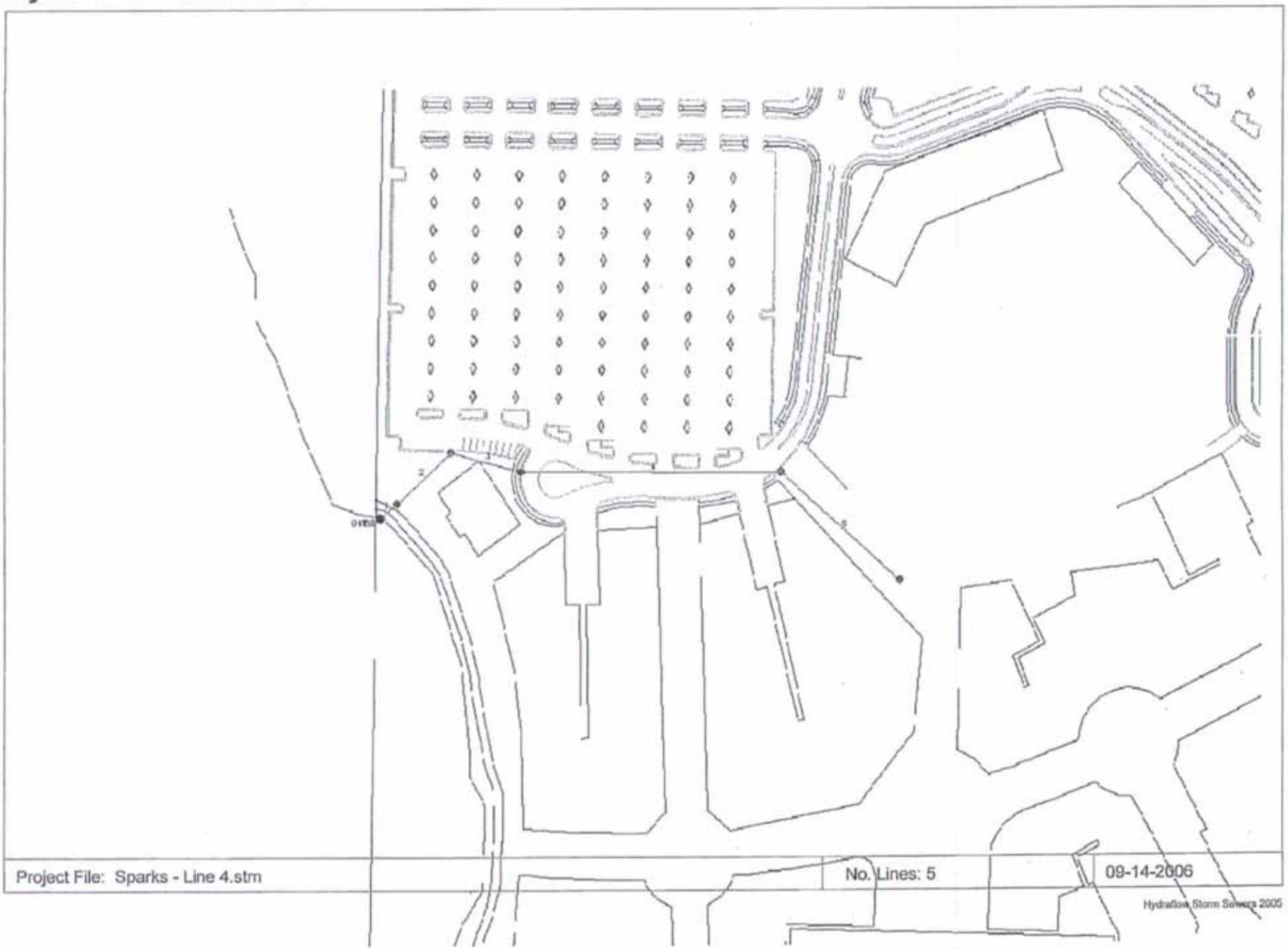
21+00

Hydraf N Plan View

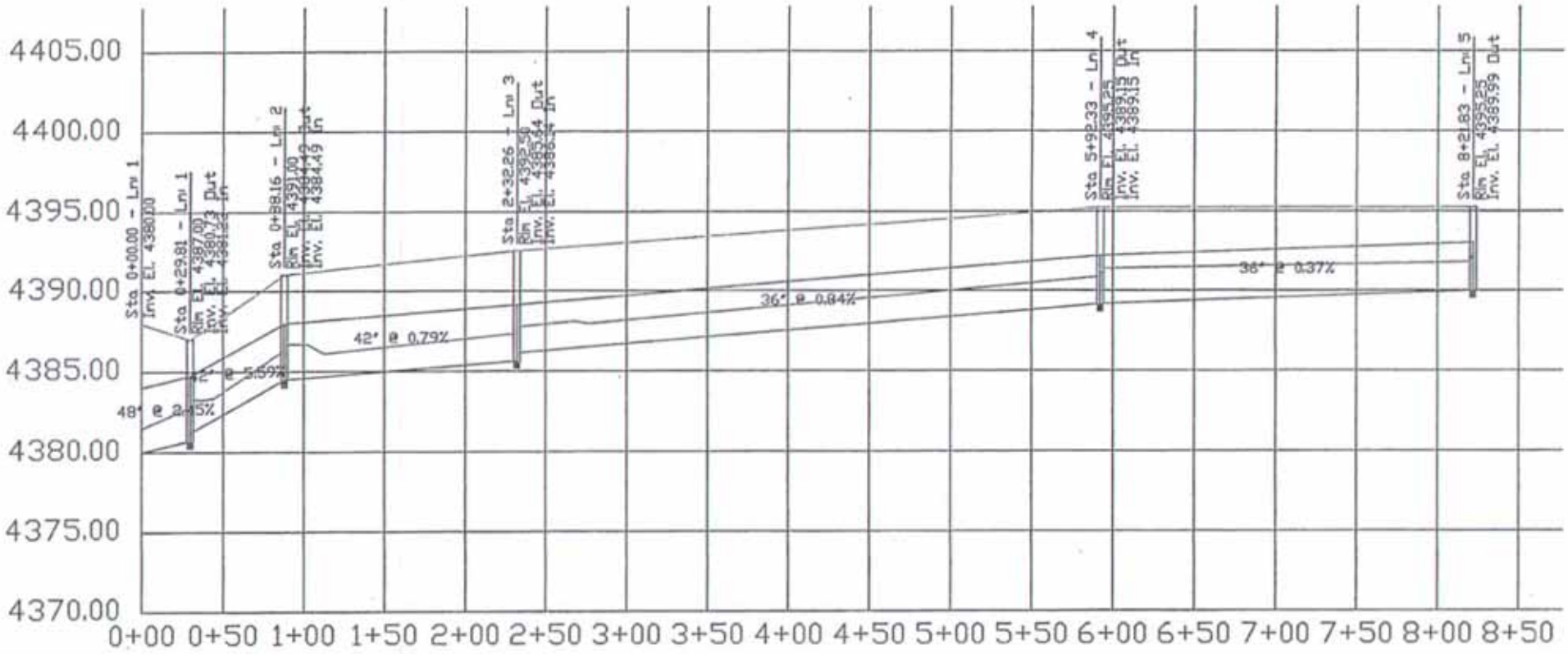


Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		45.69	48 c	29.8	4380.00	4380.73	2.450	4381.48	4382.73	n/a	4382.73	End
2		30.64	42 c	58.4	4381.23	4384.49	5.590	4383.40	4386.19	n/a	4386.19 j	1
3		30.64	42 c	144.1	4384.49	4385.64	0.795	4386.71	4387.33	n/a	4387.33 j	2
4		27.64	36 c	360.1	4386.14	4389.15	0.838	4387.78	4390.83	n/a	4390.83 j	3
5		25.73	36 c	229.5	4389.15	4389.99	0.366	4391.34	4391.75	0.56	4392.31	4

Project File: Sparks - Line 4.stm | Number of lines: 5 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; j - Line contains hyd. jump.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Sta 0+00.00 - Ln 1
Inv. El. 4380.00
Sta 0+29.81 - Ln 1
Rim El. 4387.00
Inv. El. 4380.73 Out
Inv. El. 4381.22 In
Sta 0+88.16 - Ln 2
Rim El. 4391.00
Inv. El. 4384.12 Out
Inv. El. 4384.49 In
Sta 2+32.26 - Ln 3
Rim El. 4392.50
Inv. El. 4385.64 Out
Inv. El. 4386.14 In
Sta 5+92.33 - Ln 4
Rim El. 4394.25
Inv. El. 4389.15 Out
Inv. El. 4389.15 In
Sta 8+21.83 - Ln 5
Rim El. 4395.25
Inv. El. 4389.99 Out
48" @ 2.35%
42" @ 5.59%
42" @ 0.79%
36" @ 0.84%
36" @ 0.37%

Hydraflow Plan View

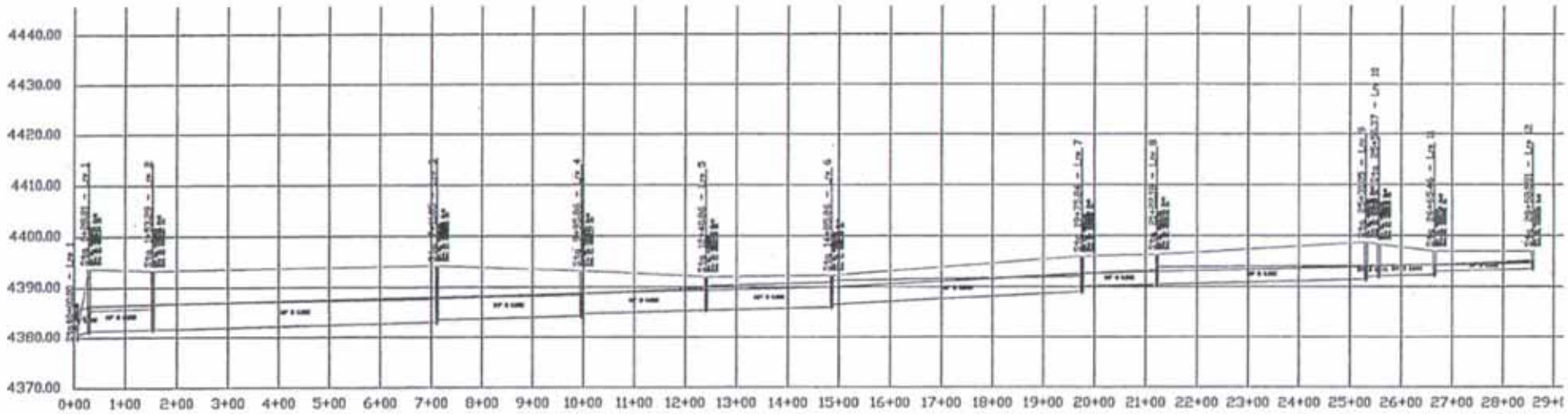
Project File: Sparks - Line 5A.stm	No. Lines: 12	09-14-2006

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		132.3	60 c	28.8	4380.37	4381.34	3.386	4383.58	4384.55	0.23	4384.55	End
2		132.3	60 c	124.5	4381.34	4381.66	0.253	4385.49	4385.80	0.52	4386.32	1
3		114.2	60 c	557.8	4381.67	4383.06	0.249	4386.70	4387.65	0.20	4387.85	2
4		103.4	54 c	284.8	4383.56	4384.27	0.249	4387.85	4388.53	0.12	4388.65	3
5		93.70	48 c	245.0	4384.77	4385.39	0.252	4388.77*	4389.81*	0.13	4389.94	4
6	(2)	75.80	48 c	245.0	4385.39	4386.00	0.249	4390.24*	4390.93*	0.08	4391.01	5
7	(2)	41.90	42 c	490.0	4386.50	4389.09	0.529	4391.28	4392.04	0.05	4392.10	6
8		27.70	30 c	146.3	4390.09	4390.46	0.253	4392.59*	4393.26*	0.21	4393.47	7
9		9.10	30 c	408.9	4390.46	4391.48	0.249	4393.91*	4394.11*	0.05	4394.17	8
10		8.10	24 c	25.3	4391.98	4392.10	0.471	4394.17*	4394.20*	0.02	4394.21	9
11		6.10	24 c	109.1	4392.10	4392.37	0.248	4394.26	4394.33	0.01	4394.34	10
12		4.10	15 c	193.1	4393.01	4393.50	0.252	4394.34*	4395.12*	0.17	4395.30	11

Project File: Sparks - Line 5A.stm | Number of lines: 12 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown).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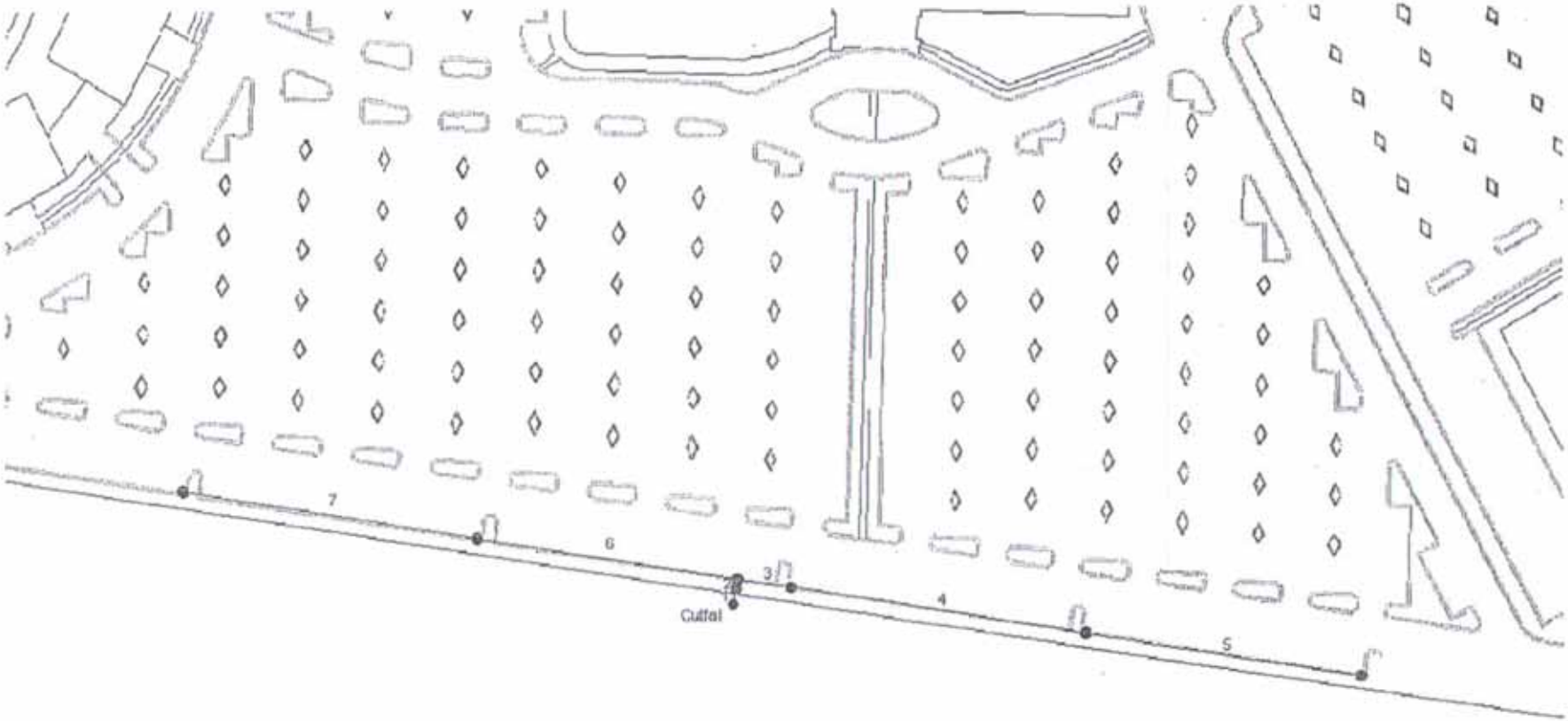
Hydraf N Plan View



Project File: Sparks - Line 6A&B.stm	No. Lines: 7	09-14-2006

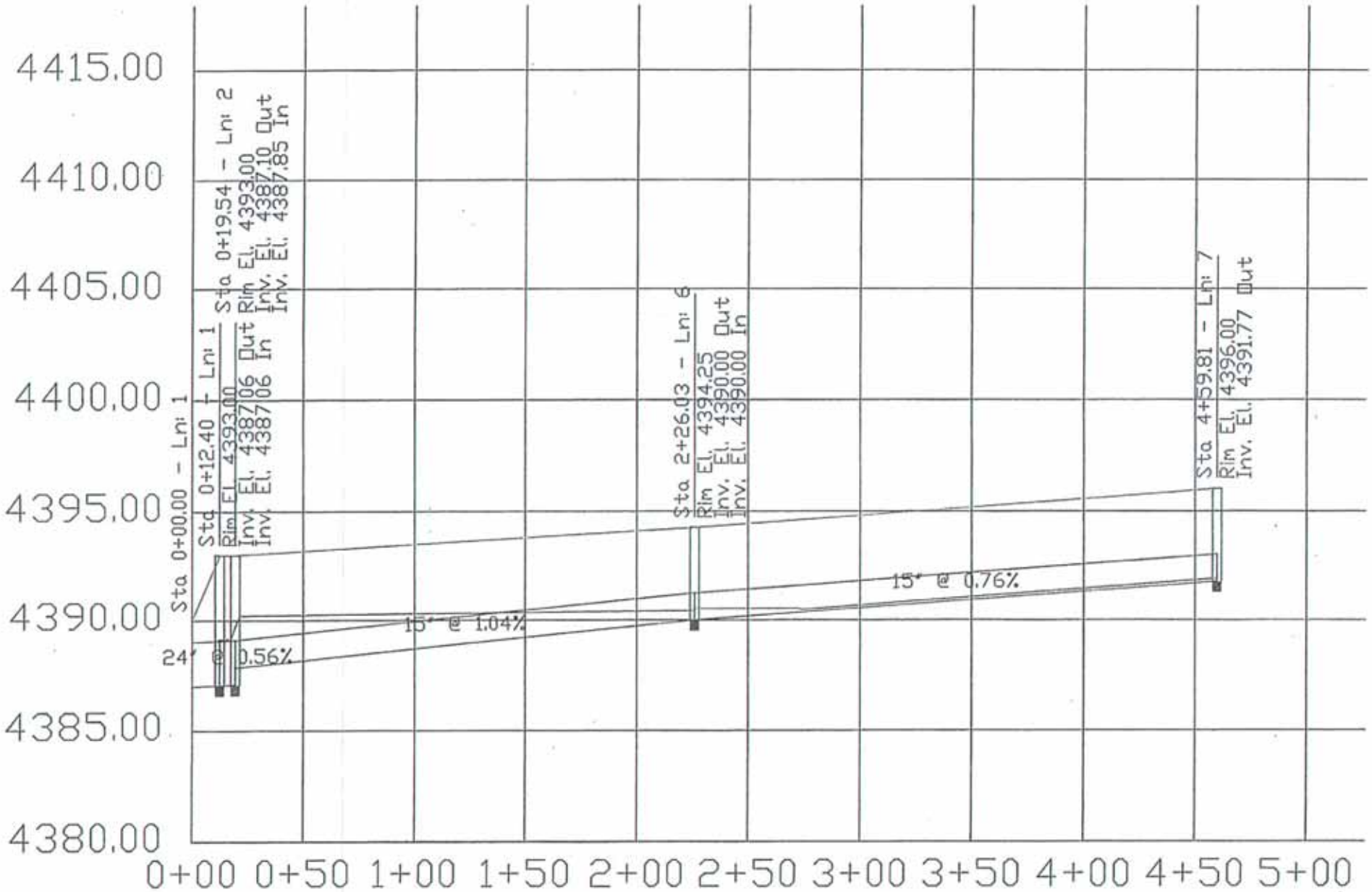
Hydraflow Storm Sewers 2005

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		18.11	24 c	12.4	4387.00	4387.06	0.484	4389.00	4389.06	0.08	4389.14	End
2		18.11	24 c	7.1	4387.06	4387.10	0.561	4389.14*	4389.18*	0.52	4389.70	1
3		4.51	18 c	42.9	4387.85	4388.61	1.762	4390.12*	4390.20*	0.02	4390.21	2
4		1.08	15 c	234.8	4389.31	4390.48	0.501	4390.30	4390.90	0.02	4390.90	3
5		0.10	15 c	219.3	4390.48	4392.00	0.691	4391.04	4392.12	n/a	4392.12 j	4
6		0.85	15 c	206.5	4387.85	4390.00	1.042	4390.21	4390.43	0.01	4390.44	2
7		0.10	15 c	233.8	4390.00	4391.77	0.759	4390.52	4391.90	n/a	4391.90 j	6

Project File: Sparks - Line 6A&B.stm | Number of lines: 7 | Run Date: 07-09-2008

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs. ; *Surcharged (HGL above crown). ; j - Line contains hyd. jump.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0+00 0+50 1+00 1+50 2+00 2+50 3+00 3+50 4+00 4+50 5+00
Sta 0+00.00 - Ln: 1
Sta 0+12.40 - Ln: 1
Rim El. 4393.00
Inv. El. 4387.06 Out
Inv. El. 4387.06 In
Sta 0+19.54 - Ln: 2
Rim El. 4393.00
Inv. El. 4387.10 Out
Inv. El. 4387.85 In
Sta 2+26.03 - Ln: 6
Rim El. 4394.25
Inv. El. 4390.00 Out
Inv. El. 4390.00 In
Sta 4+59.81 - Ln: 7
Rim El. 4396.00
Inv. El. 4391.77 Out
24" @ 0.56%
15" @ 1.04%
15" @ 0.76%

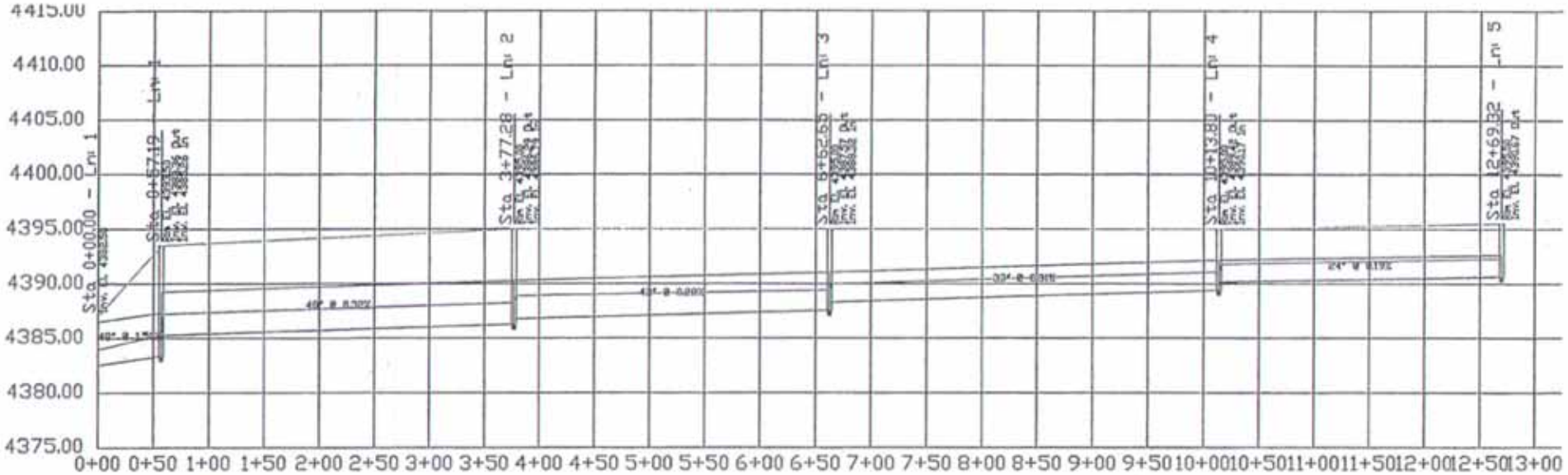
Hydraflow Plan View

Storm Sewer Summary Report

Line No.	Line ID	Flow rate (cfs)	Line size (in)	Line length (ft)	Invert EL Dn (ft)	Invert EL Up (ft)	Line slope (%)	HGL down (ft)	HGL up (ft)	Minor loss (ft)	HGL Junct (ft)	Dns line No.
1		41.50	48 c	57.2	4382.50	4383.36	1.500	4383.95	4385.27	n/a	4385.27	End
2		40.00	48 c	320.1	4385.26	4386.29	0.319	4387.25	4388.27	0.13	4388.40	1
3		30.00	42 c	285.4	4386.79	4387.57	0.275	4388.89	4389.45	0.11	4389.56	2
4		20.00	33 c	351.2	4388.32	4389.42	0.313	4389.98	4391.08	0.07	4391.14	3
5		10.00	24 c	255.5	4390.17	4390.67	0.195	4391.81	4392.31	0.20	4392.52	4

Project File: Sparks - Line 7.stm | Number of lines: 5 | Run Date: 07-11-2006

NOTES: c = cir; e = ellip; b = box; Return period = 100 Yrs.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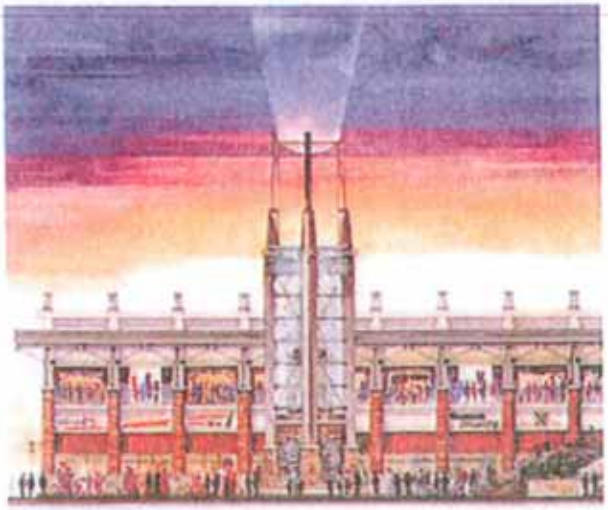
Sta 0+00.00 - Ln 1
Sta 0+67.12 - Ln 1
Sta 3+77.28 - Ln 2
Sta 6+62.60 - Ln 3
Sta 10+13.80 - Ln 4
Sta 12+69.32 - Ln 5

Tenant Handbook

The Legends at Sparks Marina

Destination Retail Development



RED℠

June 26, 2006

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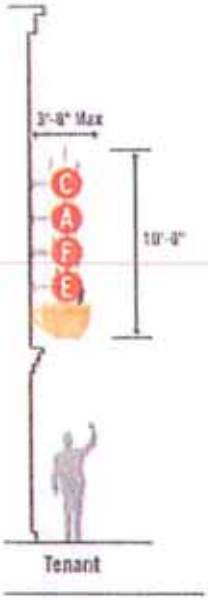


INTRODUCTION

This Handbook, identified as an Exhibit in your Lease, has been prepared to guide you, as well as your Architect, store designer, and contractor in expediting the construction of your building or lease premises. This information is a guideline for your Architect, and describes the Landlord's obligations, the Tenant's design responsibilities, and your contractor's requirements.

Landlord's Design Philosophy

Exciting and unique storefronts and merchandising designs create a shopping and entertaining environment that attracts customers and results in increased sales. Tenants are encouraged to create innovative and dramatic storefronts, shop interiors and graphics. Through the dramatic use of lighting and color, as well as careful attention to detailing, fixturing and graphics, each store can become an inviting and effective retail establishment, which will be compatible with the overall design quality of the Legends at Sparks Marina. Building materials will be a combination of quality brick of many textures and colors, ornamental iron, stone, stucco, and durable roofing materials.

Through the criteria in this Handbook, the Landlord has set certain quality and design standards, which will help the Tenants, create stores that are compatible with the overall design quality of The Legends at Sparks Marina.

ARCHITECTURAL DESIGN

Overview

This shopping and entertainment district consists of approximately 988,700 square feet of shops, restaurants, entertainment venues, national retailers, and a minor league baseball stadium located in Sparks, NV.

The layout of The Legends at Sparks Marina consists primarily of buildings of various sizes containing multiple tenant spaces. The building facades are designed with unique towers, canopies and marquis and awnings based on an "Sophisticated Western Rustic" streetscape theme.

Site

The project is located at the intersection of Interstate 80 and Sparks Blvd. in Sparks, NV. This project is highlighted by an exterior open-air streetscape that creates a pedestrian friendly environment with benches, quality landscaping, and other amenities. Typical stores will have a minimum of one entry onto the streetscape frontage. The architecture, streetscape and open-air courtyards are a major focus of this project.

Utilities

Each building will be supplied with individual electric, gas, water, telephone, and sprinkler risers to a central utility room. From that point the utilities will be diverted to the individual tenant units during the tenant improvement (TI) phase of construction. Each tenant is required to field verify the location and size of all utilities prior to commencing construction. The Tenant is required to review their lease exhibits for their individual utility responsibilities.

Exterior

Finishes
The exterior of the building will be brick with accents/banding of differing colors of brick, EIFS, cultured stone or cast stone. The storefronts will be accented with fabric awnings, marquis canopies and ornamental iron detailing and supports.

Typical exterior wall construction will be 6" or 8" metal studs, precast concrete, exterior sheathing and a brick, stone or EIFS veneer where appropriate. For maintenance reasons EIFS and other non-durable materials will be avoided in areas below 4'-0" above grade.

Service Doors
All exterior service doors will be hollow metal; primed and painted.

Signs
Building signage shall be provided by each individual tenant in accordance with this design criteria booklet and as approved by the Landlord.

Tenant Storefronts
Tenant storefronts will be primarily custom designed and Tenant installed. The storefronts shall be unique and represent the products inside the Tenant space and their national image.

Roofing

Flat Roofs
All flat roofs are sloped at approximately ¼" per foot typical to the roof drains. Each roof drain will be piped internally to the storm sewer system. Overflow drains are piped to the exterior of the building as required by local code.

Interior

Ceilings
The clear ceiling height designs of each tenant shall be based on a finished ceiling height of no more than 12'-0" for the small shops.

Slab
The floor slab is constructed of 4" concrete or as required by the structural engineer. Any modifications to the slab shall be by the Tenant at its own expense and shall meet the structural requirements of the Landlord's Engineer.

STRUCTURAL DESIGN

The roof framing system for this project varies throughout but primarily consists of structural steel girders and joist girders spanning between columns, with open-web steel joists filling in between the girders. Corrugated steel roof deck is used on top of the bar joists. The steel roof deck is used as a flexible horizontal diaphragm in some areas for the lateral system as discussed below.

Steel columns and precast concrete in some areas support the roof framing. Each column is founded on a spread footing to distribute the load to the soil. Around the perimeter of the building, cast-in-place concrete trench footings are constructed down to frost depth. Property line footings ("zero lot line") are utilized at the buildings adjacent to any anchor stores or other buildings adjacent to a property line. A concrete slab on grade, reinforced with welded wire fabric for crack control will be used.

TENANT DESIGN HANDBOOK DEFINITIONS:

Blade Sign:
Supplemental signage installed perpendicular to the storefront for visibility to pedestrians. All signage is to be provided by Tenant and approved by the Landlord's Architect.

Mall or Landlord's Bulkhead:
Element above Tenant's storefront. It defines the height of a Tenant's storefront. Tenants will be required to use a storefront system that does not extend above the Landlord's bulkhead.

Construction Coordinator:
Landlord's field representative(s) responsible for overseeing Tenant construction and compliance.

Curtain Wall:
A non-bearing exterior building wall, between piers or columns, which is not supported by the beams or girders of a skeleton frame.

Demising Partition:
A common rated wall between two adjoining shops or between a shop and a common area. The centerline of the demising partition between two adjacent lease spaces defines each Tenant lease premises.

Storefront Control Area:
The area below the bulkhead at the storefront and 4'-0" behind the lease line. The Landlord reserves the right to require above average materials in this area. No additional signs will be allowed within this area without written approval of the Landlord.

Facades:
The exterior face of the building, which is the architectural front, sometimes distinguished from the other faces of architectural or ornamental details.

Graphics:
Lettering, symbols and logos used for signage at the storefront and/or throughout the store interior.

Lease Line:
The line shown on the Tenant Lease Outline Diagram (LOD) which defines the confines of the Tenant's demised premises.

Mall Common Area:
Shopping Center streetscape, sidewalks, service halls, restrooms (if any), landscaping, children's play areas, etc. and all other areas of the Shopping Center not part of a defined lease premises.

Neutral Pier:
Architectural element separating two adjacent storefronts, or a storefront and a service corridor. Neutral piers are installed and maintained by the Landlord. The Tenant at its own expense shall repair any damage to the neutral piers during or after construction.

Parabolic:
A type of reflective lens, which provides a better control of light, reduces glare and maintains better light output. Parabolic lenses are required on all fluorescent light fixtures located in areas that are accessible to the general public.

Reveal:
Recessed separator strip between two different architectural planes. Also used to separate Tenant's storefront from Landlord's neutral piers and bulkheads.

Overdoor Transom Sign:
A sign to be located over the entry doors and below the Landlord provided bulkhead. The purpose of this sign is for store identification for pedestrians and cars in close proximity.

Display Window:
Transparent portion of storefront used for merchandise display. Tenants must maintain an innovative merchandising display within each display window at all times.

Sign Block:
Rectangular areas on building elevations, which define the allowable sign areas. Sign areas shall be in conformance with The Legends at Sparks Marina Planned Development Handbook.

Simulated:
Artificially produced to look or seem like a natural building material.

Soffit:
The exposed undersurface of any overhead component of a building, such as an arch, balcony, beam, cornice, lintel or vault.

Storefront:
Front face or other exposed exterior building wall of the store.

Store Name:
Official name of the store as written in the lease documents.

Wall Sign:
A sign to be located within the allowable sign areas as located on the Landlord building elevations.

DRAWING SUBMISSION AND APPROVAL PROCEDURE

The Landlord has established the following procedures to expedite the required approvals of the Tenant's drawings for the lease premises. Deviations from these procedures could result in needless delay and redrafting of the Tenant's Contract Documents.

All submittals shall be submitted to the Landlord's Representative.

Selection of Tenant's Architect

The Tenant, at its expense, must select a Nevada registered Architect(s) and Engineer(s) to prepare complete plans and specifications for the improvements to the premises including, but not limited to, applicable structural, plumbing, mechanical, and electrical. Tenants needing assistance in locating an experienced, locally licensed Architect(s) and Engineer(s) should contact the Landlord's Representative. The Tenant must forward a copy of this Handbook along with a print of the Tenant Lease Outline Diagram and associated Tenant Package to their Architect. It is the Tenant Architect's responsibility to obtain, review, and comply with all applicable codes. Tenant shall also notify the Landlord's Representative of the Architect's name, address and telephone number. All drawings must be signed and sealed by an Architect and Engineer registered in the State of Nevada.

In case of any discrepancy between this booklet and the Tenants Lease Document, the Lease shall govern.

After receiving the Tenant Lease Outline Diagram, carefully review the design criteria and applicable codes. Prior to starting construction drawings, the Tenant's Architect (in conjunction with the Tenant) shall proceed with the preliminary design of the Tenant's premises. It shall be the Tenant's responsibility to visit the site and verify all existing conditions prior to finalizing construction documents.

It is the responsibility of the Tenant and his Architect to schedule adequate time (approximately 2 weeks) for Landlord's preliminary review, and any subsequent revisions if required, final construction drawings and Landlord's final review (approximately 2 weeks).

Small Retail Shops

Upon execution of the lease the Landlord will provide the following as part of a "Tenant Package":

1. Lease Outline Diagram (LOD) for the proposed Tenant.
2. Tenant Design Criteria Booklet.
3. Site/Leasing Plan
4. Construction Documents of the Lease premises, if available at the time.

Upon receipt of this information the Tenant has 21 days in which to produce preliminary documents for Landlord's approval. The Landlord will review these documents within 2 weeks and return them to the Tenant marked "Approved", "Approved as Noted" or "Returned for Corrections". The Tenant will be required to submit final Construction Documents within 30 days of receipt of Landlord preliminary comments. The Landlord shall again review the documents within 2 weeks and mark them as noted above.

If final or preliminary documents are marked "Returned for Corrections" Tenant shall address all items and resubmit for approval within 10 working days. Failure by the Tenant to comply or show due diligence to the above schedule shall be considered in nonconformance with lease requirements.

PRELIMINARY DESIGN PHASE
(Small Shop Tenants)

The purpose of this phase is to acquaint the Landlord with the Tenant's intentions so that the Landlord may comment and/or advise Tenant of any changes necessary to meet the criteria before the final construction document phase begins.

Tenant's Architect shall submit the preliminary design to the Landlord's Representative for preliminary review and approval. The preliminary design shall be submitted within thirty days of receipt of the Tenant Lease Outline Diagram.

Please submit two (2) sets of all drawings to the Landlord's Representative so that one marked-up set may be returned to the Tenant and one kept on file.

Drawings shall be clearly identified with the Shopping Center name, Tenant's store name, Tenant's space number and key plan, and must include the following information as a minimum (additional information is encouraged).

- Preliminary floor plans (scale ¼" = 1'-0") indicating interior design concept, approximate location of fixtures and equipment, interior partitions, toilet rooms, exits, seating, etc., identifying all materials and colors.

- Reflected ceiling plan indicating all soffits, ceiling heights, materials, lighting layouts, and approximate locations of HVAC diffusers.

- Storefront elevation and section, including any graphics and signage. Indicate all materials and finishes (scale ¼" = 1'-0").

- Sketches, perspectives, sections or other details that will clarify the design of the storefront and the Design Control Area, or photographs of similar storefront, if related to Tenant's submission.

- Material finish and color sample board(s), properly mounted and labeled.

In addition to the above, one set of catalog cuts and/or photographs and/or samples showing the exterior lighting fixtures, if any, and other special treatments used in the sales area must be submitted so that all aspects of the public areas of the store can be reviewed by the Landlord's Representative.

If Tenant's storefront design follows a specific prototype, photographs of comparable stores should be submitted to aid the Landlord's Representative in the review process. The Landlord's Representative will review the preliminary design and make necessary comments or suggestions and return them with its approval or disapproval, one marked-up set of prints to the Tenant's Architect.

CONSTRUCTION DOCUMENT PHASE

After the preliminary drawings have been approved in writing by the Landlord's Representative, Tenant's Architect shall proceed with the final construction documents and specifications incorporating preliminary design suggestions and comments of the Landlord's Representative, in accordance with the criteria contained in this Handbook and the Tenant Lease Plan. Additional information may be required as deemed necessary by Landlord upon review of Tenant's drawings.

Final construction documents shall be submitted on or before the date required by the Lease. Final construction documents shall be submitted on an electronic CD (AutoCAD or Microstation) and with (3) three sets of prints to the Landlord's Representative. All drawings and specifications must be clearly identified with the Mall name, the Tenant's store name, a Key Plan with the Tenant space number, and the name and seal of the Architect or Engineer preparing these drawings indicating that he or she is registered in the State of Nevada. Final working drawings and specifications shall consist of a minimum of the following:

CIVIL ENGINEERING

The following is a list of Civil Engineering drawings that must be submitted for review. All drawings must: be a minimum of 22" x 36", be to scale (min. of 1"=50'), have a north arrow, show basic site plan information, contain appropriate general notes, identify materials to be used in construction, and contain any other engineering data necessary for determination of site construction. Each plan is further required to show the following:

Dimensioned Site Plan

- Property lines, setback lines, buildings, signs, sidewalks, and curbs.
- Parking stalls, aisles, and driveways.

Grading Plan

- Finished surface contours
- Erosion control methods
- Drainage structures

Utility Plan

- Existing utilities (or those provided by the landlord)
- Proposed service connections for water, sewer, storm sewer, power, natural gas, telephone, cable TV or any other underground utility.
- Appropriate details for manholes, cleanouts, hydrants, etc.

Pavement Plan

- Sidewalk and curb locations
- Elevations of curbs, parking lots, and sidewalks
- Drainage paths

Site Lighting Plan

- Pole locations and fixture mounting heights and number of and orientation of all fixtures.
- Point by point footcandle (fc) plan of parking lot with points not exceeding 20' on center grid.
- Average, maximum, and minimum fc at ground surface.
- Uniformity ratio: average-to-minimum fc and maximum-to-minimum fc.
- Pole manufacturer and model number.
- Fixture manufacturer and model number.

Landscaping Plan

- Location and identification of all plant materials
- Botanical and common name
- Size of plant material at time of installation
- Plant installation detail
- Site furnishings details (site furnishings include benches, trash containers, ashtrays, tables, etc.)

Important note: The plans must also show and define any structure or improvement that is to be constructed on the premises. Such items may include but are not limited to: retaining walls and traffic control signs. The Landlord reserves the right to include those items as part of the approval process

ARCHITECTURAL PLANS

- **Architectural Floor Plan (Scale: ¼" = 1'-0")**

Demising wall locations and dimensions.
Dimensioned interior partitions.
Restroom facilities.
Location of fixtures and equipment.
Recessed service door (if applicable).

- **Reflected Ceiling Plan (Scale: ¼" =1'-0")**

Ceiling heights including drops and curtain walls.
Types of ceiling construction.
Decor at ceiling.
Location of lighting fixtures, sprinkler heads, air diffusers, grilles, access panels and heat detectors (if applicable).

- **Storefront and Interior Elevations (Scale: ¼" = 1'-0")**

Material samples, (if not submitted with preliminary design).
Color storefront elevation and/or submit photograph of similar stores as required.
Finishes and colors.
Signage.

- **Necessary Sections and Details**

Large scale section through storefront to roof 1" = 1'-0"
Security grille detail, if applicable.
Details at neutral piers and Landlord's bulkhead at ceiling 1' - ½" = 1'-0".
Storefront details and wall sections.

- **Schedules**

Door schedules/details.
Room finish schedule.

PLUMBING PLANS

These drawings shall incorporate all minimum design and construction requirements as stated herein. If the Tenant's particular occupancy requires that these standards be exceeded to meet code or the Tenant's requirements, the Tenant shall be responsible for making the adjustments.

- **Plumbing Plan (Scale: 1/4" = 1'-0")**

Toilet facilities.
Location of other plumbing fixtures.
Location of sewer connection.
Location of plumbing vent connection.
Clean-out and floor drain location.
Domestic water distribution.
Gas piping layout (restaurant tenants, if applicable).
Water meter.
Sanitary system isometric drawings including line sizes.
Domestic water isometric indicating pipe sizes.
Water heater detail with relief valve and piping to floor drain.

HEATING VENTILATING AND AIR CONDITIONING

The HVAC drawings shall incorporate all minimum design and construction requirements, including complete calculations, indicating heat gain to and heat loss from the space for all lights, occupancy, exterior exposure (if any) and other heat producing elements.

- **Mechanical Plan (Scale: ¼" = 1'-0")**

Ductwork layout and sizes.
Heights above finished floor.
Damper locations.
Return air openings through demising walls.

Type of insulation.
Locate diffusers, grilles and registers.
Show thermostat location.
Return Air Systems: Direct / Indirect

- **Schedules and Details**

Diffuser and Grille Schedule indicating CFM capacities.
Equipment schedule.
Toilet exhaust duct connection detail.

- **Exhaust System**

Show windows (if required).
Cooking equipment (if applicable).
Specifications of exhaust equipment.
Location of equipment.
Methods of installation.
Ventilation requirements (by Tenant in the event of unusual or excessive requirements).
Fresh air intake.
Specify minimum CFM requirements.

ELECTRICAL

Electrical drawings and specifications shall show all circuits for store lighting (including emergency and night lighting), sign lighting, receptacles, toilet exhaust and other fans (if different or supplemental to Landlord's central system), and service to heating, ventilating, and air conditioning system.

Show single line power riser diagram indicating main disconnects, size of wire, conduit, panels, transformers, time clock, etc.

Show panel schedule and itemized load breakdown in connected kilowatts for the premises, including lighting, receptacles, sign lighting, water heating, special appliances, toilet exhaust fans (horsepower), make-up air fan (horsepower), miscellaneous space heating, sales door, operator motor (horsepower), fan coil unit (horsepower), return air fans (horsepower) and large motors (starter type).

These drawings shall incorporate all minimum design and construction requirements including complete calculations and show the total simultaneous load maintained at three hours or more for the store area and all other lighting, HVAC, and miscellaneous loads per square foot.

- **Electrical Floor Plan (Scale: ¼" = 1'-0")**

Location of all floor and wall outlets.
Location of Landlord's service.
Location of all fans, motors and HVAC equipment.
All loads assigned to circuits - itemized load breakdown.
Itemized Load Schedule

- **Electrical Ceiling Plan (Scale: ¼" = 1'-0")**

Lighting fixture layout including night lighting and sign lighting.
Toilet exhaust and other fans.
Emergency and exit light locations.
All lighting assigned to circuits.
Indicate sign and lights which are connected to time clock.

- **Schedules**

Lighting Fixture Schedule.
Electrical Panel Schedule.

- **Miscellaneous Details**

Electrical Distribution Riser Diagram.
Feed conduit and wire size.
Arrangement of panels, transformer, time clock, etc.
Indicate telephone conduit locations for connection to empty telephone conduit.
Conduit and wire size to Individual Units, HVAC equipment and panels as applicable.

- **Fire Protection Plans**

These plans must be prepared by a Landlord approved sprinkler contractor and submitted to the Landlord's Representative and the Mall Operation's Manager to verify compliance in accordance with the requirements of Landlord's Insurance Underwriters and must indicate the following:

- **Fire Marshall's Approval**

Location of existing sprinkler head grid with main and branch pipe sizes.
Location of branch piping.
Heights of ceilings and dropped soffits, etc.
Location of surface mounted or dropped lighting and decorative beams.
Location of curtain walls or lighting baffles.
Other construction which will affect sprinkler coverage.

- **Shop Drawings**

The Tenant shall submit to the Landlord's Representative for approval, three copies of the sign shop drawings.

Menu boards must be submitted to the Landlord's Representative for approval. Tenants are required to submit drawings or photographs that clearly indicate the size, colors and materials to be used.

The Tenant must submit three sets of the sprinkler shop drawings approved by the Fire Marshall to Mall Operations Manager.

Landlord's Representative reserves the right to request additional detailed shop drawings for review after final construction documents have been approved.

- **Final Construction Drawing Approval**

Upon receipt of complete sets of construction drawings and specifications as outlined above, the Landlord will review these drawings for compliance with the previously approved preliminary design and the other criteria of this Handbook, and return to the Tenant one set of prints marked with the approval stamp. The drawings will be marked "Approved", "Approved as Noted" or "Returned for Corrections". Drawings stamped "Returned for Corrections" or "Approved as Noted" requesting resubmittal of specific sheets shall be revised and resubmitted within 10 days of the receipt of the Drawings. Simultaneously, the Landlord's Representative forwards one set of stamped approved drawings to the Landlord's Construction Coordinator for field verification during construction. It is the Tenant's responsibility to see that the approved set of drawings with comments, if any, are distributed to his construction staff. Prior to the removal of the construction barricade, the Contractor shall request of the Operations Manager and Construction Coordinator a review and Punch List of the lease space. All Punch List items shall be completed prior to the removal of the barricade. No deviation from approved drawings will be permitted without prior written approval by Landlord. It must be understood that the Landlord's approval of the working drawings is for compliance with the criteria established in this Handbook only. By reviewing these drawings, the Landlord and its agent(s) assume no responsibility for code compliance, dimensional accuracy, engineering accuracy or completeness of these drawings for construction purposes.

SMALL SHOP STOREFRONT AND INTERIOR DESIGN
(0 square feet to 14,999 square feet)

Philosophy and Design Concept

The Legends at Sparks Marina will be a premier "value branded" shopping center in the Sparks, NV area. The quality of today's retail environment demands distinctive and high quality storefronts and presentations to enhance the shopping environment.

Storefront Design

The unique characteristics and quality tenant mix of The Legends at Sparks Marina calls for innovative and current storefronts. Critical to the design integrity and success of the shopping centers image are the individual contributions of each Tenant's store. It is essential that proper attention be paid to proportion, scale, color, and detailing so that the Tenant's store can enhance the image of the shopping center and themselves.

Storefront Entry Element

Storefronts should be designed to incorporate an entry feature at the entrance into each leased space.

For the intent of the criteria, the term "Entry" shall be described as a grand or imposing entrance and shall encompass the whole architectural composition surrounding and including the doorway.

A single portal or a series of multiple portals may be featured in the storefront design.

Entry elements shall be attached to the storefront and provide a weatherproof barrier to the public way.

Special lighting effects such as cove lights or up-lights are encouraged and subject to approval of the Landlord's Representative and will be reviewed on an individual basis.

Design Criteria

This criteria is a basic "set of tools" that the Tenant is required to work with and expand upon. Criteria are written to encourage freedom of individual expression and to provide a common point of departure for all Tenants.

Storefronts should emphasize a "sense of entry", and display of merchandise. National or regional tenants who have a typical or recognizable storefront design are expected to review the design criteria of The Legends at Sparks Marina. They shall then adjust their design to ensure compatibility, compliance and work closely with the Landlord's Architect to achieve the same level of quality as The Legends at Sparks Marina.

Key Plan

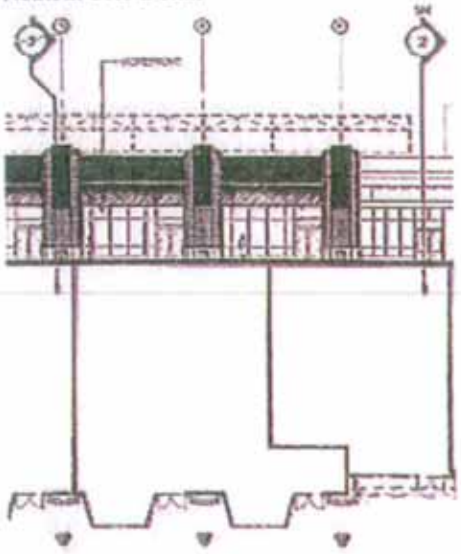
Reference Section One of the design criteria for the general overall configuration of the property. Each Tenant should refer to his Tenant Lease Plan and Tenant Package for specific information and details relative to its leased space.

Neutral Pier

The neutral piers where they occur separate adjacent lease spaces and are provided and maintained by the Landlord. The Tenant is not responsible for the neutral piers and cannot modify them or change their finishes in any way. Tenants should refer to the detail sheets, which are forwarded with the Tenant Package for detailed information regarding the configuration and construction of the neutral piers.

Tenants are also required to verify and design around the location of any fire hydrant cabinets or roof drain down spouts in the Landlord Neutral Piers. The neutral pier detail indicated below is conceptual. Refer to Construction Documents for details specific to each lease space.

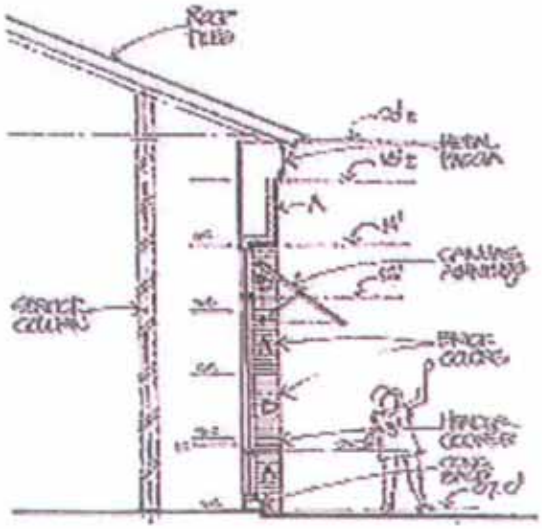
Neutral Pier Detail



Storefront Bulkhead

The bulkhead above the storefront is a standard Mall finish, provided and maintained by the Landlord on the outside of the lease premises. The Tenant cannot change or modify the bulkhead, nor is the Tenant responsible for its maintenance, except for patching and repairing the bulkhead to new condition of any damage caused by the Tenant during or after construction. Storefronts may attach to the bulkhead but may not be structurally suspended from or attached to the bulkhead. All storefronts shall be self-supporting and attached to the Landlord structure for lateral support only.

Conceptual Storefront/Bulkhead Section
(See mall drawings for specific condition.)



Design Control Area

The Design Control Area includes all display windows and retail graphics, display fixtures, signs, materials, finishes, colors, and lighting from the lease line to 4' behind the lease line.

If a Tenant chooses to recess the store closure behind the designated Design Control Line, the Design Control Area will be enlarged accordingly.

The Landlord will closely control all elements in the Design Control Area.

The soffit at the storefront entrances may be finished in the same material as the storefront. The soffit height shall be not less than the adjacent wall bulkhead at the entry. Acoustical tile is not an acceptable ceiling for any part of the Design Control Area.

Display Windows

Display windows should be unique and individual. Window displays should thus be integrated into the architectural design and character of the entire storefront. A variety of textures in display and window treatment should be explored, as well as innovative lighting and window designs (see Signage Criteria). Display windows shall be transparent and open to the store. No backdrops behind displays will be allowed.

A maximum of 75% of the storefront width is allowed for the display windows. The Landlord's Representative will evaluate exceptions on specific merchandising situations and requirements.

For greater transparency, a storefront glazing with a minimum use of mullions or frames is recommended.

Corner Tenants must install display windows and/or store openings on both elevations. Solid walls will not be permitted along the Lease Line without approval from the Landlord's Representative during the Preliminary Design Phase. Should storefront glazing extend to the floor, a minimum 6" durable base or frame is required.

Storefront Awnings

Storefront awnings are an integral part of the architectural design intent of The Legends at Sparks Marina. They provide cover from the elements as shoppers move from store to store. They also provide the Tenant additional opportunity to convey their individual architectural design and character.

The Tenant is responsible for the supply and installation of fabric awnings or steel marquis canopies as they are indicated on the Landlord's construction documents directly in front of their lease premises. The functionality of the awnings require a minimum coverage of 5'-0". Metal covered awnings are not permitted. The sides of the awnings may remain open. The awning framing system is required to be painted to an acceptable finish. The color and style of the fabric awnings shall be determined by the Tenant, but subject to approval of the Landlord. Awning signage is not permitted. The color of the steel marquis shall be as required by the Landlord's Architect. Deviation in design of the awnings and marquis are encouraged but subject to review and approval by the Landlord's Architect.

Store Closure

The level of the finished floor within the Tenant Lease Premises must match to within a ½" of the level of the public walkway finished floor at the Lease Line and specifically detailed on the plans. In other words, the Tenant flooring design shall meet the minimum requirements of ADA.

The storefront doors may be any one of the following:

- Pivoting glass doors.
- Pivoting wood doors.
- Electric Sliding doors.
- Revolving doors.

Doors

Sliding doors shall be electric horizontal doors with integrally colored aluminum frames operated by a motion sensor. Doors shall be located so motion sensor will not activate continuously due to passers by. All door tracks are to be recessed and as much as can be practically hidden from view. Pivoting doors are encouraged to be frameless, glass out-swinging doors on pivots.

All door systems shall be weather tight as required for an open-air mall. If the use of an air lock or vestibule is preferred it shall be designed as part of the storefront design and shall be located within the lease premises.

Floor and Base

The interior floors and base should be covered with the highest quality materials, conforming to the basic quality criteria outlined later in this manual. Ease of movement, safety, and maintenance should be primary considerations in floor covering.

Tenant shall have a flush transition between the shopping center walkway surface and the Tenant's floor finish, feathering the floor as necessary to meet the requirements of ADA. The use of vinyl or metal reducer strips will not be permitted.

The storefront base may be stone, precast, masonry, metal or tile, and should complement the Tenant's storefront material. A base is not required where Tenant's storefront extends to the floors. The base must be of a durable material capable to withstand the abuse of standard exterior cleaning and snow removal equipment.

Where storefront glazing continues to the finished floor, it must terminate in a 2" high sill base compatible with the store design.

Any other durable base material, easy to maintain and that matches or is compatible with the Tenant's other storefront finishes may be used. Storefront base should reflect the dimensional quality of the storefront.

Lighting

Tenants are encouraged to use decorative lighting elements as an integral part of their storefront and interior store design. In recent years, a wealth of lighting fixtures and techniques has come on the market, and innovative lighting should be explored. Lighting can play an important role in attracting customers and enhancing merchandising strategies.

All storefront and general store lighting must be reviewed and approved by the Landlord's Representative, subject to the following basic guidelines:

- The Tenant shall provide a high quality of illumination above the display area and entrances.
- No storefront lighting shall be installed in the soffit area ceiling beyond the lease line. Up lighting and halo lighting are encouraged.
- No strobe, spinner, or chase type lighting shall be used. No animated flashing or intermittent lights, black light, or strobe lights will be permitted.
- For illumination in the Design Control Area, the Landlord must approve decorative type lighting (i.e. luminous ceilings, chandeliers, pendant fixtures or wall units). Fluorescent light fixtures will not be permitted within the Design Control Area. All fluorescent lighting outside of the Design Control Area must be deep cell parabolic.
- All signs, logos, and display windows shall be illuminated during the hours the center is open and controlled by a time clock that will be connected to the Tenant's power supply: 7 days a week, 24 hours a day.
- All showcase and display cases must be adequately lighted and ventilated. Direct visual exposure of incandescent bulbs and/or fluorescent tubes is prohibited. No lamp shall extend below the ceiling line or below the window head at show windows within the Design Control Area.
- No TV monitors will be permitted in the Design Control Area.
- A mixture of lighting types is encouraged in the Sales Area.

Finishes

Materials for the storefront should suggest quality, craftsmanship, elegance and stability. Innovation and creativity are encouraged. With that in mind, the use of the following materials on the storefront is strictly prohibited:

- Imitation or simulated materials (including those available in plastic laminates); i.e., imitation brick, simulated wood, synthetic marble, etc.
- Slat wall.
- Pegboard in any form.
- Vinyl or suede wall covering or wallpaper.
- Softwood storefronts (i.e. rough sawn cedar). Avoid images that are strongly rustic or residential.
- Plywood paneling.
- Carpet or fabric (except in canvas awnings).
- Painted drywall, including Zolitone or Polymix type products.
- Metal or plastic laminates.
- Chain link fencing or rough metal.

Signage

Signage shall be as outlined in Section Eleven of this tenant handbook.

MERCHANDISING AND DISPLAY

Storefront Philosophy

One of the aspects of The Legends at Sparks Marina is the merchandising opportunity afforded the Tenants by the storefront design. The storefronts create an area visible from the shopping center that extends the store beyond the display window into the shopping center. Basically, the entire front portion of the store becomes part of the display design.

The opportunity to display merchandise to shoppers passing by opens a wealth of marketing strategies. It also affects the front layout of the store. Attractive fixtures and appropriate materials are just as crucial in the front part of the space as in the display window itself. Standard light levels should be maintained to adequately light merchandise. As the storefront exposes a portion of the store to view, all lighting and display fixtures should be of exceptional quality, and are subject to the Landlord's approval.

Merchandising Opportunity

Fixture layout and lighting can be used to highlight particular merchandise, to attract passing customers, and to enhance the image of the store. As one example, a bookstore that normally displays best sellers and new releases in its display windows now has the opportunity to also display its videotape library, gift books, or special interest or seasonal titles as well.

However, the wide visibility of the store interior also creates some restrictions. The same bookstore mentioned above would need to be careful not to place any discount tables, magazine racks, or plainly stocked shelves in areas visible from the courtyard.

General Store Criteria

With recent trends in store design evolving to more open and transparent storefronts, it is often not possible to differentiate between the storefront and the store interior. The way the Tenant displays their merchandise, the fixturing layout, and the fixture combine with the storefront architecture to create an image to the public. The Landlord is therefore concerned that the store interior be designed with the same care and attention to detail as the storefront itself. Therefore, the following criteria for interior design have been created to guide the Tenant.

Layout, Fixturing, and Merchandising

The Tenant is encouraged to use the services of a professional store planner, visual merchandiser, and/or fixturing specialist in the design and layout of his store.

A properly designed floor layout will mean an increase in sales and profit.

Display fixtures should complement the overall design of the store and present the merchandise in an appropriate manner. The Tenant is required to use only new, first quality fixturing throughout his store. Used or reconditioned display fixtures are not permitted; high quality bona fide antique furnishings may be used with prior approval.

The use of pegboard or continuous slat wall on display fixtures or as a wall finish is not permitted.

Floor Finishes

All areas of the Tenant's premises must have a finished floor.

The following are approved floor finishes.

- Quarry tile or ceramic tile.
- Stained or sealed concrete.
- Marbled or other natural stone terrazzo.
- Carpet (outside of design control area), which must be commercial grade and no less than 28 oz. per square yard face weight.
- Vinyl composition tile and base is not permitted in the sales area or anywhere visible to the public. Vinyl composition tile may be used in stock rooms or restrooms.
- Bullnose tile or carpet reducer strips are not permitted.
- Wood flooring is acceptable however, a transition at the entry area of walk off mat or tile is suggested.

Ceilings

The ceiling is an integral part of the store design and as such requires appropriate emphasis. The ceiling helps define the character of the store and when properly designed will enhance a store's appearance. The Landlord discourages the use of a ceiling in one plane throughout the store.

Through the use of coffers, drywall soffits and bulkheads, an interesting ceiling design will result. A well-designed ceiling can also help to define different lighting values.

In general all areas of the Tenant's store must have a ceiling. Exposed structure (even in storage areas) will be allowed only on an individual basis and when part of a design. Ceilings above 12'-0: in height may encounter building obstruction (i.e. sprinkler, structure, etc.). Tenant shall field verify all existing conditions. Ceilings may not be suspended from the roof deck, sprinkler pipes, electrical conduits or ductwork.

All ceiling in the Design Control Area must be drywall or a continuation of the storefront material.

All ceiling in the remaining areas of the store must be, drywall, metal linear, or acoustical ceiling tiles, or any combination thereof. Additional materials may also be approved; however, samples and photographs will need to be submitted.

If acoustical ceiling tiles are used, the following types are acceptable:

2'x2' regular edge acoustical panels.
2'x4' acoustical panels scored to disguise the 2'x4' module (such as Armstrong Second Look).
2'x2' designer panels (such as Armstrong Syllables).
Concealed spine acoustical tiles.

The Tenant shall provide access to all ductwork, heaters piping, controls, or valves located within the premises by means of accessible ceiling tile or flush access panels.

Walls and Wall Finishes

All demising walls (walls between adjacent Tenants or between a Tenant and a common area) must be constructed with 5/8" fire rated drywall, fire taped from floor slab the deck above. The wall shall meet all local requirements for a one-hour rating.

All demising walls must be finished to achieve a one-hour fire rating. Where a demising partition of the Tenant's premises is adjacent to a service corridor or other Landlord-related facility between adjacent Tenant spaces, and is not an exterior wall, the Landlord has provided a demising partition from floor slab to underside of structure above. This partition is of either 6" or 3 5/8", 25 gauge steel stud construction at 24" on center or greater. Structural columns that occur in a demising wall must be covered with fire rated drywall as part of the demising wall.

Where Tenant elects to use any type of music system or sound generating device within the premises, perimeter wall construction must be such that it does not allow the transmission of sounds to adjacent spaces. Tenant must provide any necessary construction such as sound insulation blankets or sound deadening panels to assure adjacent Tenant of the quiet enjoyment of their space.

If the Tenant plans to use a demising wall for the support of shelf standards or heavy attachments, Tenant must reinforce the wall as needed, such as by providing additional steel studs, or providing independent supports for the shelf standards. However, the wall shall maintain a one-hour UL rating.

Tenant may not install any attachments, such as shelving equipment, etc., directly against department store or exterior masonry walls without providing a furring or stud separation (while maintaining any existing expansion joints). The furring or stud separation must be adequate to support the attached shelving equipment, etc. All interior partitions must be built with metal stud framing. Fire treated wood framing is permitted for incidental blocking only.

All interior wall surfaces in the sales area must be finished in an appropriate manner. Three coats of paint, wall covering, paneling, mirror, plastic laminates, finish masonry or metal are considered suitable finishes.

Non-Combustible Construction

All Tenant construction, including storefronts must be non-combustible and subject to the approval of the Building Department and the Fire Marshal. Treated fire-resistant materials will be permitted only where approved by jurisdictional authorities.

No Modification to Landlord's Structural members or Building Systems

Under no circumstances shall the Tenant's Contractor cut or modify the Landlord's structural members, expansion joints, wind bracing, columns, beams, and bridging. Any structural framing or bracing required for Tenant's construction and to be attached to the Landlord's structure must be designed by a Structural Engineer and approved by the Landlord's Architect and Engineer.

The Landlord reserves the right to refuse to permit the installation of any roof- or wall-mounted equipment which exceeds the capability of the structural system; or to require screening if the appearance of such equipment would be detrimental to the appearance of the center.

Tenant's Contractor shall not be permitted to modify, attach or hang from the Landlord's duct work, water lines, sprinkler lines, conduit or roof deck to accommodate Tenant's construction including, but not limited to, Tenant's ceiling grid, ductwork, pipes, conduit, etc.

Mezzanines

Tenant, upon obtaining prior written approval from the Tenant Coordinator, may construct a mezzanine or storage platform subject to the following:

The mezzanine framing must be completely independent of the basic building structural frame and demising partitions, and must be designed by a licensed Structural Engineer.

Roofing System

Access to the Mall roof is restricted to the Landlord's personnel and Landlord's designated Contractors only. No Contractor or Subcontractor will be permitted on the roof unless written permission has been obtained from the Operation Manager. The installation of all flashing and curbing for Tenant related equipment must be by the Landlord's Roofing Contractor at the Tenant's expense. The furnishing of the roof curb and the installation of equipment on the roof will be by the Tenant's Contractor.

Filters used in all kitchen exhaust systems shall be of non-combustible construction and comply with NFPA requirements. All systems shall be provided with access panels and a means of collecting grease drippings from the filters.

Roof-mounted kitchen hood exhaust fans shall be of the "mushroom" type and have grease pans adequate to protect the roof. These pans shall be installed, cleaned and maintained regularly by the Tenant.

Tenant Security System

Electronic security systems and shoplifting detection services shall be concealed from public view. Freestanding posts, suspended rails, or walk through portals are discouraged. The Landlord must approve Tenant security systems prior to installation.

Fire Protection Sprinkler System

The Landlord has provided a fire protection sprinkler system including feed and/or cross-mains located within the premises, at an elevation a minimum of 13' 0" above the Tenant's slab. The Tenant is responsible for running all branch lines and dropping all sprinkler heads unless otherwise noted in the Tenant Lease Exhibit. All sprinkler heads in the storefront design control area shall be fully recessed and semi-recessed in the sales area.

Any revisions, extensions or relocations to the Landlord's standard sprinkler system shall be by the Tenant at the Tenant's expense. Work to be performed by the Landlord approved Sprinkler Contractor at Tenant's expense.

Fire Extinguisher

Tenant shall furnish and install fire extinguishers as required by the Fire Marshal.

Floor Slabs

Concrete floor slabs have a smooth troweled finish and are slab on grade. No depressions or recesses in slabs will be permitted without prior written approval.

For first floor Tenants, a slab block-out area is provided by the Landlord. The Tenant shall field verify the size and location. After the plumbing lines have been installed and inspected, Tenant shall infill the concrete slab at 3,000 psi to match the Landlord provided design.

Grease Interceptors

If the Tenant's occupancy requires the use of a sanitary sewer grease interceptor it shall be provided and installed by the Tenant at it's own expense. The location of the interceptor is subject to approval of the Landlord's Representative.

SUB-MAJOR BUILDING DESIGN

(15,000 s.f. to 119,999 s.f.)

Required Exterior Building Materials

Building elevations shall be as approved by both the Tenant and the Landlord's Representative.

- Building Materials - minimum 30% EIFS or stucco on concrete panels
- 35% Masonry or Stone veneer systems
- Final Exterior Building Design shall be as designed by or subject to approval of the Landlord's Coordinating Architect.
- Exposed pitch roofs – roofing materials shall be Landlord approved product.

Note: All design modifications and materials shall be in strict accordance with the Design Criteria and previously stated and approved building façade, design and materials. All variations are subject to Landlord approval. All building materials are subject to change in accordance with local design ordinances. The Landlord's Architect shall obtain approval from the City.

Layout, Fixturing and Merchandising

The Tenant is encouraged to use the services of a professional store planner, visual merchandiser, and/or fixturing specialist in the design and layout of his store. A properly designed floor layout will always mean an increase in sales. A high standard of finish is expected.

Floor Finishes

All areas of the Tenant's premises must have an appropriately finished floor as approved by Landlords Architect.

Ceilings

The ceiling is an integral part of any store design and as such requires appropriate emphasis. The ceiling helps define the character of the store and when properly designed will enhance a store's appearance. All ceilings shall conform to support requirements and may not be attached to roof deck, sprinkler pipes, electrical conduits or ductwork.

The tenant shall provide access to all ductwork, heaters, piping, controls, or valves located within the premises by means of accessible ceiling tile or flush access panels.

The tenant shall provide access to all ductwork, heaters, piping, controls, or valves locate within the premises by means of accessible ceiling tile or flush access panels.

Walls and Wall Finishes

All demising walls shall be as required by local governing authorities. All interior wall surfaces in the sales area must be finished in an appropriate manner (i.e. consistent with first class retail store).

No Modification to Landlord's Structural Members or Building Systems

Under no circumstances shall Tenant's Contractor cut or modify Landlord's structural members, expansion joints, wind bracing, columns, beams, and bridging. Any structural framing or bracing required for Tenant's Construction

and to be attached to Landlord's structure must be designed by a Structural Engineer and approved by Landlord's Architect and Engineer.

Landlord reserves the right to refuse to permit the installation of any roof- or wall-mounted equipment which exceeds the capability of the structural system; or to require screening if the appearance of such equipment would be detrimental to the appearance of the center. Tenant's Contractor shall not be permitted to modify, attach or hang from Landlord's duct work, water lines, sprinkler lines, conduit or roof deck to accommodate Tenant's Construction including, but not limited to, Tenant's ceiling grid, ductwork, pipes, conduits, etc.

Storefront Signage

Signage shall be as indicated in Section Eleven of this tenant handbook.

Floor Slabs

Concrete floor slabs have a smooth troweled finish and are on grade. No depressions or recesses in slabs will be permitted without prior written approval. The Tenant's Contractor shall furnish and install that portion of the concrete slab in all areas where the slab has been blocked out in conformance with Landlord standards.

Exhaust System

Tenants who have special exhaust requirements as a result of odor, moisture or high heat-producing operations shall provide separate special exhaust and make-up air facilities, to be approved by the Coordinating Architect.

Any unacceptable odor, as determined by Landlord, shall be exhausted by means of centrifugal blowers located within the premises and ducted through the roof to the atmosphere.

No openings for fans, vents louvers, grilles or other devices will be installed in any demising partition, exterior wall, or roof without Landlord's written approval and Landlord supervision.

Fire Protection Sprinkler System

Any revisions, extensions or relocations to the Landlord's standard sprinkler system shall be by the Tenant at the Tenant's expense. Work to be performed by Sprinkler Contractor acceptable to Landlord.

Roofing Systems

Access to the Mall roof is restricted to Landlord's personnel and Landlord's designated Contractors only. The installation of all flashing and curbing for Tenant related equipment must be by Landlord's roofing Contractor at Tenant's expense.

OUT PARCEL BUILDING DESIGN

General Store Criteria

Since the Individual Outparcel Tenant represents a major attraction to the shopping center, their building design needs to reflect a dramatic and individual design image, while containing shop design elements of the center. This criteria is intended to establish design standards to encourage their out parcel buildings to become a unique, yet consistent part of the overall shopping center.

These criteria are a basic "set of tools" that the Tenant is required to work with and expand upon. Criteria are written to encourage some freedom of individual expression and to provide a common point of departure for all Tenants while adhering to shopping center guidelines.

The building facades should be designed to give an innovative design concept. Tenants are encouraged to take full advantage architecturally of the shopping center design standards.

National or regional tenants who have a typical or recognizable building design are expected to review the design of the shopping center and this criteria and adjust their design to ensure compatibility and compliance with both.

As a general guideline, the following materials will be required to maintain compatibility with the shopping center.

- Main building facades – 35% scored EIFS or stucco minimum, 25% minimum approved masonry materials.
- Building parapet cap element - prefinished metal. All facades of building.
- Entry Facade Element - maintain a minimum of 25% minimum approved masonry materials.

Other acceptable exterior material - subject to Landlord design review and approvals:

- Brick masonry
- Limestone
- Concrete masonry
- Selected stone masonry
- Cast stone
- E.I.F.S.

Roofs

The roof materials and roofline compositions shall be consistent or compatible with the shopping center design and provide an integral part of the individual building design. The following are acceptable roof materials:

- Typical bituminous roof with exterior wall screening parapet.
- Exposed pitch roofs shall be Landlord approved materials.

All roof slopes and configurations are subject to Landlord approval.

Storefront Signage

Signage shall be as indicated in the Signage section (Section 11) of this tenant handbook.

Trash Enclosures

All trash enclosures and service areas shall be appropriately screened to reasonably hide them entirely from public view. All trash enclosures and service areas shall utilize the appropriate and approved masonry materials to match shopping center standards. All gates shall be metal/steel construction. All trash enclosures shall meet the City codes and planning criteria and approvals.

Screening

The following items shall be either located out of direct public view or adequately screened by a screen wall utilizing the appropriately approved masonry materials:

- Gas meters and any associated piping.
- Electric meters and any associated conduits.
- Transformers.
- Trash compactors.
- Any ground installed equipment.
- Trash dumpsters, service areas and recycling bins and grease interceptors.

All roof mounted equipment shall be adequately and completely screened from any adjacent property or adjacent public right of ways and/or pedestrians' views by means of exterior building walls or Landlord approved equipment screening. All rooftop screening shall be integrally designed into the building by use of roof parapets and walls. Painting of equipment is not allowed.

CIVIL/SITEWORK

Grading

1. The tenant's pad will be graded to the elevations and specifications as indicated on the Landlord's grading plan prior to the tenant taking possession of the pad. (Contact Landlord for plan prior to site design.)
2. The first floor elevation of the tenant's building shall be the elevation identified on the Landlord's grading plan. If the Tenant requires the first floor elevation to be anything other than the elevation given by the Landlord, the Tenant shall be responsible for contacting the Landlord's Engineer stating the reasons for the new first floor elevation.
3. Parking lot grades shall not exceed 4.0%
4. Sidewalk grades shall comply with ADA codes and regulations.
5. Slopes in green areas shall not exceed 3:1 (horiz:vert).
6. Methods to control siltation and erosion of soil onto adjacent properties during construction must be incorporated into the Tenant's design plans. It shall be the Tenant's responsibility to erect and maintain erosion control measures.

Utility Plan

1. Connections for water service and sanitary service shall be designed to the City of Sparks, NV standards and any other applicable local, state or federal standards.
2. Underground stormwater collection systems shall be utilized to collect stormwater runoff for the 10 year storm or as otherwise required by the City of Sparks, NV. (Overland flow across lot/parcel lines is allowed only in areas where no receiving storm sewer is present.)
3. Storm sewer curb and grate inlets shall be used to collect surface water. Inlets used shall match those used within the shopping center. (Contact Landlord's Engineer for appropriate inlet types.)
4. The use of concrete flumes to direct stormwater across landscaped areas or sidewalks is prohibited.
5. Location of electrical transformer must be shown.

Paving Plan

1. Parking stalls shall be minimum of 9.0' wide and 18.0' in length or greater in size if required by the City of Sparks, NV.
2. Location and number of handicap stalls shall be in accordance with ADA guidelines.
3. Drive aisles shall be a minimum of 24' in width from back of curb to back of curb. Whenever possible, drive aisles shall be 26' in width.
4. All parking rows shall have a landscaped island at each end.
5. No single or double loaded parking row shall exceed 25 cars without an internal tree island. Tree islands shall be a minimum of 64 square feet.
6. Concrete curb and gutter must be used in all appropriate locations. Asphalt curb and gutter is prohibited. (Contact the Landlord's Engineer for acceptable curb and gutter sections.)
7. All parking lots and driveways must utilize curb and gutter.

Site Lighting Plan

Site lighting design shall be compatible with the overall site lighting concept of the shopping center.

See Section Nine for Design Criteria.

Landscaping

Landscape design shall be compatible with the overall landscape design concept of the Shopping Center. (Contact Landlord's Landscape Architect for a current listing of plant materials.)

See Section Eight for additional Design Criteria

General Notes for Outparcel Tenants

1. Due to the changing nature of project, the Landlord may have criteria not explicitly defined in this manual which may apply to all or portions of the project. As the project moves forward, additional criteria may be added and/or existing criteria modified or clarified which may affect the Tenant's design documents. Please contact the Landlord and its Engineer for supplemental criteria, which may be available. The Landlord also reserves the right to evaluate specific items not addressed by these criteria if those items affect the function or overall appearance of the project.
2. Substitutions and exceptions to these criteria may be granted in extreme cases.
3. Local Government rules and regulations shall govern if in conflict with these criteria.
4. Review of the plans by the Landlord's engineer is for checking conformance to the shopping center design criteria provided herein only. Review and approval of the Tenant's design plans is not meant to provide quality assurance/quality control measures.

ENGINEERING OVERVIEW

HVAC

Tenants shall provide heating, ventilation and air conditioning by means of packaged rooftop units. All Tenant provided equipment and materials shall be furnished and installed by the Tenant in locations approved by the Landlord. The Tenant shall verify location of any Landlord provided equipment prior to commencing construction. The Tenant shall notify the Landlord immediately of any discrepancies.

Electrical

Electrical service is 277/480V, 3 phase. The Landlord shall provide a fusible service switch and meter socket in the Landlord's electric room. Tenants shall install service fuses, meter and feeders which connect the Tenant's space with the Landlord's base building electrical system wireway. The Tenant shall install transformers, distribution panels, miscellaneous devices, lighting fixtures and all required electrical wiring within the space. Permanent electrical service shall be metered and billed to the Tenant by the local utility company.

Plumbing

The Landlord shall provide a sanitary sewer tap and a cold water service supply tap. Tenants shall be allowed to make one roof penetration for plumbing vents. The Landlord's Roofing Contractor at the Tenant's expense shall construct all roof penetrations. The Tenant shall furnish and install all required plumbing piping and fixtures within the space unless otherwise noted in Lease Exhibits.

Gas Service

Natural gas is available if required by the Tenant. The Tenants are solely responsible for installing the gas service piping from the gas company connection point outside of the building to the Tenant's space in accordance with the Landlord's pipe routing requirements. Gas service where used shall be metered and billed to the Tenants by the local utility company.

Sprinkler

All Tenant spaces are to be fully sprinkled. The Landlord shall provide a sprinkler riser to the building and a bulk main within or near the rear of the space. The Tenant shall tap the bulk main and install a complete sprinkler system with drops and heads within the space. All sprinkler heads are required to be quick response type.

Fire Alarm

Each Tenant shall be required to install a fire alarm system if required by Code or if indicated by this tenant handbook. The Tenant's panel shall report alarm, trouble and supervisory signals to the Landlord's central fire alarm system. The Landlord's designated Fire Alarm Contractor at the Tenant's expense shall perform all system connection and device installation work for components that are directly connected to the Landlord's central fire alarm system.

DEVELOPMENT STANDARDS

Unless otherwise permitted pursuant to a Venture Agreement, the following Development Standards shall apply to all improvements in the Development Site:

Uses

The Development Site shall be used only for retail sales, retail warehouse, retail and/or wholesale distribution, theaters, museums, tourist purposes, lodging, offices, entertainment, restaurants or other permitted commercial purposes in accordance with Governmental Requirements. Without limiting the generality of the foregoing, the following uses shall be prohibited:

(a) Any use which is offensive by reason of odor, fumes, dust, smoke, noise, or pollution, or which constitutes a nuisance or is hazardous by reason of fire or explosion, or injurious to the reputation of any Tract with the Development Site. No oil, gasoline or flammable liquid shall be stored in bulk of more than fifty-five gallons gross capacity except in underground storage tanks. This restriction is not intended to prohibit a gas station or car wash facility.

(b) A facility primarily used as a storage warehouse operation, mini-warehouse, or freight terminal (for purposes hereof, a "storage warehouse operation" or freight terminal shall not be construed to include retail merchandise stored on the premises with the main use, or to preclude the operation of a "wholesale club" user.

(c) A facility for the assembling, manufacturing, refining, or smelting (provided this restriction shall not preclude the assembly of merchandise to be sold at a facility). Drilling, mining, exploring or the producing of oil, gases or other minerals.

(d) Any use which involves the raising, breeding, or keeping of any animals or poultry except on a temporary basis, in which case such areas are to be cleaned and maintained by the owner of such Tract so that it is not a nuisance to any other Tract (provided this restriction shall not preclude an aquarium or retail facility that merchandises domestic animals).

(e) Salvage or reclamation yards and the storage of inoperative vehicles; however, this restriction shall not preclude the operation of a vehicle sales dealer or similar operation, for which inoperative vehicles may be stored on a temporary basis for repair, so long as reasonably screened from view.

(f) Any pawn shop or "second hand" store.

(g) Any mobile home park, camp ground, trailer court, or labor camp; provided, however, this prohibition shall not be applicable to the temporary use of construction trailers during periods of construction, reconstruction or maintenance or for shag trailers, delivery trucks or recreational vehicles of Permittees.

(h) Any dumping, disposing, incineration or reduction of garbage; provided, however, this prohibition shall not be applicable to garbage compactors located in an approved and screened service area.

(i) Any fire sale, bankruptcy sale (unless pursuant to a court order) or auction house operation (but this provision shall not restrict the absolute freedom of an Occupant to determine its own selling prices nor shall it preclude the conduct of periodic seasonal sales, promotional or clearance sales).

(j) Any central laundry, or Laundromat; provided, however, this prohibition shall not be applicable to a drop-off and pickup facility, or a central laundry or Laundromat that complies with all Environmental Laws.

(k) Any automobile, truck, trailer or recreational vehicle with outside sales, leasing, or display prior to approval by the Landlord. Notwithstanding the foregoing, boat, trailer, and recreational vehicle sales and storage are allowed as an accessory use in the Outside Sale Areas.

(l) Any mortuary or funeral home.

(m) Any establishment selling or exhibiting pornographic materials or which sells drug-related paraphernalia or which exhibits either live or by other means to any degree, nude or partially clothed dancers or wait staff and/or any massage parlors or similar establishments; except that this provision shall not be deemed to preclude the operation in the Development Site of either a nationally or regionally recognized book store, or a drug store or pharmacy, or a department within a retail store offering for sale its usual or customary inventory of books, magazines and/or related pharmaceutical materials.

(n) A flea market.

(o) No Party shall knowingly use, or permit the use of, Hazardous Materials (as hereinafter defined) on, about, under or in its Tract, or the balance of the Development Site, except in the ordinary course of its usual business operations conducted thereon, and any such use shall at all times be in compliance with all Environmental Laws (as hereinafter defined). Each Party agrees to defend, protect, indemnify and hold harmless each other Party from and against all claims or demands, including any action or proceeding brought thereon, and all costs, losses, expenses and liabilities of any kind relating thereto, including but not limited to costs of investigation, remedial or removal response, and reasonable attorneys' fees and cost of suit, arising out of or resulting from any Hazardous Material used or permitted to be used by such Party, whether or not in the ordinary course of business. The term (ii) "Hazardous Materials" shall mean: asbestos, polychlorinated biphenyls, radioactive materials and all other dangerous, toxic or hazardous pollutants, contaminants, chemicals, materials or substances listed or identified in, or regulated by, any Environmental Law, and (ii) "Environmental Laws" shall mean: all federal, state, county, municipal, local and other statutes, laws, ordinances and regulations which relate to or deal with human health or the environment, all as may be amended from time to time.

(p) A service station shall only be permitted on Outlot Tracts, and there shall be no more than two service stations located at the Development Site. No service station shall be on a parcel greater than 60,000 square feet, and no service station shall be located at the entry to the Development Site. Fueling stations shall be of the size and shall not exceed the number customarily located in residential neighborhoods. No service bays shall be permitted. Service stations must be property landscaped as required within the Landscaping Section of this criteria. Any service station shall be of similar architectural quality and shall be consistent with the architectural themes of the Development Site. No fuel pumps shall be located on any Tracts other than Outlot Tracts on which a service station is located.

(q) No merchandise, equipment or services, including but not limited to vending machines, promotional devices and similar items, shall be displayed, offered for sale or lease, or stored within the sidewalks, parking and/or driveways; provided, however, the foregoing prohibition shall not be applicable to: (i) the permanent storage of shopping carts used by retail customers, so long as reasonably screened; (ii) the seasonal display and sale of bedding plants on the sidewalk in front of any Building with the Landlord's approval; (iii) temporary Development Site promotions, except that no promotional activities will be allowed without the prior written approval of the Landlord; (iv) any recycling center required by law, the location of which shall be subject to the approval of the Landlord; (v) any designated Outside Sales Area, which Outside Sales Area may be designated by any Owner at any time without limitation by written notice of the Landlord, until all structures in a Tract have been fully completed; or (vi) kiosks, merchandise carts, or any other similar structures approved by the Landlord.

Building Setback Lines

All Buildings shall be erected so as not to encroach over the Building Setback Lines as indicated within the Landlord Venture Agreement.

Height Restrictions

All Buildings, structures, mechanical features, architectural features or similar element are limited to seventy-five (75) feet, except:

(a) Notwithstanding the provisions of this subsection, towers, tower antennae, satellite receiving or transmitting equipment or architectural element may exceed these heights if approved by the Landlord and the

City of Sparks, NV.

Parking

(i) Adequate off-street parking shall be provided by each Owner and tenant. Each Owner and/or Occupant must meet the minimum parking requirement of local codes and ordinances and the respective Venture Agreements.

(ii) Building entrances shall be located and site layout shall be designed in such a manner which discourages patrons from parking in an area which ownership differs from the ownership of the destination tract.

(iii) Employees must park and work a tract with same ownership.

Sidewalks

All sidewalks and pedestrian aisles shall be concrete or other materials (stained or colored concrete, concrete pavers, bricks).

Screening

Exterior mechanical and electrical equipment, including without limitation air conditioning equipment, air handling equipment, transformers, pump houses, communication towers, vents and fans, whether mounted on the roof or walls of any Building or on the ground, shall be placed or screened pursuant to this tenant handbook so that the predominant design lines of the Building or structure continue without visual distraction or interruption. If, in the judgment of the Landlord, the Building exterior walls do not adequately screen any such equipment, such equipment shall be separately screened either by approved building materials or otherwise, such as by berming or dense landscaping. The height of any such screening shall be at least equal to the height of the equipment to be screened.

Loading Docks

Unless otherwise approved by the Landlord, loading docks shall be located in the rear of the Building on each Tract and reasonably screened from view so that trucks using such docks will not be readily visible from the Site Roads. Such screening shall be accomplished by the applicable building or through the use of landscaping, walls or decorative fencing. Except during the process of loading or unloading, trucks and commercial vehicles (other than buses) shall not be parked or stored on any Tract in the Development Site, unless parked in the rear of a building on such Tract or in suitably screened areas.

Outside Storage

No outside storage of any type will be permitted without the written approval of the Landlord. When such approval is given, the outside storage area shall be totally enclosed with a screen of approved materials so that storage is not visible from the neighboring Tracts or from any adjacent Site Roads. The foregoing does not apply to any period of any construction or reconstruction of buildings on any Tract.

Building Materials

The Landlord shall adopt standards for building facades, which provide architectural continuity throughout the Site while maintaining design flexibility and corporate identity. The primary intention of establishing standards is to seek excellence in design throughout the development. Material guidelines are specifically intended to specify a palette and finish range for all facade materials used throughout the Site.

Walls

Special attention should be placed on the facade guidelines for walls with high visibility from public streets and walls along the rear of buildings and loading dock areas. Rear walls fronting neighborhoods or streets will be embellished/appointed so as to minimize the "rear facing" nature of the building facade and to minimize any such "rear facing" effect.

Paving

A selection of paving materials shall be established for the following outside public areas:

(i) Public areas such as building entrances and plazas;
(ii) Vehicular/pedestrian interfaces such as street intersections or pedestrian crosswalks; and,
(iii) All other paved areas used for non-vehicular activities.

Use of stone paving, Grasscrete or special pavers are encourage in order to identified special areas. Asphalt and/or composite asphalt paving is not acceptable.

Masonry

All brick masonry must adhere to ASTM-C216-87 for FBS brick and ASTM C90 or comparable standards. Glazed brick and glazed tile may be used for accent and fine details only. Glass block is not considered highlight brick, but may be used at the owner's discretion. Concrete masonry units are acceptable construction materials when the units have an architectural finish and are used as an accent material. All masonry finishes are subject to the approval by the Landlord.

Metals

All metals, for roofing and flashing shall be factory finished. All exterior ferrous metals should be galvanized.

Wall Panel Systems

The use of wall panel systems in architectural finishes is subject to approval of the Landlord.

Cast Stone/Pre-Case Concrete

Precast concrete systems are acceptable construction materials but are subject to the Landlord's approval.

EIFS

EIFS systems are an acceptable construction material but are subject to the Landlord's approval.

Veneering

Use of stone as a veneer is encouraged and use of native stone is also encouraged.

Roofing

All roofing shall be light in color. All roofing shall be visually homogenous. All roof apparatus, whether ventilation equipment, HVAC equipment, or other, shall be screened as required, as well as consistent in color with roof finish.

Wood

Wood siding and wood shingles are not acceptable as a permanent veneering material unless approved by the Landlord.

New Technologies and Synthetics

The use of new technologies and materials, in new ways and for new uses is encouraged. All materials are subject to the Landlord's approval.

LANDSCAPE CRITERIA

This pedestrian-oriented shopping mall will be a unique shopping environment. We encourage the use of high quality planting with colorful flowerbeds. Blooming of these flowers throughout the growing season will enhance both the uniqueness of the Shopping Center and the quality of the Shoppers visit.

The Landscape Plan shall meet or exceed The Legends at Sparks Marina Planned Development Landscape Standards.

The Landlord's Landscape Architect shall approve all plant materials used. A list of approved plant materials shall be provided upon request.

Purpose

The purpose of this section is to establish standards for providing adequate landscaping in and adjacent to parking areas and driveways serving those parking areas, to carry out the following objectives:

(1) provide screening and buffering between parking areas and adjacent uses, structures, and public alleys and roadways;
(2) provide shading for paved parking areas;
(3) provide adequate green space and planting areas in and adjacent to parking areas;
(4) decrease storm water run-off and erosion;
(5) aid in directing the circulation of vehicles and pedestrians;
(6) identify entrances and exits;
(7) conserve or enhance property values;
(8) improve the relationship between the site design of the proposed development and public safety issues such as fire safety and crime prevention; and
(9) improve the appearance of parking areas and the overall area.

Approval

All landscaping design shall be approved by the Landlord and the City of Sparks, NV.

Parkway Landscape Zone

A landscape zone the width of the required set backs shall be provided adjacent to all public streets and access easements per the Planned Development Handbook guidelines.

Trees

Trees shall be planted per the Planned Development Handbook guidelines.

Parking Lot Landscaping

Parking areas shall have trees and landscaping planting areas. The shade tree requirements shall be calculated as not less than 1 tree per 10 parking spaces, per the Planned Development Handbook Guidelines.

Each tree planting island shall contain at least 64 square feet of area per ornamental tree and 100 square feet per shade tree.

Landscape planting areas which equal at least 10% of the parking area square footage shall be provided for all off-street parking areas.

Maintenance

All landscaping must be irrigated with an automatic system. Plants that die must be replaced within six months.

Open Areas

All open areas on each Tract not occupied by Buildings and paved areas, shall be suitably graded and drained and shall be landscaped with lawns, trees and shrubs.

Development Standards

The Planned Development Handbook provides for specific landscaping standards, and each Party and Occupants of each Tract shall comply with such standards, including, without limitation, standards applicable to Landscape Buffers.

Landscape Plan

A landscape plan shall be submitted as necessary to show conformance with this section. The plan shall be prepared by an individual knowledgeable of plant materials, landscape and site design, construction processes, and growing conditions in this region.

SITE LIGHTING DESIGN

This section shall apply to all lighting installed for illumination of parking lots. This section does not apply to any fixtures mounted in or on buildings or common gathering structures.

All site lighting installed within the development property shall be designed according to the following standards.

(A) All fixtures shall be of Metal Halide type and include High Power Factor Ballasts. High-pressure sodium light fixtures are prohibited.

(B) All parking surfaces, walkways, and drive spaces shall be illuminated such that 0.9 footcandles (fc) min. is maintained at ground surface. Average illumination for the same spaces shall be minimum 3.0 fc. Setback and landscaped areas, which do not contain walkways or parking, are not bound by these requirements.

(C) Footcandle levels shall be shown out to the lot line or to a zero footcandle level. These levels shall be shown on the drawings but not included in calculations obtained in B) above.

(D) Uniformity fc ratios:
Shall not exceed 5:1 average-to-minimum
Shall not exceed 20:1 maximum-to-minimum
These uniformity levels shall be followed unless the City having jurisdiction designates stricter requirements. Verification of requirements for the area having jurisdiction must be completed and implemented prior to submitting plans.

(E) Site lighting poles for main parking lot areas shall not exceed 43 feet in height measured from the ground surface. Calculation of the pole height shall include the base if a base is used.

(F) Light poles used shall match the Landlord's standard pole in all locations unless otherwise approved by Landlord. Exceptions for walkways and common gathering places may be granted if deemed appropriate for the development by the Landlord. No substitutions of pole manufacturer will be allowed.

(G) Each pole shall be installed upon a round concrete base with 2'-6" height above grade and 24" diameter.

(H) Fixtures matching Landlord's standard fixture shall be used in all locations. Exceptions for walkways and common gathering places may be granted if deemed appropriate for the development by the Landlord. The finish of the fixtures shall match Landlord's standard unless otherwise approved. Distribution type shall be as needed to meet design criteria. Voltage shall be what is available.

(I) No more than two fixtures may be mounted (at any height) on one pole.

STORE CONSTRUCTION PROCEDURES

To expedite the construction of the Tenant's premises, the Landlord has established the following guidelines.

The construction of the Tenant's premises is to be performed in accordance with the Tenant's approved plans and specifications by a bonded Contractor selected by the Tenant. Tenants needing assistance in locating experienced local General Contractors should contact the Landlord's Representative for assistance. The State of Nevada must license the General Contractors, as well as Heating, Ventilation and Air Conditioning, Plumbing, Gas Piping, Electrical and Sign Subcontractors.

The Tenant or Tenant's Architect must also forward a copy of this Handbook to the Tenant's General Contractor.

The General Contractor selected by the Tenant must provide a full time Superintendent on site at all times during construction and must observe suitable safety practices.

The Tenant's General Contractor shall coordinate all construction staging with Landlord's Coordinator prior to construction.

Contractors and Subcontractors shall not discriminate against any person or group of persons on account of race, sex, marital status, age, handicaps, color, creed, religion, national origin or ancestry. All Contractors shall have good labor relations, be capable of performing quality workmanship and work in harmony with Landlord's Contractors and other Contractors on the job, and any other labor entity at or servicing the center. Tenant's Contractor shall cooperate with the Landlord's Construction Superintendent and any other Contractor by coordinating its work in order not to delay other work in progress, interfere with the operations of existing stores, or impede or endanger the safety of Contractors and the public.

Permits and Certificate of Occupancy

The Tenant or his Contractor must submit the required number of sets of Landlord approved final working drawings and specifications and apply for all necessary permits, including a building permit from the City of Sparks, Nevada Building Department, and pay all associated fees. All permits and one set of approved drawings must be kept at the construction site during construction.

In addition, if applicable, the Tenant may have to obtain certain permits or approvals from the Health Department or other governing authorities and pay for any applicable fees.

All Contractors must be licensed in the State of Nevada and possess a local business licenses.

It is the Tenant's Contractor's responsibility to schedule periodic inspections by the appropriate Building Department and other inspectors as necessary and to comply with their requirements and all codes and regulations. Inspections should be scheduled so that there is adequate time to make any changes required by the inspectors prior to the store opening.

Tenant shall obtain a Certificate of Occupancy from the Building Department and submit a copy of this certificate to the Landlord's Representative before opening the premises to the public. The original must be posted within the Tenant space.

PREREQUISITES TO TENANT CONSTRUCTION

Before the Tenant's Contractor will be permitted to start construction, the Contractor must comply with the following prerequisites:

Pre-Construction Meeting

A pre-construction meeting must be held with the Landlord's Construction Coordinator to determine suitable access routes to the site and the premises, designated parking for Contractors, designated loading, unloading and storage areas if available and approved by the Landlord.

Bi-weekly Construction Meetings

The Tenant's General Contractor shall schedule bi-weekly meetings with the Landlord's Coordinator to update Construction Schedules and conformance with documents.

Bond and Insurance

Any Contractor engaged by the Tenant having a Contract shall furnish a performance bond naming both the Tenant, the Landlord's Lender and the Landlord, as additional insured, as their interest may appear. Evidence of the bond must be filed with the Landlord's Construction Coordinator before construction may begin. Insurance requirements should follow the details under Insurance Exhibit within the Tenant's Lease.

Progress Schedule

The Tenant's Contractor shall promptly submit to the Landlord's Construction Coordinator a progress schedule or bar chart showing the work schedule and anticipated completion of the store.

Changes In Design or Materials

Tenant's Contractor shall not deviate from approved drawings and specifications without obtaining prior written permission from Tenant, Landlord's Construction Coordinator, and the Building Department and/or other governmental agencies.

Demolition

Tenant's Contractor shall perform all demolition of existing construction necessary to build the new store per the approved plans and specifications. Previous tenant construction can be retained provided it is reused and returned to "as-new" condition (e.g. air handling units, toilet rooms, etc.) All abandoned equipment, conduit, ceilings, lights, ducts, etc. which is not being reused, must be removed and properly disposed of. All active utility lines must be properly capped if not being reused.

Construction Utilities

During construction, the Tenant's electrical contractor shall be required to provide ground fault protection for all power equipment used in the premises.

Access to Roof

Access to shopping center roof is restricted to the Landlord's personnel and the Landlord's designated Contractors. No Contractor or Subcontractor will be permitted on the roof unless specific permission has been obtained from the Landlord's Construction Coordinator. The penetration of the roof deck and the installation of all flashing and curbing for Tenant related equipment must be by the Landlord's roofing Contractor at the Tenant's expense. Installation of equipment on the roof will be by the Tenant's Contractor.

Rubbish Removal

The Tenant or Tenant's Contractor shall be responsible for the daily removal of construction debris from the premises and the site. Should the Tenant fail to comply with the above the Landlord will proceed, upon 24 hour notice, with the necessary cleanup, demolition and removal, and charge the Tenant accordingly. Curb and wall damage will be deducted from Tenant Construction allowance.

The week before the Shopping Center Grand Opening and beyond all Tenant dumpsters shall be centralized. Each unfinished Tenant space will be charged a pro-rate rubbish fee until the Tenant space is complete.

Courtyards, public corridors, service corridors and adjacent Tenant spaces must be kept clear of all trash, refuse, fixturing and merchandise at all times.

Damage

Tenant and his Contractor shall be responsible for the cost of any damage to structure, utilities or vehicles caused by any persons or equipment under Tenant Contracts.

Permits Inspection and Acceptance

Tenant's Contractor must provide evidence of all necessary permits prior to start of construction. It is the Tenant Contractor's responsibility to schedule inspections by the appropriate building department and other inspectors as necessary, and to comply with their requirements, and all codes and regulations. A copy of all inspection reports must be submitted to the Landlord Construction Coordinator. From time to time, and upon completion, Landlord's Construction Coordinator will inspect Tenant's construction for compliance with approved drawings and specifications. Deviations or unsatisfactory workmanship must be immediately corrected, regardless of their acceptance by public authority or the Tenant. The Tenant's General Contractor, for prompt completion, will do a punch list and give the punch list to the Landlord Construction Coordinator for review.

Landlord's Prior Rights

The Landlord retains prior rights to the space above 12'-0" of the Tenant's premises, to accommodate the shopping center's structural, mechanical or electrical requirements, such as piping, ducts, conduit, etc.

Furthermore, Landlord, Landlord's agent(s), or an authorized utility company (subject to Landlord's written permission) shall have the right to run utility lines, pipes, conduits, or duct work where necessary or desirable, through air space above the Tenant's ceiling, column space or other parts of the leased premises, and to maintain, repair, alter, replace or remove the same, all in a manner which does not interfere unnecessarily with the Tenant's use thereof.

Violations

In the event the Tenant is notified of any violations of codes, ordinances, or regulations, or of its obligations hereunder, either by the jurisdictional authorities or by the Landlord, Tenant shall correct such violations within seven (7) calendar days, or the Landlord will correct, at Tenant's expense, such violations at Landlord's actual cost plus fifteen percent (15%) cost of administration.

Repair of Adjacent Finishes

It is the Tenant's responsibility to repair all existing Landlord or adjacent Tenant construction finishes which may have become damaged as a result of Tenant's construction to new condition.

Temporary Tenant Enclosure

During construction, the Landlord will require the Tenant's premises to be separated from the mall concourse by a temporary tenant enclosure.

The enclosure shall be 12'-0" high and constructed of metal studs with ½" fire rated plywood, with one coat of flat latex paint to match Mall standard and shall be located no more than three feet in front of the lease line. The entire enclosure must be installed on carpet strips so as not to damage the Mall flooring. Black visqueen shall be installed between the top of the temporary tenant enclosure and Landlord's bulkhead to prevent dust from migrating to the common area.

An access door will be permitted in the temporary tenant enclosure only if there is no rear service door. Modifications to the temporary tenant enclosure during construction will be the Tenant Contractor's responsibility and shall be done immediately.

Clean-Up

Tenant shall provide daily cleanup of its premises and the surrounding area during construction, and the removal of all construction debris from the Tenant space to the trash containers. Upon completion of Tenant's construction, Tenant shall promptly dismantle, remove and dispose of the temporary tenant enclosure and any excess materials from the premises. Should the Tenant fail to comply with the above, Landlord will proceed, upon 24 hours notice, with the necessary cleanup, demolition and removal, and charge the Tenant.

Security

Access to the site will be restricted to only those employees working on the project.

Contractors wishing access to the site after normal working hours must obtain approval from the Landlord Construction Coordinator.

Waivers of Lien

In making progress and final payments to Contractors, Tenant should obtain valid Waivers of Lien, indicating payment in full for labor, materials, and subcontractors. Landlord may request to see evidence of such Waivers before permitting Tenant to open the premises for business.

Certificate of Occupancy

Tenant shall obtain a Certificate of Occupancy from the City of Sparks, Nevada and submit a copy of this certificate to the Landlord's Construction Coordinator before opening the premises to the public. Further, turn over "as-built" drawings at the completion of the punch list and for final inspection by the Landlord Construction Coordinator.

Tenant shall provide monthly updates to Landlord for lost construction days due to weather beyond the normal weather conditions of that region.

SIGNAGE CRITERIA

OBJECTIVE

The objective of the sign design guidelines is to provide standards and specifications that assure consistent quality, size variety and placement for Tenant signs throughout the shopping center.

The guidelines are intended to stimulate creative invention and achieve the highest standard of excellence in environmental graphic communication. We encourage open and frequent dialogue between Tenant, Landlord, and the project's graphic design consultant.

Excellent signs at The Legends at Sparks Marina are an integral part of the overall experience, of the center's image and visual appeal. Signs will be evaluated for appropriateness in terms of their placement and scale relationship to the architectural facade on which they are placed. Quality design, fabrication, materials, and installation will enhance the overall customer experience and contribute to the overall project success.

OVERVIEW

The Landlord is responsible for:

A. Provide a standard base building design and construction package to the tenant's design consultant.

B. Convey and communicate the criteria that will be used to evaluate tenant storefront and identity signs.

C. The review, revision and approval of the tenant sign submittals.

The Tenant will be responsible for the:

Design, approval submittal, fabrication, permitting and installation of signs, including any electrical service and/or structural support and any special installation conditions requiring modification to the shell building approved by the Landlord.

The Tenant shall employ sign fabricators and installers approved by the Landlord. Demonstrated experience and qualifications in the techniques and procedures required to implement the sign's approved design intent are mandatory.

The Tenant and Tenant's design consultant will abide by all provisions, guidelines and criteria contained within the Sparks Municipal Code, the Planned Development Handbook, and these sign criteria.

Only those sign types, and approved quantities, provided for and specifically approved by the Landlord in Tenant sign submission documents will be allowed. The Landlord may, at the Landlord's discretion and at the Tenant's expense, correct, replace or remove any sign that is installed without Landlord's written consent, or that is not executed in conformance with the approved submission.

The Tenant must supply the Landlord with a copy of all sign fabrication and installation permits prior to installation.

DESIGN

Primary Identification Signs for The Legends at Sparks Marina Tenants

A. The Legends at Sparks Marina sign criteria supersedes all less restrictive codes by the City of Sparks, NV. If the Tenant can show good cause for a deviation from size limitations outlined below, which does not violate any Sparks written sign code, the Planned Development Handbook, or other regulation regarding the size and placement of storefront signs, the Landlord will consider such requests.

B. Tenant is allowed one primary identification sign located in the designated sign zone just above the store entrance.

C. Tenants with building elevations facing multiple exposures may incorporate one additional primary identification sign per exposure, with a maximum of two per store, subject to Landlord's approval.

D. Signs may identify the business name and a minimum generic word description of the service. No product identity or specific service descriptions may be displayed.

E. Unless treated as the primary identification sign, blade signs, flags, banners and window text sign, where permitted, shall not count against the Tenants' overall sign square footage allowance.

F. Sign size is based upon the Tenants leased frontage, as measured in a straight line from the lease line to lease line for each elevation. Tenants are allowed one square foot of primary sign area per lineal foot of store frontage.

G. The overall width of any sign shall not exceed 80% of any uninterrupted architectural treatment.

Primary Identification Sign Exception for Outparcel Tenants

A. Outparcel buildings may have signs on three elevations based on the formula of one square foot of sign area per lineal foot of frontage onto the façade to which it is attached. One additional monument sign will be allowed. The monument graphics and materials must be compatible with the Landlord's standard monument sign structure.

B. Identification of primary and secondary outparcel façades is subject to Landlord approval. No Tenant may have more than one primary and two secondary façades. Any additional signs on other elevations are subject to landlord approval.

SIGN TYPES

Monument Signs, Outparcel

A. A single Outparcel monument sign is allowed for each Outparcel. Only one monument sign will be permitted and must be located with the Landlord's approval.

B. The sign shall be constructed as indicated and shall not exceed 75 square feet in area and 10 feet in height. Sign setback shall not be less than 10 feet from the property line. The Tenant's monument graphics and materials must be compatible with the standard monument sign structure, designed by the Landlord's signage consultant.

C. The actual signage shall be illuminated letters on an opaque background. The letters may be individual channels or halo-lit. Sign boxes or cabinets will not be allowed. Remote lighting may be used to enhance the monument sign. All monument signs shall have required landscaping at base, subject to Landlord approval.

Projecting Signs

A. For some Tenants, the Landlord may require that the Tenant's primary identification sign be a projecting sign. The size and placement of these projecting signs will vary depending upon the store frontage and location, and will be determined at the sole discretion of the Landlord. Projecting signs will be sized to complement the architectural elements on which they are placed. For Major Tenants occupying buildings of 25,000 square feet or more, these signs will project a maximum of five (5'-0") feet from the building and have a maximum height of twenty (20'-0") feet. However, the exact size of the individual sign will be determined during the design and submission process, and will be subject to the Landlord's final approval.

B. Where a projecting sign becomes the Tenants primary identification sign, the Landlord will permit a secondary storefront sign or marquee sign visible to pedestrians at the level of entry.

C. For other Tenant's where the landlord will allow projecting signs, the signs may project a maximum of three feet eight inches (3'-8") from the building and have a maximum height of ten (10'-0"). The size of each sign is to be determined during the design and submission process and must complement the architectural style and scale of the area receiving the sign subject to the subject to the sole discretion of the Landlord.

Rooftop Signs, Primary Development

A. 15% of total number of tenants will be allowed rooftop signage.

B. The sign shall be constructed to compliment the design of the development. Signs will be evaluated for appropriateness in terms of their placement and scale relationship to the architectural facade on which they are placed. All rooftop signs are subject to Landlord approval.

C. The rooftop sign shall not exceed 40% of the overall storefront square footage.

Storefronts

A. For Major Tenants occupying buildings of 25,000 square feet or more, the maximum height for letters of a wall sign shall not exceed 54". Wall signs shall be as located on the approved building elevations and shall not to exceed 1 sign per storefront with a maximum of 3 signs total.

Signs shall not extend more than 10" beyond the face of the surface to which the sign is mounted.

B. For Tenants occupying buildings less than 25,000 square feet, the maximum height for letters of a wall sign shall not exceed an average cap height of 30".

Signs shall not extend more than 8" beyond the face of the surface to which the sign is mounted.

C. Reverse "halo" lighting is acceptable and encouraged.

D. Wall signs shall be illuminated individual letters mounted to the exterior building face/wall. There shall be no raceways/signage backer panels applied to the Landlord provided sign band. A colored or frosted Plexiglas face is required.

E. "Tag lines" will be allowed at a maximum of 10" high. "Tag lines" shall be directly below the wall sign and shall not extend wider than the approved wall sign.

F. Double stacked letters will be allowed but the total height shall not exceed 1.5 times the allowable height for the wall signs.

Blade Signs

A. Each tenant is required to have one blade sign. The blade sign may not be the primary store identification sign and will not be included in the calculation for the overall sign area permitted. The blade sign program requires that each Tenant's graphic identity be transformed into a three-dimensional double-faced sign. The Landlord encourages the tenant to propose blade sign designs, which enrich the pedestrian environment with a creative use of color and material combined with a strong store name and identification.

B. Additionally:
Blade signs may be illuminated or non-illuminated. Illuminated signs may have an internal or external light source.

C. Blade signs shall project no more than 3'-6" from the building face, and shall be no more than 3'-0" in height, with a maximum of 7-1/2 square feet of area for each face. Clearance from the underside of the blade sign to the finished common area paving shall be a minimum of 8'-0".

D. Proposals for blade sign designs will be reviewed at the time of the tenants overall sign design submission. It is the responsibility of the tenant to ensure that his fabrication and installation contractor includes adequate support for the blade sign and all required electrical services and connection.

COMPONENTS

Sign Styles & Materials

There are many acceptable sign treatments including a mixed media approach combining several different fabrication and lighting techniques. Tenants are strongly encouraged to consider the specific architectural style of their façade, the overall concept of the project, the scale of the proposed sign and the critical viewing angles and sight lines when designing appropriate graphics and signs for the storefront. Note that specific locations and surrounding architectural treatments can limit the maximum sign height and length, which may differ from the general guidelines proposed above. The Landlord reserves the right to approve or reject any proposed sign on the basis of its size, placement and style.

Acceptable sign styles include:

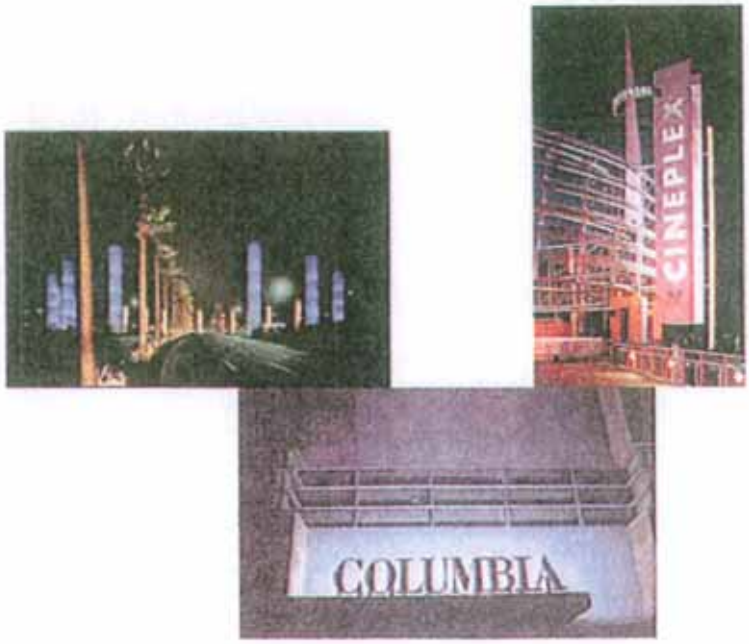
1. Front and Halo illuminated channel letters. Note: acrylic face, internally illuminated channel letters will not be permitted unless successfully used as a part of an approved "mixed media" sign;
2. Mixed media, three dimensional signs painted gold, silver or copper leaf; Halo illuminated letters, 3" deep minimum;
3. Channel letter with exposed neon, 2-1/2" deep minimum. Exposed neon will be approved at the sole discretion of the Landlord and should be proposed only if a part of the overall tenant design concept. However, neon will not be allowed in any storefront windows;
4. Dimensional, geometric shapes; Screens, grids, or mesh; Etched, polished, patina or abraded materials;
5. Sand blasted, textured, and/or burnished metal-leaf faced dimensional letters, pin mounted from façade;

6. Signs mounted to hard canopies, eyebrows or other projecting architectural elements

   For example:
   a. Prismatic face letter forms with full facet strokes;
   b. Rounded face letter forms with radius faces and eased edges

Logos and Typestyles
The use of logos and distinctive type styles is encouraged for all tenant signs. Sign lettering should be combined with other graphic and or dimensional elements denoting the type of business. The typeface may be arranged in one or two lines of copy and may consist of upper and/or lower case letters. The tenant should identify trademark-protected type and marks in their sign submission to assist the Landlord in the review process.

Color
The following guidelines are for selecting colors for Tenant's signing. The project and the individual building façade will consist of a variety of colors and materials. The Landlord encourages the tenant to consider these colors when choosing sign colors, and where feasible the Landlord will consider the tenants color scheme when making final building color and material choices. Tenants are requested to make early color submission for review by the Landlord, although final determination of building colors will follow from on site mock-ups and drawdowns reviewed and approved during construction of the base building shell.



Lighting
Tenant signs should be creatively illuminated using a variety of lighting techniques.

One or more of the following are allowed:

1. Front and/or halo illuminated pin-mounted reverse channel letters;
2. Reverse channel neon with silhouette illumination
3. Open channel neon with exposed neon, fiber optics, incandescent light bulbs are subject to the scrutiny by the Landlord.
4. Internally illuminated signs with seamless opaque cabinets and pushed-thru lettering and/or neon. Visible neon will be approved at the sole discretion of the Landlord and should be proposed only if a part of the overall tenant design concept.
5. Cove lighting

All front lighting must be baffled and obscured from direct visibility with recessed channels, which are fully integrated into the building façade elements. Visible weep holes on all letters and cabinets must be baffled. Exposed lamps or tubing, raceways, crossovers or conduits will not be permitted.

Decorative shrouds or housings custom designed and fabricated to maintain or enhance the architectural integrity of the building may be used to conceal "off the shelf" standard fixtures subject to Landlord's approval. Visible standard (non-custom) "gooseneck" lamps and similar fixtures will not be approved. All housings and posts for exposed neon signs must be painted to match or compliment the building façade color immediately behind and adjacent to the sign.

All illuminated signage will be controlled by a time clock and shall have a dimmer switch, located for easy access by Landlord.

All electrical signs and installation methods must meet UL standards and contain a UL label.

PROHIBITED SIGNS

1. Rectangular boxed or cabinet signs.
2. Flashing, oscillating, animated lights or other moving sign components, except as specifically approved by the Landlord.
3. Noise and odor making signs, except as specifically approved by the Landlord.
4. Exposed junction boxes, wires, transformers, ballast boxes, lamps, tubing, conduits, raceways or neon crossovers of any type.
5. Exposed fasteners, unless essential to the sign design concept. (Subject to approval)
6. Signs using trim-cap retainers that do not match the color of the letter or logo returns (polished gold, silver or bronze trim caps are not permitted).
7. Pre-manufactured signs, such as franchise signs, that have not been modified to meet these criteria.
8. Formed plastic or injection molded plastic signs.
9. Manufacturers' labels, underwriters' labels, brackets, or any other form of extraneous advertising attachment or lighting devices shall be fully concealed from public view.
10. Fabric awning signage, except as specifically approved by the Landlord.
11. Temporary wall signs, pennants, banners, inflatable displays or sandwich boards.
12. Window signs. Decals and lettering or other signing indicating products lines or credit card acceptability shall not be permitted on the storefront glazing other than stores operating hours. Note: box signs and neon hanging in display windows are not allowed.
13. Gold leaf treatments on windows.
14. Paper, cardboard or Styrofoam signs, stickers, or decals hung around or behind storefronts. Signs, letters and symbols painted directly on exterior surfaces
15. Simulated materials such as wood grained plastic laminates or wall coverings.
16. Rooftop signs or signs projecting above roof lines or parapets, except as specifically approved by the Landlord.
17. Signs on mansard roofs or equipment screens, except as specifically approved by the Landlord.
18. Advertising or promotional signs on parked vehicles.

SUBMITTAL & APPROVAL

The Landlord has hired the services of a sign consultant who will assist in the review and approval of tenant sign submissions. This is to insure each Tenant's conformance to the project's overall Sign Criteria.

1. Upon receiving final plan approval from the Landlord's architect, the Tenant shall submit two sets of complete and fully dimensioned shop drawings of the tenant's sign, to the Landlord's sign design consultant, at the following address:

   Jeff McMahon
   RED Design Services
   4717 Central — Phone - (816) 777-3500
   Kansas City, MO 64112 — Fax - (816) 777-3501

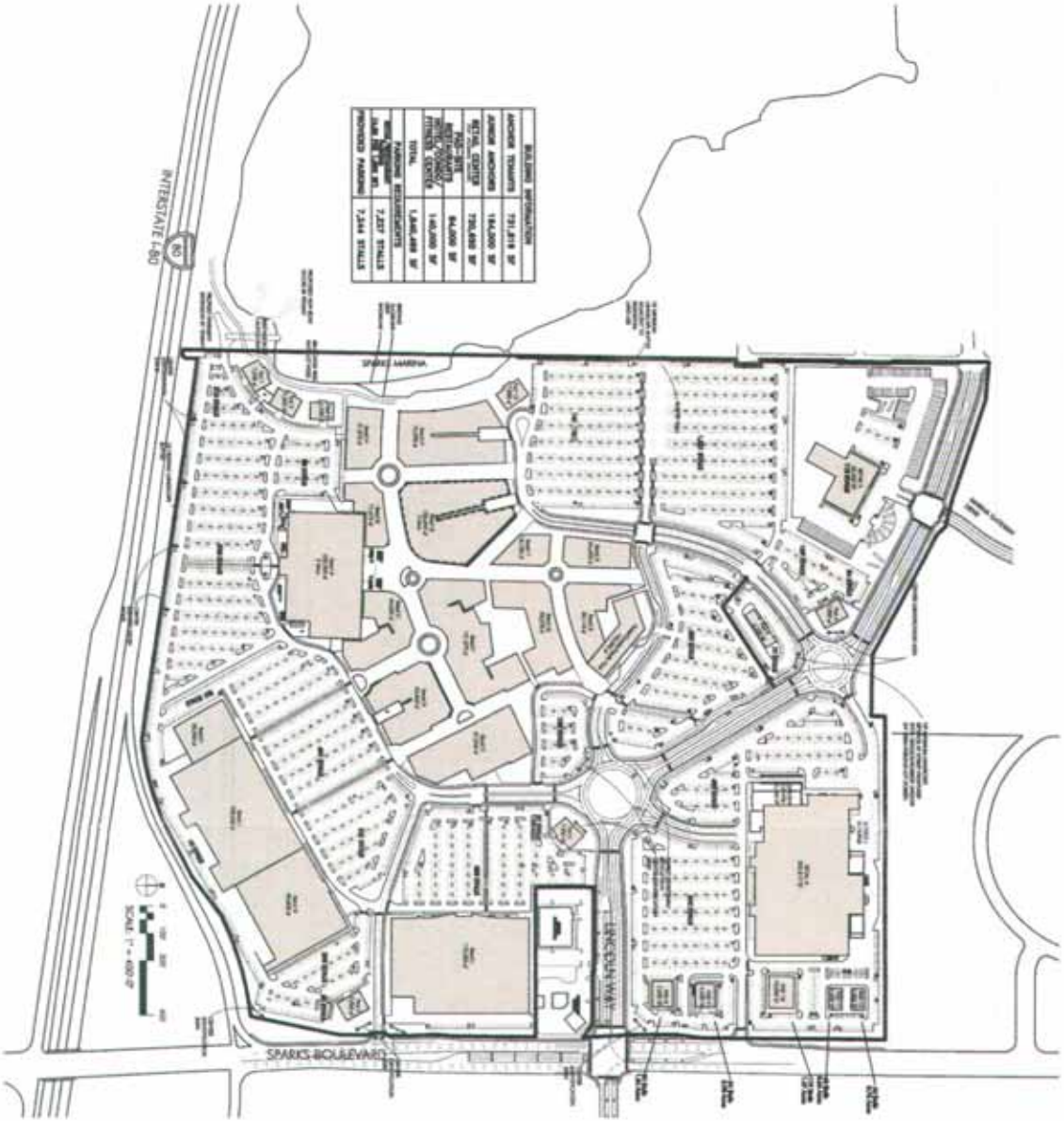
2. Shop drawings shall include at least the following:
   a. Tenant's entire building façade elevation, showing the proposed sign, in color drawn to scale of 1/4" - 1'-0".
   b. Storefront (partial building) elevation, showing the location.
   c. Size, color, construction and installation details of the tenant's proposed sign.
   d. Typical "section-through" letter and/or sign panel showing the dimensioned projection of the letter or panel face and the illumination method.
   e. Color and material samples board with a photograph (if possible) of a similar installation.

3. Within twenty one (21) days of receipt of the sign submission, the Landlord's representative will approve, as noted, or disapprove with comments the Tenant's sign design. Tenant must respond to the Landlord's comments and re-submit within ten (10) days of their receipt, and repeat this process until all sign design, fabrication and installation issues are resolved to the Landlord's satisfaction. Allow reasonable time for Landlord's review and Tenant's revision of submission in advance of sign fabrication.

4. The Landlord reserves the right approve exceptions to these standards on a case-by-case basis.

5. Upon receipt of final sign approval, permits required for fabrication and installation shall be obtained by the Tenant, at the Tenant's expense. Submit required drawings to the City of Sparks, NV.

**It is the responsibility of the Tenant or Tenant's architect to field verify all conditions and dimensions within the leased premises before commencing work.



THE LEGENDS AT SPARKS MARINA

EXHIBIT A-11
CONCEPTUAL ALTERNATE
DEVELOPMENT PLAN

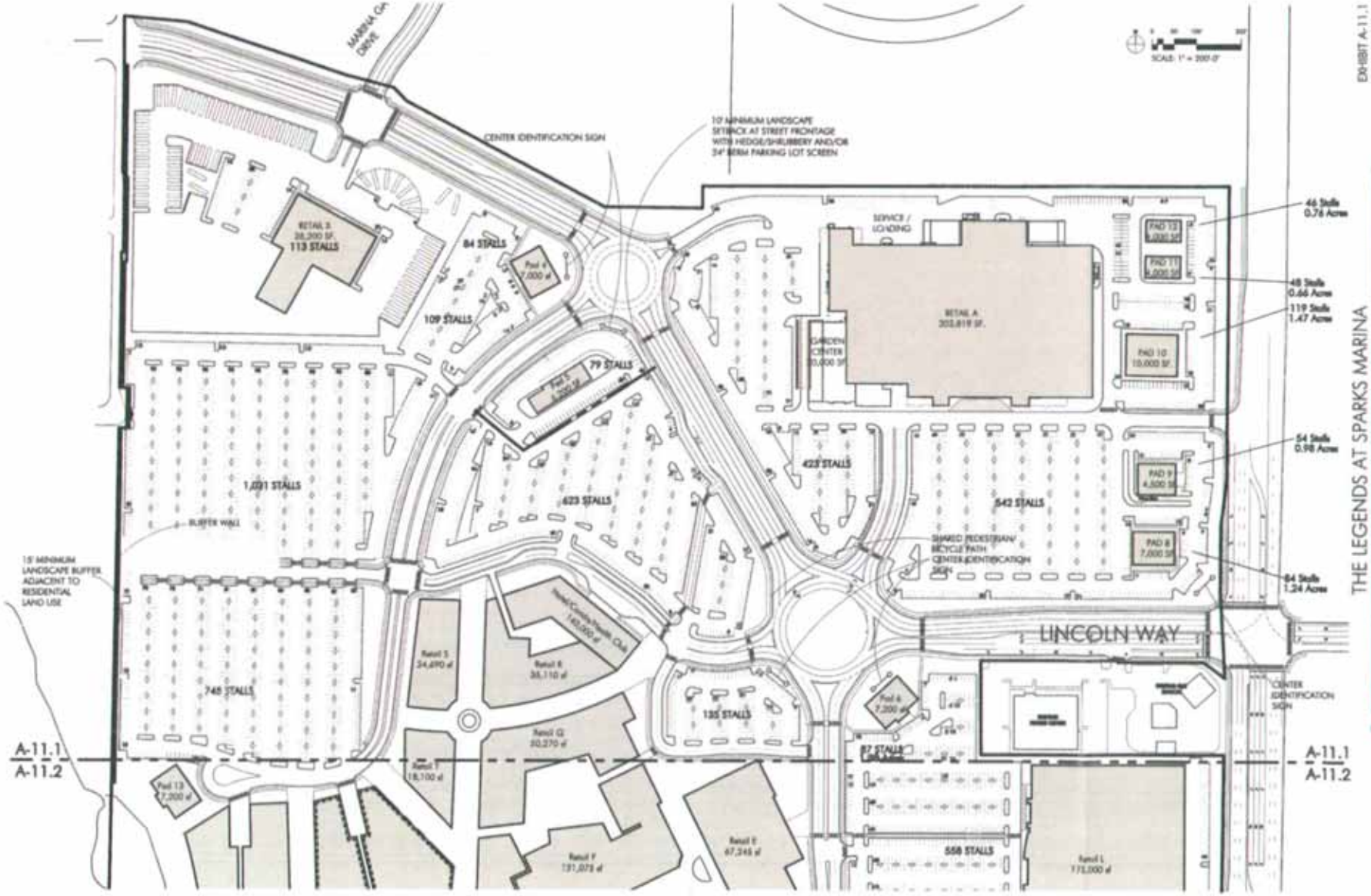


EXHIBIT A-11.1
CONCEPTUAL
DEVELOPMENT PLAN
THE LEGENDS AT SPARKS MARINA
SCALE: 1" = 300'-0"
MARINA CH DRIVE
CENTER IDENTIFICATION SIGN
10' MINIMUM LANDSCAPE SETBACK AT STREET FRONTAGE WITH HEDGE/SHRUBBERY AND/OR 3'-4' BERM PARKING LOT SCREEN
RETAIL B 28,300 SF.
113 STALLS
84 STALLS
Pad 4 7,000 sf
109 STALLS
79 STALLS
Pad 5 4,500 sf
1,031 STALLS
BUFFER WALL
15' MINIMUM LANDSCAPE BUFFER ADJACENT TO RESIDENTIAL LAND USE
623 STALLS
745 STALLS
Retail S 24,890 sf
Retail R 36,110 sf
Retail Q 50,270 sf
Retail T 18,100 sf
Pad 13 7,200 sf
Retail F 131,073 sf
Retail E 67,245 sf
135 STALLS
SERVICE / LOADING
RETAIL A 303,819 SF.
GARDEN CENTER 20,000 SF
423 STALLS
542 STALLS
SHARED PEDESTRIAN/ BICYCLE PATH CENTER IDENTIFICATION SIGN
PAD 12 6,000 SF
PAD 11 6,000 SF
PAD 10 10,000 SF
PAD 9 4,500 SF
PAD 8 7,000 SF
46 Stalls 0.76 Acres
48 Stalls 0.66 Acres
119 Stalls 1.47 Acres
54 Stalls 0.98 Acres
84 Stalls 1.24 Acres
LINCOLN WAY
Pad 6 7,200 sf
87 STALLS
558 STALLS
Retail L 175,000 sf
CENTER IDENTIFICATION SIGN
A-11.1
A-11.2
A-11.1
A-11.2


EXHIBIT A-11.2
CONCEPTUAL
DEVELOPMENT PLAN
THE LEGENDS AT SPARKS MARINA
SPARKS BOULEVARD
SPARKS MARINA
A-11.1
A-11.2
745 STALLS
135 STALLS
87 STALLS
558 STALLS
301 STALLS
601 STALLS
208 STALLS
65 STALLS
88 STALLS
218 STALLS
928 STALLS
201 STALLS
48 STALLS
Retail A
225,000 sf
2 Story
Retail B
17,675 sf
Retail C
44,525 sf
Retail D
53,850 sf
Retail E
67,245 sf
Retail F
121,075 sf
Retail G
186,845 sf
2 story
Retail H
74,290 sf
Retail I
30,000 sf
Retail J
128,000 sf
Retail K
98,800 sf
Retail L
175,000 sf
Retail Q
50,270 sf
Retail R
35,110 sf
Retail S
24,690 sf
Retail T
18,100 sf
Retail U
21,875 sf
Pad 1
7,200 sf
Pad 2
7,200 sf
Pad 3
5,500 sf
Pad 6
7,200 sf
Pad 7
7,000 sf
Pad 13
7,200 sf
EXISTING SHORELINE
NEW SHORELINE
PROPOSED NEW BOAT DOCKS BE TENANT
RELOCATED BIKE PATH (12' WIDE)
EXISTING BOAT LAUNCH RAMP
PROPOSED PAVEMENT EXTENSION BY TENANT
CENTER IDENTIFICATION SIGN
10' FREEWAY LANDSCAPE BUFFER
CENTER IDENTIFICATION SIGNS
SCALE: 1" = 200'-0"

BHC RHODES
CIVIL ENGINEERS • SURVEYORS

July 11, 2006

Mr. Jeff McMahon
Sparks Legends Development, Inc.
4717 Central
Kansas City, MO 64112

Re: Legends of Sparks Development
Traffic Impact Study
Second Revision

Dear Mr. McMahon:

Per your request BHCRHODES has updated the revised traffic study that was completed on March 16, 2006, by BHCRHODES for the Legends of Sparks Development located in Sparks, NV. The original traffic study was completed in September, 2005 by Soleagui Engineers.

This report will take into account the new alignment of Lincoln Way and the five legged roundabout located at the main entrance to the Legends of Sparks. The trip generation for the development has been revised to reflect the current land-use plan.

TRIP GENERATION

BHCRHODES completed a comparison of the trip generation that was reported in the original traffic study dated September 8, 2005, with the most current site plan that reflects the alignment change of Lincoln Way. The attached ***Table 1*** is a copy of the trip generation table that was reported in the original traffic study.

As part of the traffic study revision, BHCRHODES revaluated the trip generation used in the original traffic study. In the original traffic study the quality restaurant land-use was used in addition to the shopping center. In the I.T.E. Trip Generation Handbook, the definition of the Shopping Center (Land Use Code 820) includes the peripheral buildings in the trip generation. This would include facilities such as restaurants, office buildings, movie theaters, post offices, and banks. BHCRHODES used the overall square footage for the entire development in the shopping center trip generation calculation. ***Table 2*** depicts the amount of site-generated trips that would be expected for the current site plan.

As shown in ***Table 2***, the amount of traffic entering the development during the P.M. peak period is expected to increase about 9 percent from the original traffic study and the exiting traffic is expected to increase about 23 percent. It should be noted, that the increase in the site-generated traffic is due to the increase in the amount of square footage

of the Shopping Center. The overall square footage of the Shopping Center has increased from 1,076,700 square feet, reported in the original traffic study completed by Soleagui, to the current plan of 1,597,199 square feet of Shopping Center. During the Saturday peak period, the amount of traffic entering and exiting the development is expected to increase about 14 and 18 percent, respectively.

TRAFFIC ASSIGNMENT

BHCRHODES used the same trip distribution used in the original traffic study (Figure 2 of the original traffic study). The attached ***Figures 1 and 2*** summarize the total traffic volumes that would be expected at the study intersections with the addition of the entire development during the P.M. peak commuter period and the Saturday peak period.

The same pass-by trip rate used in the previous study completed by BHC was used in the traffic assignment. The rates used were 19.4 percent for the Weekday P.M. peak period and 12.7 percent for the Saturday peak period. The existing traffic volumes from the original traffic study were used in the traffic volume development. The attached ***Figures 3 and 4*** summarize the pass-by trip assignment used in this study.

CAPACITY ANALYSIS

BHCRHODES completed intersection capacity analysis at the study intersections located along Sparks Boulevard and Lincoln Way to determine the impacts the new alignment and increase in retail space will have on the improvements that were reported in the original traffic study. The capacity analyses were completed using Synchro/Sim Traffic software. This software allows for the intersections to be analyzed as part of a signalized corridor system versus an isolated intersection analysis used in HCS. HCS Unsignalized Software and aaSIDRA roundabout software were used in the analysis of the unsignalized and the roundabout intersections. ***Table 4*** from the original study was revised to reflect the additional traffic analysis at each of the study intersections located along the development. The levels of service for the existing weekday traffic volumes and the existing lane configurations were the same results reported in the original traffic study. ***Figures 5 and 6*** summarize the lane arrangements and the overall levels of service for each of the study intersections during the P.M. and Saturday peak periods, respectively.

Sparks Boulevard and Lincoln Way: Based on the signalized capacity analyses of the intersection, the overall intersection would be expected to operate with a Level of Service "C" and "D" during the P.M. and Saturday peak periods with the recommended geometric improvements. ***Figures 5 and 6*** summarize the lane arrangements and the overall levels of service at the study intersections.

The estimated 95th percentile queue length for the eastbound dual left-turn lanes is expected to be about 340 feet and 380 feet during the Weekday PM and Saturday peak

periods, respectively. The westbound approach was analyzed with an exclusive left-turn lane and a shared through/right-turn lane. However, to achieve lane balance dual left-turn lanes may be necessary. The estimated vehicle queue length for the westbound left-turn lane is expected to be about 150 feet and 120 feet during the Weekday P.M. and Saturday peak periods, respectively. The estimated left-turn queue for the southbound left-turn lane is expected to be about 70 feet and 130 feet during the Weekday P.M. and Saturday peak periods, respectively. The northbound triple left-turn lanes are expected to experience an estimated vehicle queue of 355 feet and 380 feet during the weekday and Saturday peak periods, respectively.

Due to the increase in overall traffic at the intersection of Lincoln Way and Sparks Boulevard, the southbound and westbound right-turns were analyzed as free flowing right-turn lanes to remove the heavy right-turn movement from the traffic signal. The right-turn lanes should be extended to the upstream driveway (i.e. Driveway 1 for the southbound right-turn lane on Sparks Boulevard and Driveway 3 for the eastbound right-turn lane on Lincoln Way.)

Sparks Boulevard and Westbound I-80: The overall level of service that would be expected at the intersection of Sparks Boulevard and the Westbound I-80 ramps is a Level of Service "A" and "C" during the P.M. and Saturday peak periods, respectively. The northbound through movement was analyzed with three lanes because in the original traffic study, the eastbound I-80 off-ramp was reported to have triple left-turn lanes. Therefore, the three through lanes are required to receive the triple left-turn lanes. The 95th percentile queue length for the northbound dual-left turn lanes would be expected to be about 75 feet and 40 feet during the P.M. and Saturday peak periods.

The overall intersection would be expected to operate with acceptable levels of service. However, the southbound right-turn movement onto the westbound I-80 on-ramp was beginning to reach and exceed the capacity for a single turn lane. The southbound right-turn lane was analyzed with a single free flowing right-turn lane. The single free flowing right-turn lane was expected to operate with a Level of Service "A" and "F" during the P.M. and Saturday peak periods. Due to the capacity concern and poor level of service during the Saturday peak period, the intersection was analyzed with southbound dual right-turn lanes that would be controlled by the traffic signal. With the addition of the second right-turn lane, the level of service during the Saturday peak period would be expected to improve to a Level of Service "D". The estimate queue length would be expected to decrease from about 1390 feet with the signal lane to about 180 feet for the dual right-turn lanes. However, it would be beneficial to the site driveway onto Sparks Boulevard if the second right-turn lane would be extended to the north to Driveway 2. This will allow the exiting right-turn vehicles to enter the traffic stream in their own lane.

Sparks Boulevard and Eastbound I-80: Based on the signalized capacity analyses of the intersection, the overall intersection would be expected to operate with a Level of Service "D" and "C" during the P.M. and Saturday peak periods, respectively. The

intersection was analyzed with dual southbound left-turn lanes and eastbound dual left-turn lanes and one shared left/through/right-turn lane. The southbound dual left-turn lanes were used to acquire lane balance across the bridge based on the number of lanes reported for the westbound ramp in the original traffic study.

The estimated 95th percentile queue length for the southbound dual left-turn lanes is about 220 feet and 235 feet during the Weekday P.M. and Saturday peak periods, respectively. Based on the capacity analysis, the eastbound triple left-turn lanes would be expected to experience a vehicle queue of about 650 feet and 535 feet during the Weekday P.M. and Saturday peak periods.

Lincoln Way and the Main Access Drive (5-Legged Roundabout): The roundabout intersection was analyzed using aaSidra software. Based on the roundabout capacity analysis, the intersection would be expected to operate with an overall Level of Service "A" during both the P.M. and Saturday peak periods. The proposed roundabout was analyzed with an exclusive left-turn lane, a shared left-turn/through lane, and a shared through/right-turn lane for the westbound approach of Lincoln Way. The eastbound Lincoln Way approach was analyzed with a shared left-turn/through lane and a shared through/right-turn lane. The northbound approach was analyzed with a shared left-turn/through lane and an exclusive right-turn lane and the southbound approach was analyzed with an exclusive left-turn lane and a shared left-turn/through/right-turn lane. The eastbound approach of the ball park drive was analyzed with a shared left-turn/through lane and an exclusive right-turn lane.

Because of the complexity of this intersection, BHC has contracted with Ourston Roundabout Engineering to review the roundabout layout and complete additional capacity calculations to ensure that this intersection will work under the proposed traffic conditions.

Lincoln Way and the West Access Drive: Based on the roundabout capacity analysis, the intersection would be expected to operate with an overall Level of Service "A" during both the P.M. and Saturday peak periods. The proposed roundabout was analyzed with a shared left-turn/through lane and a through/right-turn lane for the westbound approach. The eastbound approach was analyzed with a shared through/left-turn lane and a shared through/right-turn lane. The northbound approach was analyzed with shared left-turn/through lane and an exclusive right-turn lane. The southbound approach was analyzed as a single lane approach.

Lincoln Way and the Marina Gateway: Based on the unsignalized intersection capacity analyses, majority of the turning movements would be expected to operate with a Level of Service "C" or better during both the P.M. and Saturday peak periods. However, the southbound left-turn movement would be expected to experience a Level of Service "F" during both of the peak periods. The 95th percentile queue length for the

southbound left-turn movement was estimated to be about 175 and 470 feet during the P.M. and Saturday peak periods, respectively.

Due to the poor level of service, traffic signal warrant analyses were completed. Based on the signal warrant analysis of the proposed traffic volumes, the peak hour warrant (Warrant 3) would not be expected to be met and a traffic signal would not be required at this time. The exclusive eastbound and westbound left-turn lanes should be designed with a minimum of 150 feet of storage to allow for extra storage should the intersection be signalized in the future.

Driveways 1, 2, and 3: Based on the unsignalized intersection capacity analyses, the turning movements would be expected to operate with a Level of Service "D" or better during both the P.M. and Saturday peak periods. The Driveways 1 and 2 were analyzed as a Right-In/Right-Out intersection with a southbound right-turn lane located at each driveway. The southbound right-turn lane at both locations should be designed with a minimum of 150 feet of deceleration length, as reported in the original traffic study. Similarly, Driveway 3 was analyzed as a Right-In/Right-Out intersection with an eastbound right-turn lane.

SUMMARY / RECOMMENDATIONS

Based on the capacity analyses, the geometric improvements that were identified in the original traffic study will adequately serve the increase in traffic from the change in land-use. ***Figures 5 and 6*** summarize the recommended geometrics and overall levels of service for the study intersections. The only additional geometric improvement that should be considered which would be in addition to the improvements reported in the original traffic study, is the second southbound right-turn lane at the intersection of Sparks Boulevard and the westbound I-80 ramps. The dual right-turn lanes should be included as part of the traffic signal instead of a free flowing right-turn lane. Therefore, the traffic signal will need to be modified to add the appropriate signalization equipment.

We appreciate the opportunity to be of service to you on this very important project. Please feel free to contact us if you should have any questions.

Sincerely,

BRUNGARDT HONOMICHL & COMPANY, P.A.

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SAINT PATRICK JOHNSON, JR.
Exp: 6-30-07
CIVIL
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Project Manager

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James M. Jussel, P.E., PTOE
Project Traffic Engineer

7/11/06

Cc: File

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TABLE 1
TRIP GENERATION

LAND USE	WEEKDAY ADT	WEEKDAY PM PEAK HOUR IN	WEEKDAY PM PEAK HOUR OUT	WEEKDAY PM PEAK HOUR TOTAL	SATURDAY ADT	SATURDAY PEAK HOUR IN	SATURDAY PEAK HOUR OUT	SATURDAY PEAK HOUR TOTAL
PHASE 1								
Shopping Center								
826,700 Square Feet	24,443	1,107	1,200	2,307	31,713	1,620	1,495	3,115
Quality Restaurant								
61,100 Square Feet	5,496	307	151	458	4,785	324	225	549
Baseball Stadium								
7,500 Seats	3,282	257	18	275	3,282	0	0	0
Total	33,221	1,671	1,369	3,040	39,780	1,944	1,720	3,664
Shopping Center PB Trips	-4,747	-224	-224	-448	-6,548	-198	-198	-396
Phase 1 New Trips	28,474	1,447	1,145	2,592	33,232	1,746	1,522	3,268
PHASE 2								
Shopping Center								
250,000 Square Feet	7,384	335	362	697	9,580	489	452	941
RV Sales								
10,000 Square Feet	333	10	16	26	210	15	15	30
Total	7,717	345	378	723	9,790	504	467	971
Shopping Center PB Trips	-1,441	-68	-68	-136	-1,222	-60	-60	-120
Phase 2 New Trips	6,276	277	310	587	8,568	444	407	851
BUILDOUT								
Shopping Center								
1,076,700 Square Feet	31,827	1,442	1,562	3,004	41,293	2,109	1,947	4,056
Quality Restaurant								
61,100 Square Feet	5,496	307	151	458	4,785	324	225	549
Baseball Stadium								
7,500 Seats	3,282	257	18	275	3,282	0	0	0
RV Sales								
10,000 Square Feet	333	10	16	26	210	15	15	30
Total	40,938	2,016	1,747	3,763	49,570	2,448	2,187	4,635
Shopping Center PB Trips	-6,188	-292	-292	-584	-7,770	-258	-258	-516
Buildout New Trips	34,750	1,724	1,455	3,179	41,800	2,190	1,929	4,119

Table 2

ITE TRIP GENERATION RATES
7th Edition
(Revised by BHC Rhodes - July 3, 2006)

LAND USE CODE	LAND USE	FLOOR AREA Sq. Ft.	MISC. Quantity	MISC. Unit	ADT (VPD)	P.M. PEAK HOUR (VPH) IN	P.M. PEAK HOUR (VPH) OUT	ADT (VPD)	SATURDAY PEAK HOUR (VPH) IN	SATURDAY PEAK HOUR (VPH) OUT
PHASE 1										
820	Shopping Center	1,365,880			35,170	1,600	1,733	45,271	2,331	2,152
(1)	Baseball Stadium (7,500 Seats)				3,282	257	18	3,282	0	0
310	Hotel		100	Room	522	31	28	667	41	32
230	Condominium / Townhouses		60	Unit	416	26	13	645	15	13
492	Health/Fitness Club	30,000			988	62	60	802	40	38
					35,170	**1,976**	**1,852**		**2,427**	**2,235**
	Pass By Trips for Shopping Center (19%)				-6,823	-323	-323		-285	-285
	Phase 1 Total Trips				**28,347**	**1,653**	**1,529**		**2,142**	**1,950**
PHASE 2										
820	Shopping Center	231,319			5,956	271	294	7,667	395	364
841	RV Sales	10,000			333	10	16	210	15	15
					6,290	**281**	**310**		**410**	**379**
	Pass By Trips for Shopping Center (19%)				-1,156	-55	-55		-48	-48
	Phase 2 Total Trips				**7,445**	**226**	**255**		**362**	**331**
FULL BUILDOUT										
820	Shopping Center	1,597,199			41,126	1,871	2,027	52,938	2,726	2,516
841	RV Sales	10,000			333	10	16	210	15	15
(1)	Baseball Stadium (7,500 Seats)				3,282	257	18	3,282	0	0
310	Hotel		100	Room	522	31	28	667	41	32
230	Condominium / Townhouses		60	Unit	416	26	13	645	15	13
492	Health/Fitness Club	30,000			988	62	60	802	40	38
					44,742	**2,257**	**2,162**		**2,837**	**2,614**
	Pass By Trips for Shopping Center (19.4% Weekday Peak, 12.7% Sat Peak)				-7,979	-378	-378		-333	-333
	Total Full Buildout Trips:				**36,763**	**1,879**	**1,784**		**2,504**	**2,281**
	Full Buildout Trips (Table 1 of the Original Study):				**34,750**	**1,724**	**1,455**		**2,190**	**1,929**
	Percent Change:					**9%**	**23%**		**14%**	**18%**

(1) Trip Generation from Original Traffic Study

Revised Table 4
Intersection Level Service and Delay Results
Weekday PM Peak Hour and Saturday Peak Hour

INTERSECTION	WEEKDAY EXISTING	WEEKDAY EXIST. + BUILDOUT	SATURDAY EXIST.+ BUILD
Lincoln Way and Sparks Blvd			
Signalized			
Existing Lanes #	C21.5	F166	F301
With Improvements	N/A	C28.9	D40.5
Sparks/ South Access (Drive 2)			
Unsignalized			
EB Right	B10.1	C18.4	D29.5
Sparks/ I-80 WB Ramps			
Signalized			
Existing Lanes #	B13.7	D40.9	C29.5
With Improvements	N/A	A3.3	C34.6
Soutbound Dual Right-turn lanes	N/A	A6.8	B17.4
Sparks/ I-80 EB Ramps			
Signalized			
Existing Lanes #	C31.2	F105	D46.0
With Improvements	N/A	D46.2	C33.0
Lincoln/ East Access (Drive 3)			
Unsignalized			
NB Right	N/A	C15.5	C18.6
Lincoln/ Main Access			
Roundabout			
EB Lincoln Way Approach	N/A	A8.4	B15.8
WB Lincoln Way Approach	N/A	A4.5	A5.4
South Approach	N/A	A6.4	A8.3
North Approach	N/A	B11.7	C20.4
Ball Park Approach	N/A	A7.7	A9.8
Lincoln/ West Access			
Roundabout			
West Approach	N/A	A3.2	A4.2
South Approach	N/A	A3.4	A4.1
East Approach	N/A	A4.4	A4.7
North Approach	N/A	A5.5	A6.5
Lincoln/ RV Sales Access/ Marina Gateway			
Unsignalized			
WB Left	N/A	A7.8	A8.0
NB Left	N/A	B14.8	C18.2
NB Right	N/A	B11.3	B12.6
EB Left	N/A	A2.4	A2.2
SB Left	N/A	F64.4	F317.0
SB Right	N/A	A5.5	B10.8
Sparks/ North Access (Drive 1)			
Unsignalized			
EB Right	N/A	B11.0	B12.3

Capacity analysis results completed by Solaegui Engineers, LTD.

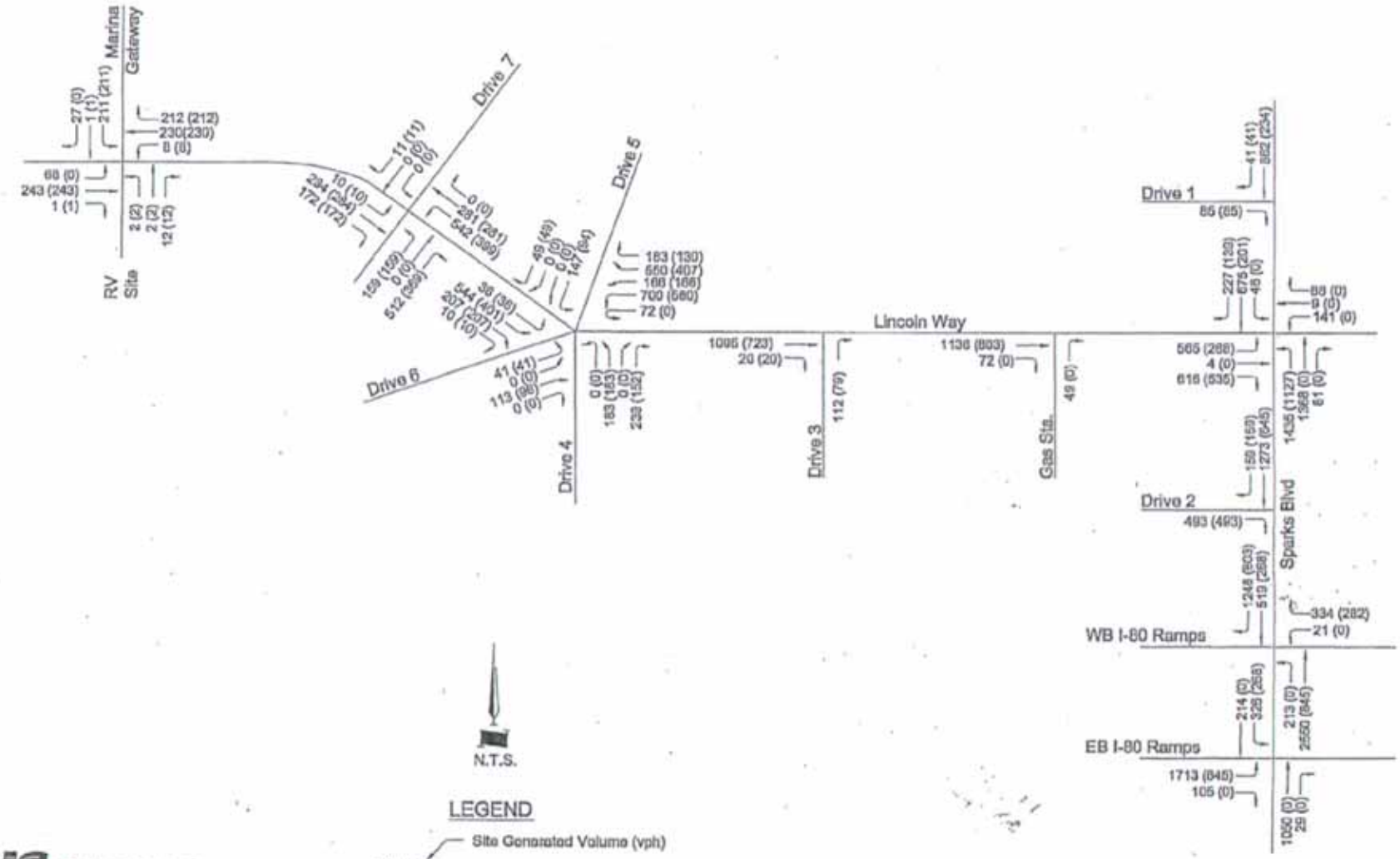

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FIGURE 1
Existing Plus Full Development Traffic Volumes
P.M. Peak Hour

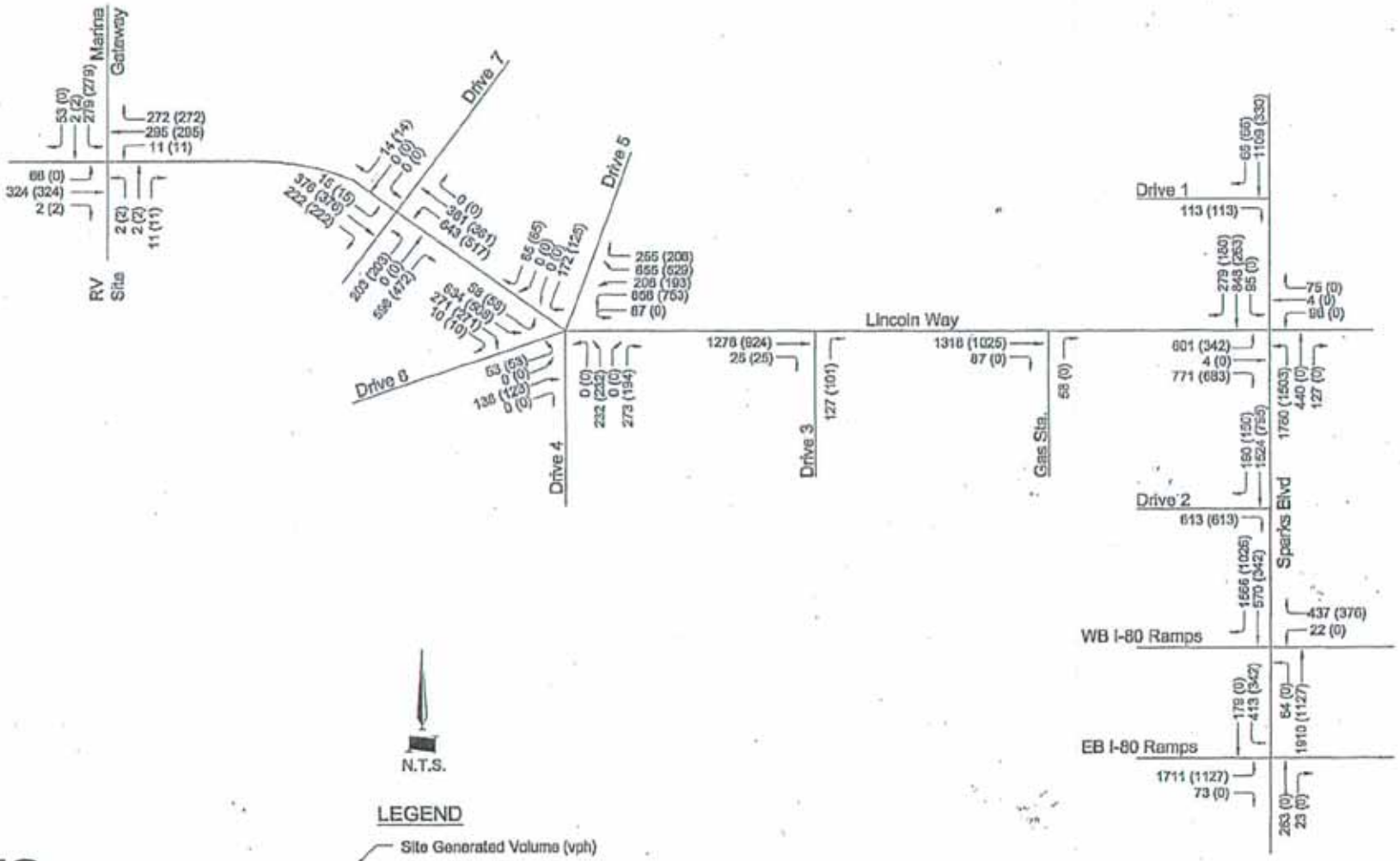

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FIGURE 2
Existing Plus Full Development Traffic Volumes Saturday Peak Hour

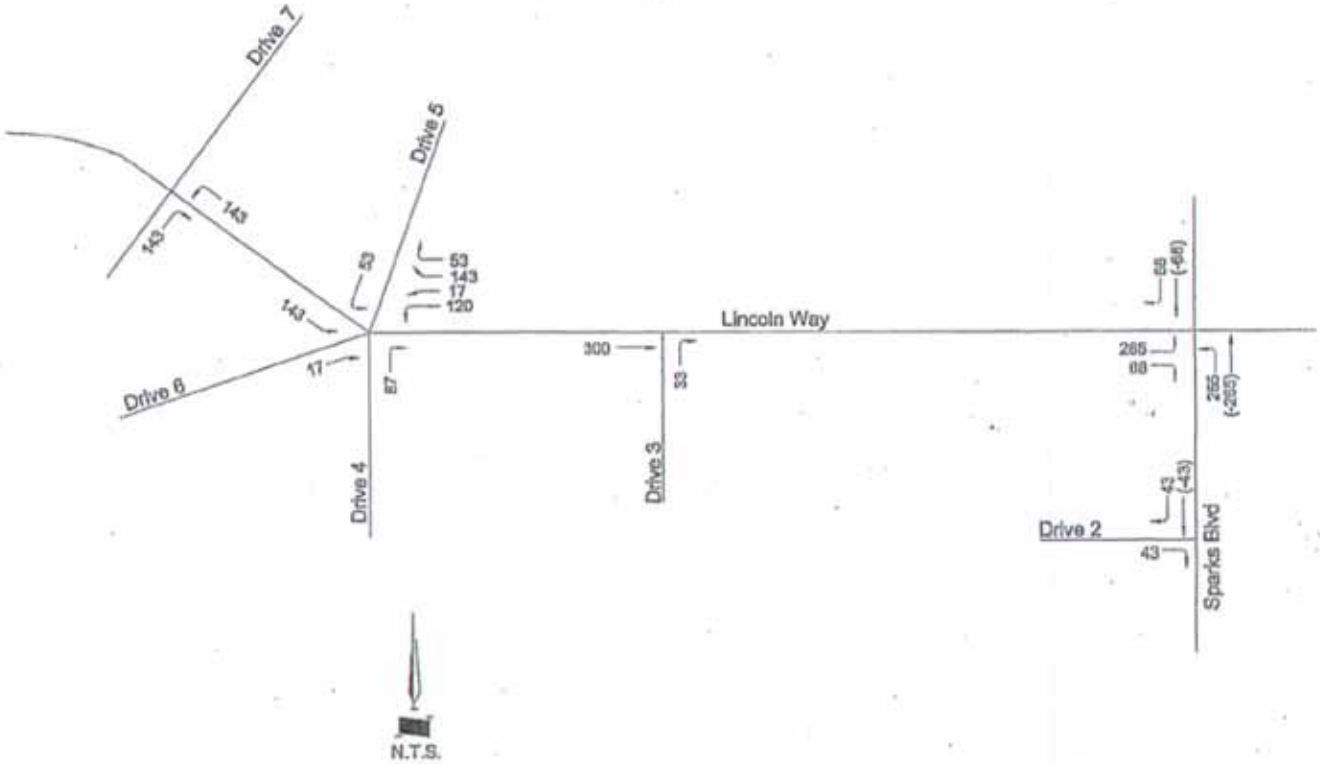

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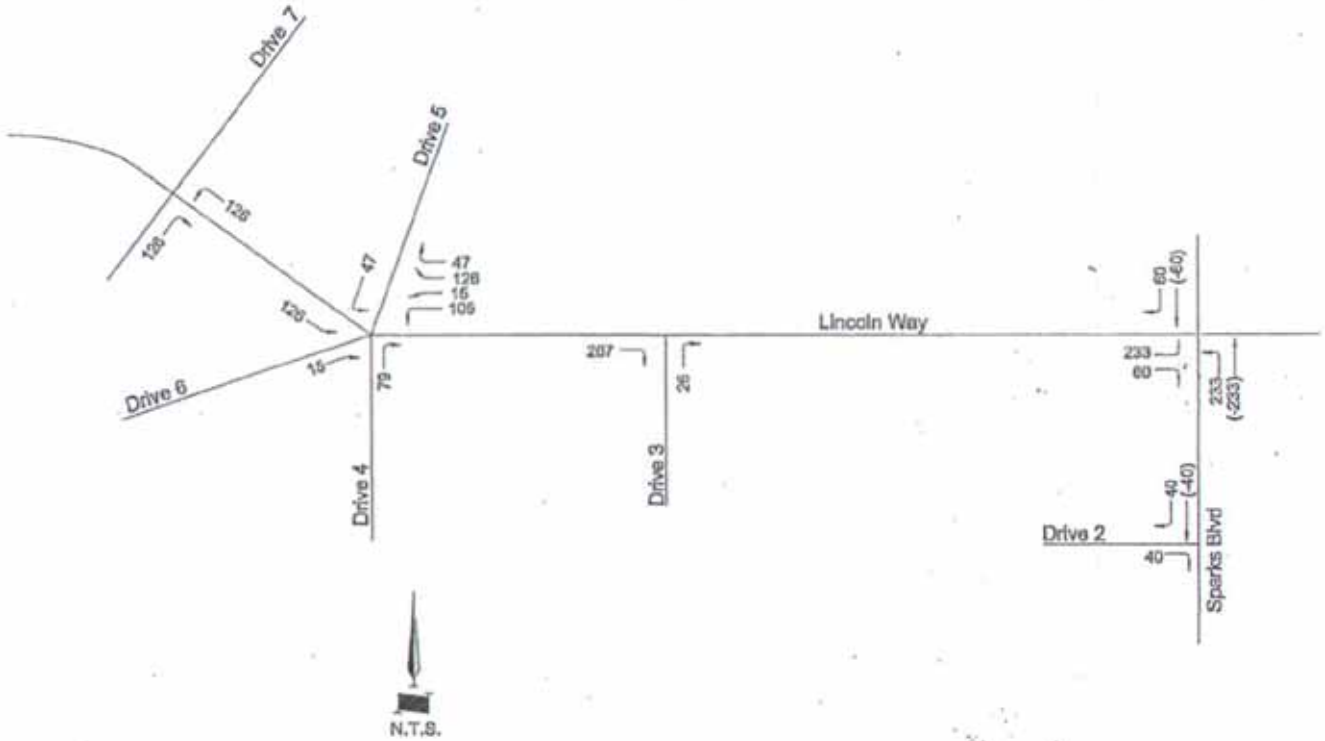
FIGURE 3
Existing Plus Full Development Pass-By Trips
P.M. ık Hour



LEGEND

500 — Total Pass-By Trip Volume

— Vehicle Movement

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FIGURE 4
Existing Plus Full Development Pass-By Trips Saturday Peak Hour

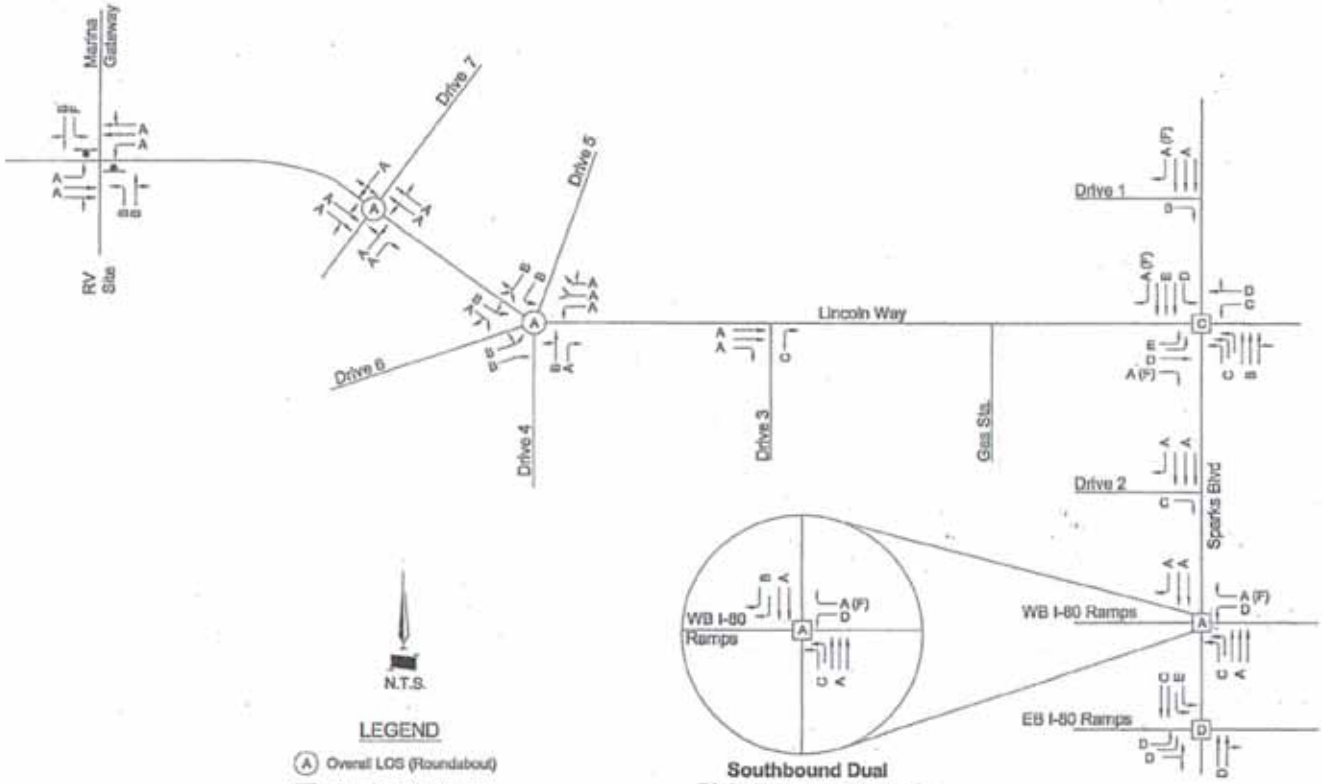

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FIGURE 5
Existing Plus Full Development Level of Service P.M. Peak Hour

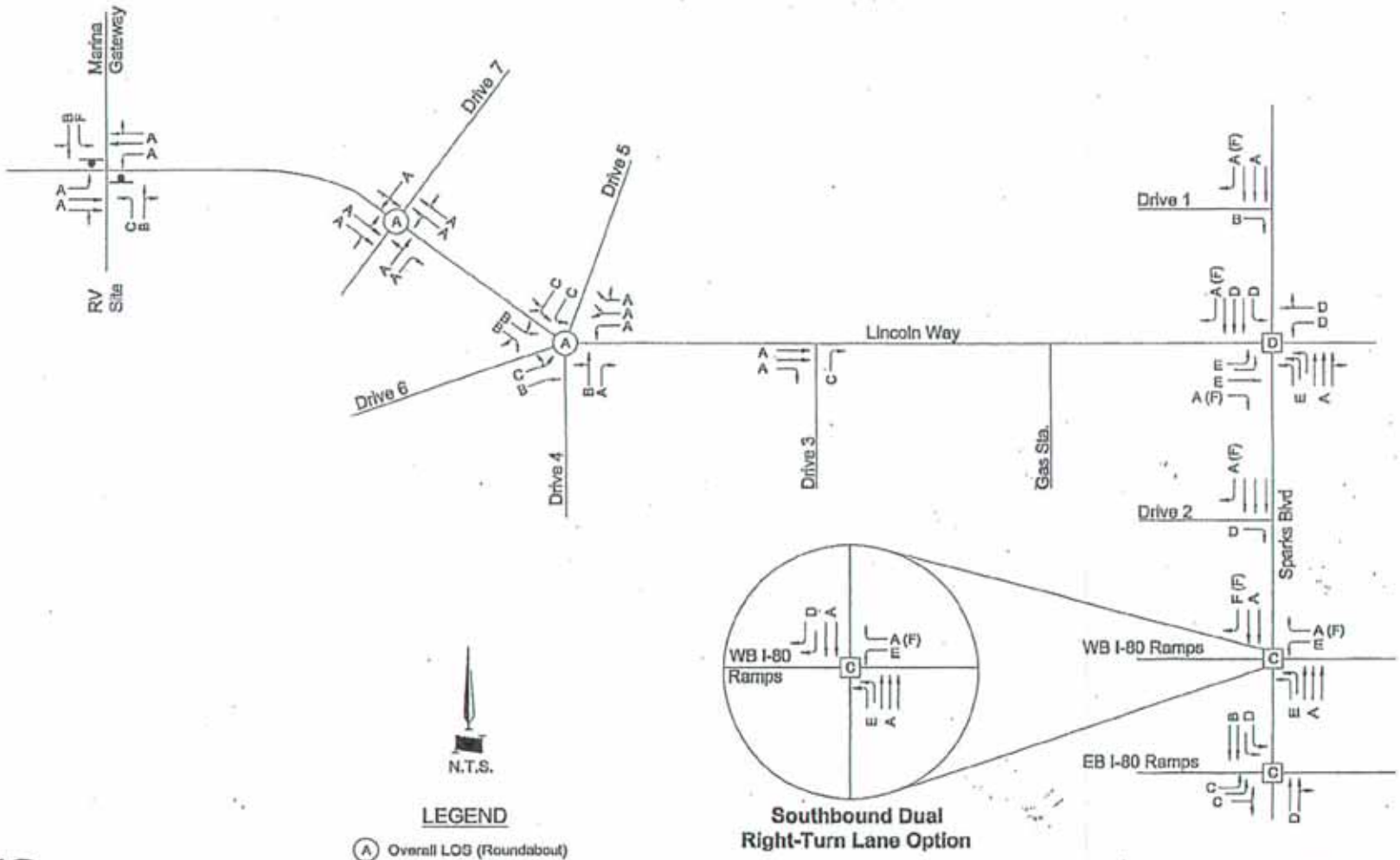

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FIGURE 6
Existing Plus Full Development Level of Service Saturday Peak Hour

Ourston Roundabout Engineering

July 11, 2006

Brungardt Honomichl & Company, P.A.
10895 Grandview, Suite 150
Overland Park, KS 66210

Attention: Mr. James M. Jussel, P.E., PTOE

Dear Jim:

Re: DRAFT Roundabout Peer Review
Lincoln Way at Drive 4/5/6 and Lincoln Way at Drive 7
Legends of Sparks Development, Sparks, Nevada
Our Project No. 06913.800 and 801

Ourston Roundabout Engineering has conducted a preliminary review of two multi-lane roundabouts proposed in the Legends of Sparks development in Sparks, Nevada. The easterly roundabout is located at Lincoln Way and Drives 4, 5 and 6, and is a 5-leg design with a 3-lane entry westbound on Lincoln Way. The distance between the roundabout and Sparks Boulevard to the east is about 760 feet, with a 3-lane section westbound due to the triple left turn northbound at Sparks Boulevard. The westerly roundabout is located at Lincoln Way and Drive 7, and is a 4-leg design. We will refer to them henceforth as "5-leg" and "4-leg".

In the process of our review we checked the following items:

- Location and size of the roundabout ("composition").
- Accommodation of the design vehicle.
- Lane configurations.
- Deflection (fastest-paths).
- Entry and exit path overlap.
- Other considerations such as pedestrian and cyclist facilities, etc.

We have provided a written description of our review. Even with this review it may still be difficult to achieve a design that is ideally balanced between safety, capacity and property impacts. This is the substance of expert design that our comments cannot provide.

The review is split into two parts: a traffic analysis, in which we looked at traffic volumes and proposed lane configurations, including a Rodel analysis, and a design review, in which we checked for deflection, path overlap, etc.

Results of Traffic Analysis

Our traffic analysis focused on Existing plus Full Development traffic forecasts for the PM and Saturday peak hours, as provided on July 7, 2006.

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The Rodel analysis for the 5-leg roundabout shows the number of lanes is correct. A 3-lane entry is shown for Lincoln Way westbound – it may flare from 2 lanes to 3, or be 3 continuous lanes as shown in the site layout. The left lane should be striped as an exclusive lane to Drive 4, the middle lane should lead to Drive 4, Drive 6 and Lincoln Way, and the right lane should lead to Lincoln Way and Drive 5. This in effect creates a double "left" turn westbound.

For Lincoln Way eastbound, the movement to Lincoln Way needs to be split over two lanes, i.e. a double "left" eastbound. However, to do this with a 2-lane circulatory road means creating the potential for an exit-circulating crash at Drive 4, as motorists in the outer lane making a "left" turn from Lincoln Way westbound have to cross the exit stripe at Drive 4. Therefore, a 3-lane circulatory road and spiral striping is also required between Lincoln Way eastbound and Drive 4. With this, an entry that flares to 3 lanes may be provided eastbound on Lincoln Way.

A conceptual plan of the proposed lane configurations is provided as Figure 1.

The exit of Drive 5 only needs to be 1-lane. This will allow the Yield line westbound on Lincoln Way to be better aligned so that motorists in the right lane can see to the left past queued vehicles to view circulating traffic.

The Rodel analysis for the 4-leg roundabout shows the number of lanes is correct. The analysis for both roundabouts at the 50th percentile confidence level is included at the end of this report.

Results of Design Review

Due to uncertainty with the layout of the 5-leg roundabout we performed design checks on the 4-leg roundabout first.

For the 4-leg design, entry deflection is too low on Lincoln Way westbound and on Drive 7 northbound, and is too high on Lincoln Way eastbound. We estimate the following R1 values:

- 234 feet for Drive 7 southbound.
- 195 feet for Lincoln Way eastbound.
- 527 feet for Drive 7 northbound.
- 457 feet for Lincoln Way westbound.

Deflection is adequate on Drive 7 southbound. Deflection is also adequate for the worst-case westbound right turn from Lincoln Way to Drive 7. We estimate an R5 value of 244 feet.

Entry and exit widths on Lincoln Way are likely wider than necessary for the design vehicle, and could be narrowed to increase deflection. A suggested width for a 2-lane entry is 27 feet, measured to curb face. (Dimensions are input into Rodel

measured to curb face, not edge of pavement). Deflection could also be increased by re-aligning the approaches to the left of centerline. Deflection could be lessened on Lincoln Way eastbound by re-aligning the approach, or by relocating the roundabout further south. Relocating the roundabout further south could also help to balance deflection along Lincoln Way and create better overall speed consistency.

There is entry path overlap on Drive 7 northbound. Otherwise, entry and exit paths are acceptable. Overall the roundabout is appropriately sized.

We noted insufficient deflection on certain approaches for the 5-leg roundabout as well. We did not perform all possible design checks since a 3-lane circulatory road is needed between Lincoln Way eastbound and Drive 4. However, overall the roundabout is appropriately located and sized. We do note the 1-lane section of the circulatory road should be made wider to accommodate the design vehicle, or a partial truck apron introduced for vehicle overtracking.

Conclusions

For the 5-leg roundabout, we recommend that:

- The lane configurations be as per Figure 1, with a 3-lane circulatory road between Lincoln Way eastbound and Drive 4 to eliminate the potential for an exit-circulating crash at Drive 4.
- The exit for Drive 5 be 1-lane so the Yield line westbound on Lincoln Way can be better aligned.
- Once the layout is modified, deflection be checked to ensure appropriate R1 values on all entries and good speed consistency.
- The 1-lane section of circulatory road be made wider to accommodate the design vehicle, or a partial truck apron introduced for vehicle overtracking.

For the 4-leg roundabout, we recommend that:

- Deflection be increased on Lincoln Way westbound and Drive 7 northbound, and lessened on Lincoln Way eastbound.
- Consideration be given to relocating the roundabout further south to balance deflection along Lincoln Way and create better overall speed consistency.
- Entry path overlap be eliminated on Drive 7 northbound.

The geometric design of the 5-leg roundabout will be critical to ensure it operates safely and efficiently, and within the range of delay and queuing values predicted with Rodel. For example, the tight entry radii between certain legs of the 5-leg roundabout will negatively affect capacity. We recommend that expert advice be sought early in any future design process so that both designs are ideally balanced between safety, capacity and property impacts.

Good signing and striping will also be critical to ensure the success of both roundabouts, in particular the 5-leg roundabout. For example, the circulatory road will need to be wider than the preceding entry, and striped so that the outside lane is wider than the inside lane. We recommend that at least 2 sets of lane arrows and accompanying lane use control signs be installed on the westbound approach of the 5-leg roundabout to get motorists in the correct lane before they enter. Otherwise, there will be lane changing and a corresponding higher probability of collisions inside the roundabout.

Yours truly,

OURSTON ROUNDABOUT ENGINEERING, INC.

Phil Weber, P.Eng.
Senior Project Manager

PW/br
Attch.

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Ourston Roundabout Engineering

Project:	Legends of Sparks Development	**Drawn By:**	PAW
Project No.:	06913.800 **Date:** July 10/06	**Sheet:**	1 of 1
Intersection:	Lincoln Way and Drive 4/5/6		
Time Period:	Peak Hours, Existing plus Full Development		

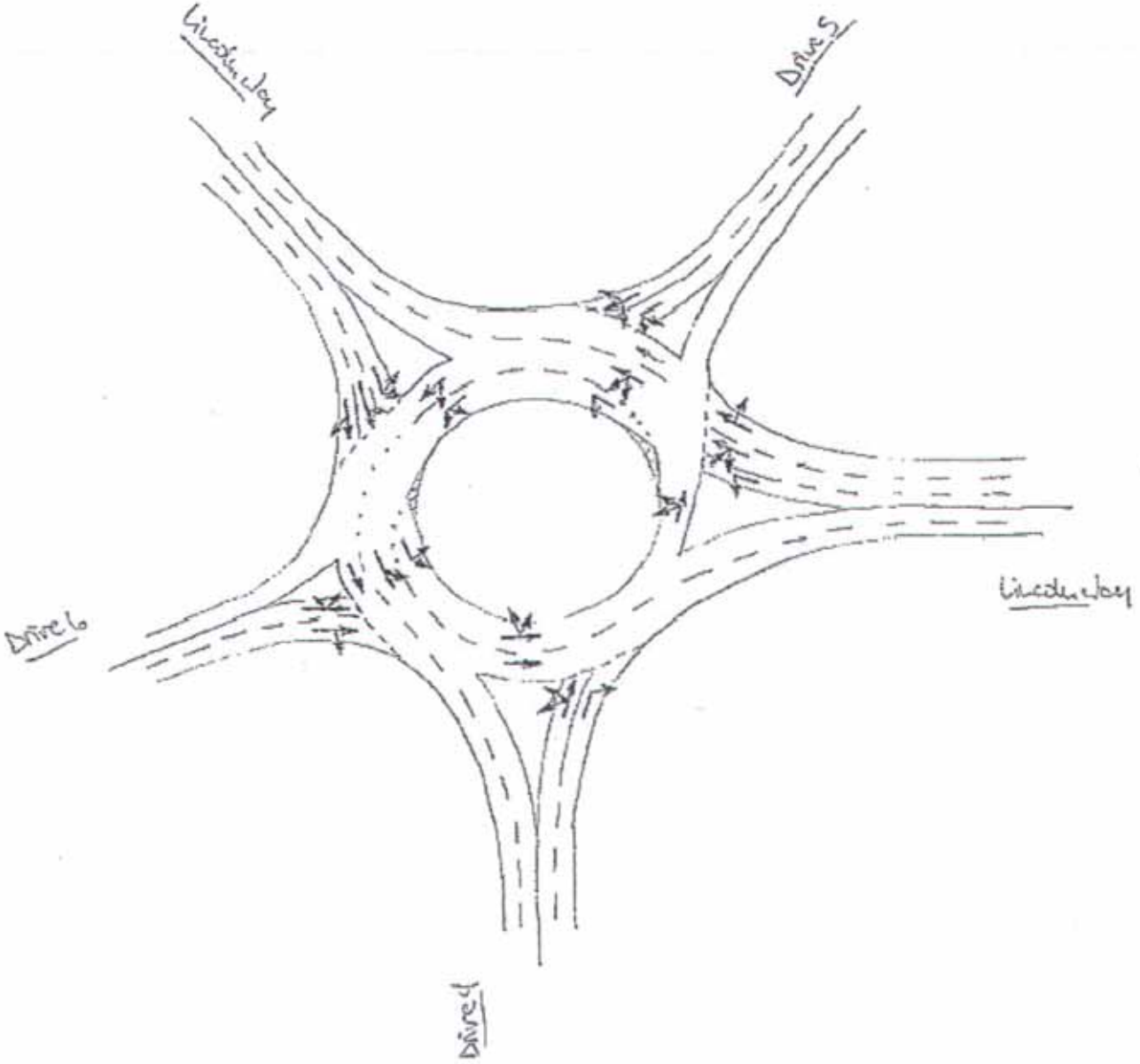


Figure 1

LINCOLN WAY/ DRIVE 4/5/6
LEGENDS OF SPARKS DEVELOPMENT, NEVADA

RODEL 1 Roundabout Analysis, Existing plus Full Development
50th Percentile Confidence Level

RODEL 1.9.2

		8.00	11.25	8.00	8.00	11.50									90
		30.00	20.00	30.00	30.00	30.00									15
		7.30	7.30	7.30	7.30	7.30								15	75
		20.00	20.00	20.00	20.00	20.00									15.00
		25.00	25.00	25.00	25.00	25.00								15	75
		70.00	70.00	70.00	70.00	70.00									VEH
		0	0	0	0	0									PM
DRIVE 5	1.05	49	0	0	147	0		1.00	50	0.75	1.125	0.75	15	45	75
LINCOLN EB	1.05	10	207	544	36	0		1.00	50	0.75	1.125	0.75	15	45	75
DRIVE 6	1.05	0	113	0	41	0		1.00	50	0.75	1.125	0.75	15	45	75
DRIVE 4	1.05	239	0	183	0	0		1.00	50	0.75	1.125	0.75	15	45	75
LINCOLN WB	1.05	183	550	166	700	72		1.00	50	0.75	1.125	0.75	15	45	75
FLOW	veh	196	797	154	422	1671									11.0
CAPACITY	veh	1260	2080	1264	1736	2803									B
AVE DELAY	mins	0.19	0.18	0.19	0.18	0.19									B
MAX DELAY	mins	0.21	0.20	0.21	0.19	0.21									
AVE QUEUE	veh	0	1	0	0	2									9.9
MAX QUEUE	veh	0	1	0	0	2									149

PM Peak Hour

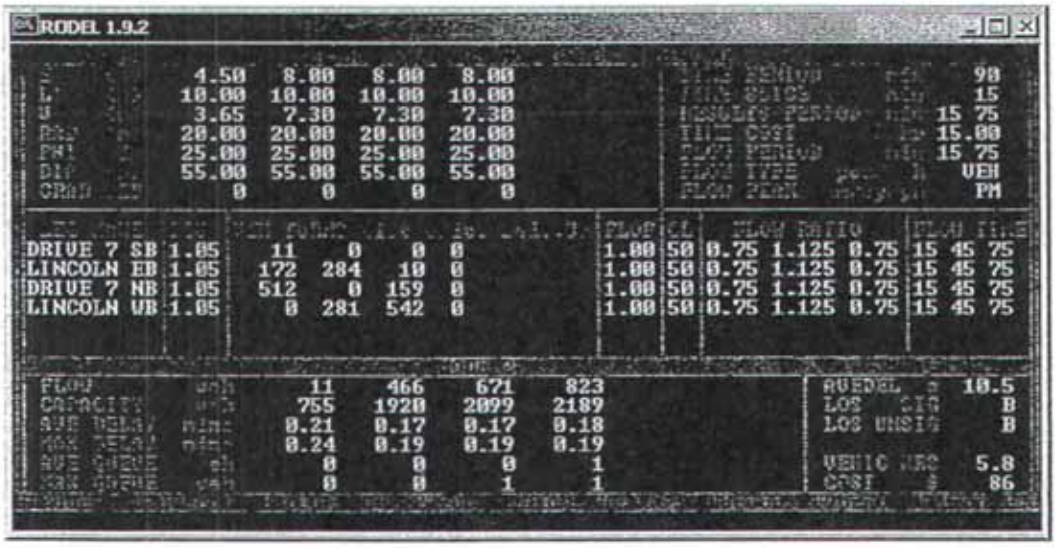
RODEL 1.9.2

		8.00	11.25	8.00	8.00	11.50									90
		30.00	20.00	30.00	30.00	30.00									15
		7.30	7.30	7.30	7.30	7.30								15	75
		20.00	20.00	20.00	20.00	20.00									15.00
		25.00	25.00	25.00	25.00	25.00								15	75
		70.00	70.00	70.00	70.00	70.00									VEH
		0	0	0	0	0									OP
DRIVE 5	1.05	65	0	0	172	0		1.00	50	1.00	1.000	1.00	15	45	75
LINCOLN EB	1.05	10	271	634	58	0		1.00	50	1.00	1.000	1.00	15	45	75
DRIVE 6	1.05	0	138	0	53	0		1.00	50	1.00	1.000	1.00	15	45	75
DRIVE 4	1.05	273	0	232	0	0		1.00	50	1.00	1.000	1.00	15	45	75
LINCOLN WB	1.05	255	655	208	858	87		1.00	50	1.00	1.000	1.00	15	45	75
FLOW	veh	237	973	191	505	2063									12.5
CAPACITY	veh	1020	1908	1029	1617	2742									B
AVE DELAY	mins	0.21	0.20	0.20	0.19	0.22									B
MAX DELAY	mins	0.21	0.20	0.20	0.19	0.22									
AVE QUEUE	veh	0	1	0	0	3									13.8
MAX QUEUE	veh	0	1	0	0	3									207

Saturday Peak Hour

LINCOLN WAY/ DRIVE 7
LEGENDS OF SPARKS DEVELOPMENT, NEVADA

RODEL 1 Roundabout Analysis, Existing plus Full Development
50th Percentile Confidence Level



RODEL 1.9.2

E	4.50	8.00	8.00	8.00
L	10.00	10.00	10.00	10.00
I	3.65	7.30	7.30	7.30
DAD	20.00	20.00	20.00	20.00
PHI	25.00	25.00	25.00	25.00
DIA	55.00	55.00	55.00	55.00
GRAD	0	0	0	0

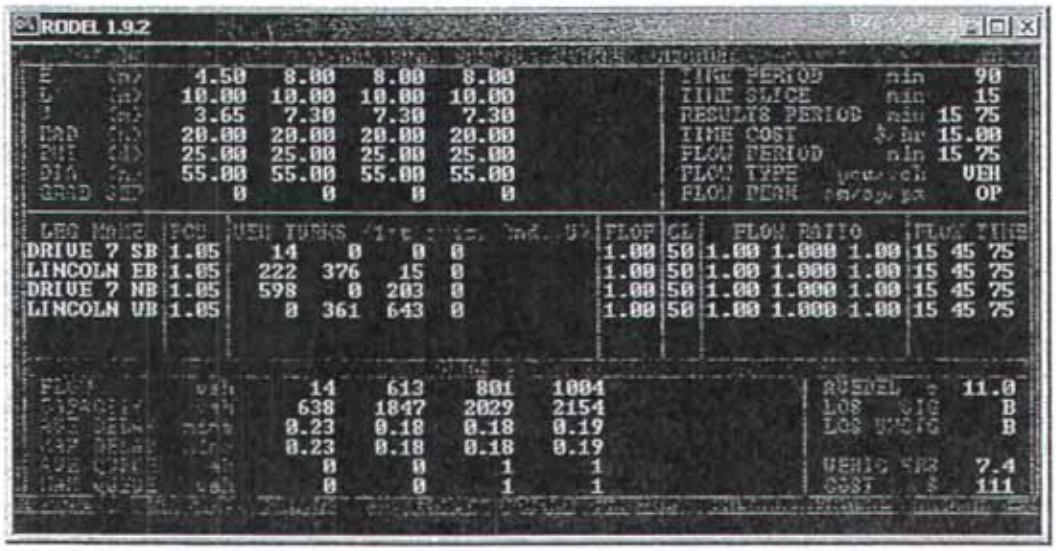
TIME PERIOD min 90
TIME SLICE min 15
RESULTS PERIOD min 15 75
TIME COST $/hr 15.00
FLOW PERIOD min 15 75
FLOW TYPE VEH
FLOW PEAK PM

LEG NAME	PCU	VEH TURNS				FLOW	CL	FLOW RATIO			FLOW TIME
DRIVE 7 SB	1.05	11	0	0	0	1.00	50	0.75	1.125	0.75	15 45 75
LINCOLN EB	1.05	172	284	10	0	1.00	50	0.75	1.125	0.75	15 45 75
DRIVE 7 NB	1.05	512	0	159	0	1.00	50	0.75	1.125	0.75	15 45 75
LINCOLN WB	1.05	0	281	542	0	1.00	50	0.75	1.125	0.75	15 45 75

FLOW veh	11	466	671	823
CAPACITY veh	755	1920	2099	2189
AVE DELAY min	0.21	0.17	0.17	0.18
MAX DELAY min	0.24	0.19	0.19	0.19
AVE QUEUE veh	0	0	0	1
MAX QUEUE veh	0	0	1	1

RUEDEL s 10.5
LOS SIG B
LOS UNSIG B
VEHIC DEL 5.8
COST $ 86

PM Peak Hour



RODEL 1.9.2

E	4.50	8.00	8.00	8.00
L	10.00	10.00	10.00	10.00
I	3.65	7.30	7.30	7.30
DAD	20.00	20.00	20.00	20.00
PHI	25.00	25.00	25.00	25.00
DIA	55.00	55.00	55.00	55.00
GRAD	0	0	0	0

TIME PERIOD min 90
TIME SLICE min 15
RESULTS PERIOD min 15 75
TIME COST $/hr 15.00
FLOW PERIOD min 15 75
FLOW TYPE VEH
FLOW PEAK OP

LEG NAME	PCU	VEH TURNS				FLOW	CL	FLOW RATIO			FLOW TIME
DRIVE 7 SB	1.05	14	0	0	0	1.00	50	1.00	1.000	1.00	15 45 75
LINCOLN EB	1.05	222	376	15	0	1.00	50	1.00	1.000	1.00	15 45 75
DRIVE 7 NB	1.05	598	0	203	0	1.00	50	1.00	1.000	1.00	15 45 75
LINCOLN WB	1.05	0	361	643	0	1.00	50	1.00	1.000	1.00	15 45 75

FLOW veh	14	613	801	1004
CAPACITY veh	638	1847	2029	2154
AVE DELAY min	0.23	0.18	0.18	0.19
MAX DELAY min	0.23	0.18	0.18	0.19
AVE QUEUE veh	0	0	1	1
MAX QUEUE veh	0	0	1	1

RUEDEL s 11.0
LOS SIG B
LOS UNSIG B
VEHIC DEL 7.4
COST $ 111

Saturday Peak Hour

THE LEGENDS AT SPARKS MARINA

EXHIBIT A-12
SITE PLAN
REVIEW ZONES

WASHOE COUNTY
RECORDER

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KATHRYN L. BURKE, RECORDER

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TIMOTHY THOMPSON
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